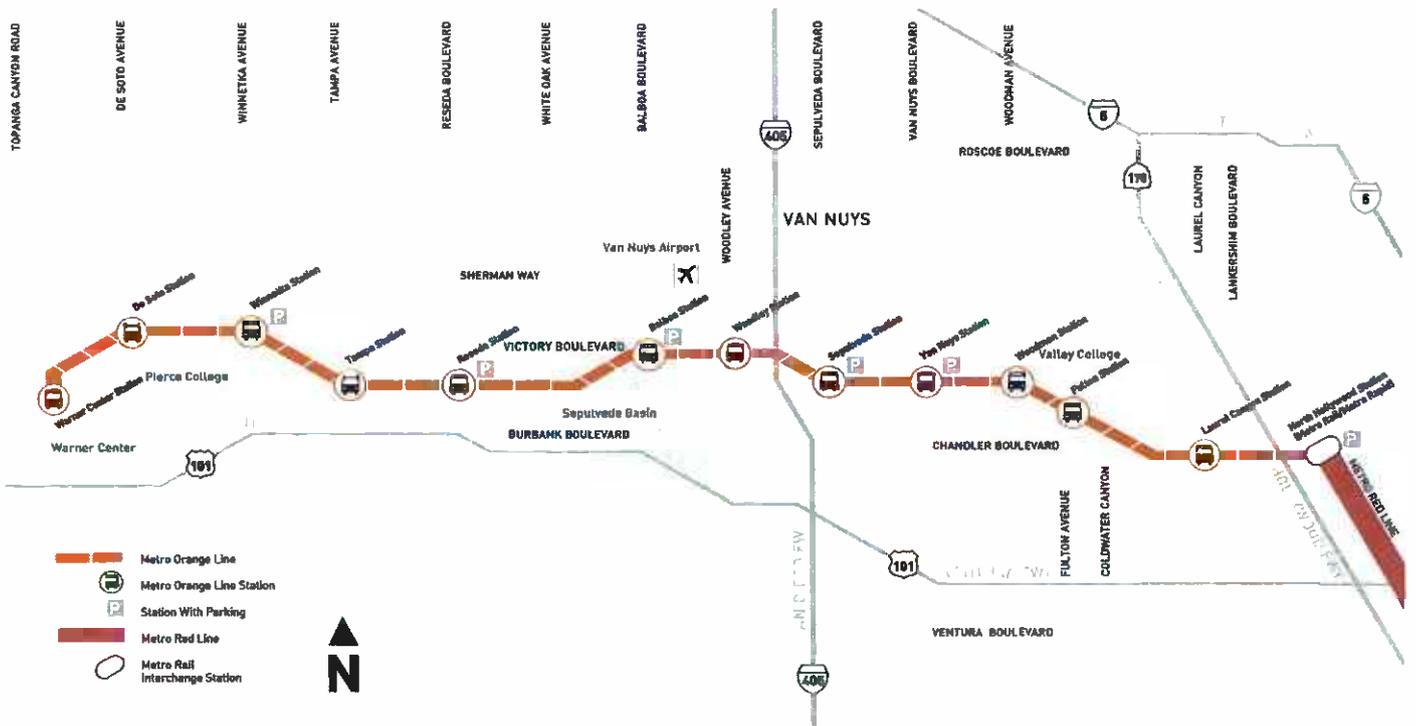


# Metro Orange Line

October 2004



## TABLE OF CONTENTS

	Page No.
Project Overview.....	1
Management Issues.....	2-4
Project Status	
• Project Scope.....	5
• Schedule	
o Key Milestones Six-Month Look Ahead .....	6
o Summary Schedule.....	7
o Schedule Narrative .....	8
• Project Cost Status and Analysis – Busway.....	9
• Project Cost Status and Analysis – Bikeway.....	10
• Project Cost Status and Analysis – Pipeline.....	11
• Financial/Grant Status.....	12-13
• Staffing.....	14-16
• Real Estate.....	17
• Environmental.....	18
• Community Relations.....	18
• Quality Assurance.....	18
• Safety.....	19
• Art Development.....	19
• Contract Construction Status.....	20-23
Chronology of Events.....	24-28
Project Photos.....	29-37
Appendices.....	38-41

## PROJECT OVERVIEW

*The Metro Orange Line Project consists of buses operating in exclusive lanes within an approximate thirteen (13) mile stretch on LACMTA right-of-way (ROW) and one (1) mile of mixed flow operation within public streets. Terminal stations are located near the North Hollywood Metro Red Line Subway Station and the planned Warner Center Transit Hub. The Orange Line will have roughly one stop per mile located at major cross streets. Low floor CNG-powered articulated vehicles will be given signal priority at grade and at cross streets, which will reduce end to end travel times between the thirteen (13) stations. With the exception of the Warner Center Transit Hub, all stations will provide platforms for east bound and west bound travel. Warner Center Transit Hub is currently being constructed by the City of Los Angeles and will include bus stops for loading/unloading of passengers and layover space on Owensmouth Avenue. Canopies will be provided at all stations. Station equipment and amenities will include ticket vending machines, stand alone validators, benches, bike racks, map case(s), signage, public telephones, closed circuit television cameras and a public address system. Variable message signs will provide real time information on bus arrival times at the respective station. In addition to the existing 915 spaces at the North Hollywood Metro Red Line Subway Station and the 150 parking spaces at the Balboa Park and Ride, the Orange Line will provide approximately 3,213 new parking spaces for the park and ride station locations. Other related project scope includes modifications to an existing Metro bus division, vehicle procurement and implementation of the Universal Fare System (UFS).*

The Contractor submitted a schedule update this month that forecasts a cumulative six-month delay to the Contractor's Contract Substantial Completion Milestone date of June 16, 2005. Based on the forecast delay by the Contractor, the August 2005 Revenue Operations Date appears to be in jeopardy. MTA requested and received a recovery plan from the Contractor. The Board of Directors may elect to consider a recovery plan in December 2004, which would mitigate a portion or all of the delay to Contract Milestones.

The Contractor's design percent complete is 98.6% and construction physical percent complete is 44.7%.

To date, the expenditures for the busway portion of the Orange Line are \$136.3 million or 41.4% of the \$329.5 million Original Budget. The expenditures for the bikeway portion are \$1.7 million or 15.6% of the \$10.6 million Current Budget.

All real estate new acquisitions have been acquired and turned over to the C0675 Design/Build Contractor. The number of leases to be terminated for the project is 101 with 99 available to the Contract C0675 Design/Build Contractor. The remaining two leases have been permitted to remain at this time and are under review for final disposition. There is no impact to the Project.

Other Projects providing equipment for the Orange Line Project remain on schedule. Project staff continues to meet with appropriate MTA staff to discuss status of other related projects (managed by other MTA departments) to identify any schedule risks that may result in impact to the Contract C0675 Contract Milestones or to the Orange Line Project Revenue Operation Date. These projects being monitored are Articulated Vehicle Procurement, Advanced Traffic Management System (ATMS), and Universal Fare System (UFS).

## MANAGEMENT ISSUES

**Concern No. 1:** Citizens Organized for Smart Transit (COST) has filed a lawsuit contesting the validity of the Environmental Impact Report of the Metro Orange Line.

**Status/Action** The trial was held on December 20, 2002 and the judge ruled in favor of the MTA. On July 19, 2004 the California Court of Appeal reversed the December 2002 decision of the Los Angeles Superior Court, which had rejected a challenge to the Environmental Impact Report (EIR) for the Orange Line. The Court of Appeal rejected each of the grounds except one. The Court found that the MTA should have studied the possibility of multiple east-west Rapid Bus lines in the San Fernando Valley as an alternative to the Orange Line, and that the failure to do so renders the EIR invalid. The Court of Appeal decision did not enjoin further construction on the Project. On July 30, 2004, the Superior Court denied COST request for Stay of Project citing lack of jurisdiction. On August 2, 2004 the California Court of Appeal issued a temporary stay and the MTA on August 3, 2004, issued a suspension of work to the C0675 Design/Build Contractor. After the temporary stay expired on August 19, 2004, the MTA lifted the suspension of work to the C0675 Design/Build Contractor on August 26, 2004. Please see Chronology of Events section for COST related actions in September and October 2004.

**Concern No. 2:** Park-and-Ride site at Metro Orange Line's western terminus.

**Status/Action** The western terminus at the Warner Center Transit Hub does not currently include parking for Orange Line Project patrons. In February 2004, the MTA Board approved proceeding with negotiations to purchase the Boeing site identified, as the MTA Board preferred option for a park-and-ride site. MTA and Boeing have signed a purchase agreement that includes environmental contingencies. Boeing is nearing completion of the demolition of the structure on the property. After demolition is complete, MTA will then proceed with additional environmental assessment, which must be completed prior to the sale being finalized. MTA staff continues to develop a "construction only" procurement package for the park-and-ride scope of work, which includes extending the busway to the new station at the park-and-ride location.

As requested by LADOT, LABOE and Councilman Zine's office, the MTA has tentatively agreed to include the widening of Canoga Avenue as part of the Project provided that the City pays the cost of construction. Subject to City Council approval, the widening, which is included in the latest zoning plan, may be funded as part of the Warner Center Specific Fund. Staff continues to prepare an Addendum/Modified Initial Study for the development of a satellite surface park-and-ride lot on MTA-owned property just north of the Boeing property to augment parking to be provided at the Boeing site. MTA staff will request adoption by the MTA Board of the satellite EIR in the near future. The current total cost forecast for this scope of work is in excess of the \$16.5 million allowance included in the adopted budget.

## MANAGEMENT ISSUES

**Concern No. 3:** Traffic Index (TI) for the busway pavement design

**Status/Action** MTA technical staff determined that the pavement thickness proposed by the C0675 Contractor for Asphalt Concrete (AC) paved segments of the busway is not sufficient to ensure a twenty (20) year design service life under axle loads anticipated from the articulated buses proposed for use on the facility. To rectify this situation, the MTA has issued change orders to the C0675 Design/Build Contractor to modify the pavement design. These changes will ensure the desired design service life, enable MTA Operations staff to budget for maintenance activities and costs, and ensure satisfactory busway service quality. A Contract Modification has been negotiated with the C0675 Design/Build Contractor, approved by the Board of Directors and is waiting to be signed by the Contractor.

**Concern No. 4:** C0675 Design/Build Contractor Schedule Performance

**Status/Action** The Contractor submitted a schedule update this month that forecasts a cumulative six-month delay to the Contractor's Contract Substantial Completion Milestone date of June 16, 2005. Based on the forecast delay by the Contractor, the August 2005 Revenue Operations Date appears to be in jeopardy. MTA requested and received a recovery plan from the Contractor. The Board of Directors may elect to consider a recovery plan in December 2004 which would mitigate a portion or all of the delay to Contract Milestones.

**Concern No. 5:** Contract No. C0675 Design/Build contaminated soils removal

**Status/Action** Due to limited access into the work areas, several stockpiles of contaminated/non-hazardous soils that have been generated are still awaiting disposal. The stockpiles were covered with polyethylene to prevent wind or storm water runoff entrainment. MTA is continuing its pre-profiling of all soils to be generated during the swale and bikepath construction. Pre-profiling will allow determination of soil disposition (i.e., reuse in the right-of-way or off-site disposal) prior to excavation. All pre-profiling is scheduled to be complete in the next reporting period. Contaminated/non-hazardous soil removal activities will remain an integral part of the Project until all grading and landscaping operations are completed.

**Concern No. 6:** Soil Contamination and Potential Impact on Project Landscape

**Status/Action** MTA has developed a phased approach in sampling, soil amendment, then implementation of proper landscaping protocols for the Orange Line Project. Soil sampling for agronomic purposes has commenced and samples are currently being analyzed for plant suitability. Pot culture tests, which are an integral part of the soil suitability determination is also currently being performed. Soil amendments considered range from the addition of essential nutrients, increasing the plant pit size up to twice the rootball diameter, up to replacing up to 1/2 of the total volume of backfill soil.

## MANAGEMENT ISSUES

Based on initial soil suitability results, it was determined that backfill replacement was the most appropriate solution to increase plant survivability along the Chandler Corridor, between Tujunga and Whitsett. Mitigation in other parts of the right-of-way are being determined through continuing soil testing and pot cultures.

MTA is also currently identifying the cause of plant failure in significant reaches of the Burbank Bike Path project. While unrelated to the Metro Orange Line, that project's alignment is part of the total Southern Pacific railroad alignment that includes the Orange Line. Therefore, any cause of plant failure at the Burbank Bike Path project may be relevant to the Orange Line mitigation to ensure soil suitability for plant growth.

### **Concern No. 7:** Warner Center Transit Hub

**Status/Action** The City of Los Angeles started construction of the Warner Center Transit Hub (WCTH) in May 2004. An October 1, 2004 access date for the C0675 Design/Build Contractor, SOJV, to perform work related to the busway terminal area was included in the bid documents based on original input from the City of Los Angeles. At this time, full access may not be available to SOJV as the City of Los Angeles contractor(s) may still be working in the area. However, LADOT has agreed to place the 7-footings necessary for the MTA C0675 portion of the busway platform. Therefore, it is not necessary for the C0675 Contractor to perform any work at the WCTH until after the LADOT contractor completes its work in November 2004. The C0675 Contractor access to the WCTH is anticipated to be January 18, 2005 (after the LADOT's "Construction Moratorium" during the holidays) to perform electrical, signage, and miscellaneous work to complete the busway platform. MTA, SOJV and the City of Los Angeles will work together to assure that the Warner Center Transit Hub can be constructed to support the Project's Revenue Operation date of August 2005.

### **Concern No. 8:** Federal Funding for Orange Line Landscape Enhancement

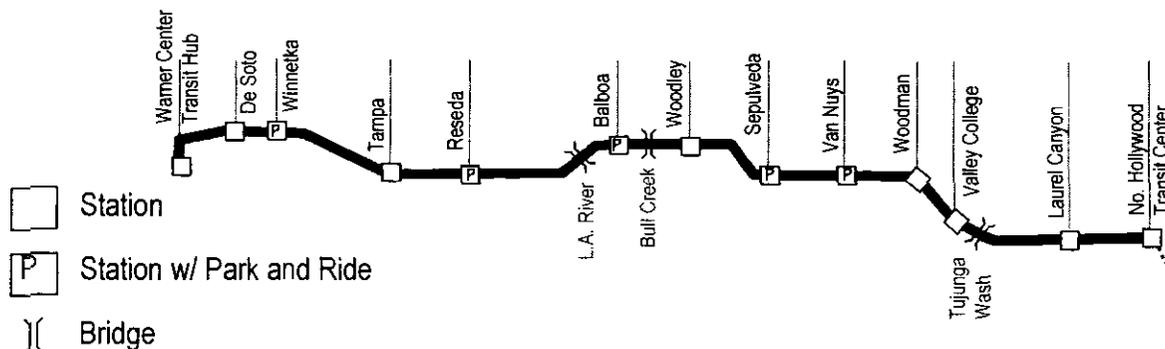
**Status/Action** MTA is pursuing additional Federal Funding for Project landscaping enhancements. The Federal Transit Administration (FTA) has completed its review of MTA's request for a Letter of No Prejudice as a result of this review. MTA may proceed to incur costs for the landscape enhancements without prejudice to possible future Federal participation. Congress has not passed a funding bill consequently this source of funding is not being relied on.

## PROJECT SCOPE

The **Metro Orange Line** includes a busway, which will be 26 feet wide in most locations consisting of one 13-foot travel lane in each direction. The 26 foot wide busway will be located within the LACMTA Right Of Way (ROW), which is generally 100 feet wide in most locations. Within the ROW, landscaping, fencing, and soundwalls, will be provided in accordance with the Final EIR requirements. Concurrent with busway and stations a Bikeway will be constructed. The Bikeway will include a pedestrian path.

Along the ROW, there are approximately 32 street crossings and three pedestrian crossings, which will require some modifications. Traffic signals will be required where the busway crosses streets and at designated pedestrian crossings.

Systems included in the project are variable message signs, Closed Circuit TV, Public Address, Passenger Assistance Telephones, Public Phones, Fiber Optic Cable Transmission and a Universal Fare System. Other related project scope includes Bus Division No. 8 modifications and procurement of twenty-two 60-foot long articulated buses. Lastly, all the systems will be managed from the Bus Operations Control Center, which will be located on the 6<sup>th</sup> floor of the LACMTA Gateway Plaza Headquarters.



**Warner Center Transit Hub**, an LADOT project, is located at the western terminus of the Orange Line. The D/B contractor (*Contract C0675*) will only install all underground utilities and system equipment at the station.

**Three bridges** will be replaced for the Orange Line as follows: 1) Bull Creek Bridge, 2) Tujunga Wash Bridge and the 3) Los Angeles River Bridge.

The largest of these bridges is the Los Angeles River Bridge located in the north end of the Sepulveda Basin. To reduce schedule exposure, MTA designed this bridge to 100%. The new Los Angeles River Bridge was completed in December 2003.

**Recycled Water Pipeline**, the Los Angeles Department of Water and Power has requested the LACMTA to not proceed with the Recycled Water Pipeline. Only short sections of the Pipeline will be completed at Bull Creek and Tujunga Wash Bridges in addition to the Pipeline already completed at the Los Angeles River Bridge.

**Thirteen Stations** will be completed for the Orange Line with locations from east to west identified as follows: 1) No. Hollywood Transit Center, 2) Laurel Canyon, 3) Valley College, 4) Woodman, 5) **Van Nuys**, 6) **Sepulveda**, 7) Woodley, 8) **Balboa Blvd**, 9) **Reseda Blvd**, 10) Tampa Ave, 11) **Winnetka** 12) De Soto and 13) Warner Center Transit Hub. The stations enumerated in bold text above indicate the locations for the Park and Ride facilities. Park and ride facilities will be included in 5 stations and will total approximately 4,278 parking spaces for the anticipated customers, which includes the 915 existing spaces at the NH MRL subway station and the 150 spaces at the Balboa park and ride. The D/B contractor (*Contract C0675*) will build all the stations except for the Warner Center Transit Hub.

**KEY MILESTONE SCHEDULE SIX-MONTH LOOK AHEAD**

	Milestone Date	Oct-04	Nov-04	Dec-04	Jan-05	Feb-05	Mar-05
C0675: Colfax Avenue: Open Intersection to Traffic	10/8/04A	<input type="checkbox"/>					
C0675: De Soto Avenue: Open Intersection to Traffic	10/21/04A	<input type="checkbox"/>					
C0675: Landscape 100% Design - Issue for Construction	10/29/04A	<input type="checkbox"/>					
(Forecast) C0675: Sepulveda Park & Ride 100% Design - Issue for Construction	11/1/04*		<input type="checkbox"/>				
(Forecast) C0675: Bus & Maint. Yard - Crane/Catwalk 100% Design - Issue for Construction	11/1/04*		<input type="checkbox"/>				
(Forecast) C0675: C0675 Bikeway 100% Design Segment 3 - Issue for Construction	11/1/04*		<input type="checkbox"/>				
(Forecast) C0675: Intersection Group 7 100% Design Issue for Construction	11/14/04*		<input type="checkbox"/>				
(Forecast) C0675: Systems Communications Design 100% - Issue for Construction	11/16/04*		<input type="checkbox"/>				
(Forecast) C0675: Tampa Avenue: Open Intersection to Traffic	11/23/04*		<input type="checkbox"/>				
(Forecast) C0675: Ethel Avenue: Open Intersection to Traffic	11/24/04*		<input type="checkbox"/>				
(Forecast) C0675: Oxnard Street: Open Intersection to Traffic	12/01/04*			<input type="checkbox"/>			
(Forecast) Potential MTA Board Action on Revised EIR	12/13/04*						
(Forecast) C0675: White Oak Avenue: Open Intersection to Traffic	12/14/04*			<input type="checkbox"/>			
(Forecast) C0675: Hazeltine Avenue: Open Intersection to Traffic	12/14/04*			<input type="checkbox"/>			
(Forecast) C0675: Corbin Avenue: Open Intersection to Traffic	12/21/04*			<input type="checkbox"/>			
(Forecast) C0675: Wilbur Avenue: Open Intersection to Traffic	12/27/04*			<input type="checkbox"/>			
(Forecast) C0675: Busway - Complete Construction Along Chandler Boulevard	1/17/05				<input type="checkbox"/>		
(Forecast) C0675: Van Nuys Boulevard: Open Intersection to Traffic	1/20/05*				<input type="checkbox"/>		
(Forecast) C0675: Burbank Blvd/Fulton Avenue: Open Intersection to Traffic	2/4/05*					<input type="checkbox"/>	
(Forecast) C0675: Tyrone Avenue: Open Intersection to Traffic	3/2/05*						<input type="checkbox"/>
(Forecast) C0675: Reseda Boulevard: Open Intersection to Traffic	3/2/05*						<input type="checkbox"/>
(Forecast) C0675: Chandler Boulevard North: Open Intersection to Traffic	3/24/05*						<input type="checkbox"/>
(Forecast) C0675: Cold Water Canyon Avenue: Open Intersection to Traffic	3/29/05*						<input type="checkbox"/>

 MTA Staff	 P.E. Design Consultants	 Metro	 MTA Board Action
 Other Agencies	 Contractor	 *	New Date



## SCHEDULE NARRATIVE

The C0675 Design/Build Contractor's remobilization effort has improved but the work force level has not reached the pre Court ordered work suspension level of August 3, 2004. A primary reason is there is a commercial contract dispute between the Contractor and its grading subcontractor that is preventing remobilization of forces to be completed. The contractor is evaluating alternatives to completing this work.

The Contractor submitted a schedule update this month that forecasts a cumulative six-month delay to the Contractor's Contract Substantial Completion Milestone date of June 16, 2005. Based on the forecast delay by the Contractor, the August 2005 Revenue Operations Date appears to be in jeopardy. MTA requested and received a recovery plan from the Contractor. The Board of Directors may elect to consider a recovery plan in December 2004, which would mitigate a portion or all of the delay to Contract Milestones.

The current schedule update is showing the fabrication and installation of station canopies, installation of communications equipment at stations and the testing of all systems as the most critical path. The completion of station furnishings and terrazzo flooring are on the secondary critical path. In addition, systems design remains near the secondary critical path. The Contractor is concentrating its work force on intersections, which are nowhere near the critical path. Busway and bikeway construction are not on the critical path but may become critical if there is any mitigation to the procurement of station canopies.

The Contractor substantially completed Colfax and DeSoto Avenue intersections to allow them to be opened to traffic. Rough grading and fine and base grading along the busway west of the L.A. River were completed. Work continues to progress well this period on station canopy foundations to the east of the alignment and installation of retaining walls under the I-405 freeway. Installation of the irrigation system started this period along Chandler Boulevard. The installation of sound wall pilasters continued on the west end of the Project alignment.

The C0675 Contractor continues to submit remaining design packages for City of Los Angeles and MTA review.

### PROJECT COST STATUS

PROJECT 800112 - METRO ORANGE LINE (BUSWAY)

\$ in Millions

COST SUMMARY

Description	Original Budget	Current Budget	Previous Forecast	Current Forecast	Forecast Variance	Commitments	Expenditures
Guideways	124.2	124.2	126.8	126.8	0.0	117.4	72.7
Yards & Shops	1.2	1.2	1.3	1.4	0.1	1.1	0.1
Systems/Equipment	12.7	12.7	10.2	10.1	(0.1)	8.7	2.0
Stations	30.4	30.4	32.1	32.1	0.0	30.5	6.6
Vehicles & Buses	17.5	17.5	15.7	15.7	0.0	15.4	0.0
Special Conditions	24.2	24.2	34.5	34.5	0.0	24.8	13.4
Right-of-Way	24.9	24.9	19.3	19.3	0.0	14.3	12.2
Professional Services	45.7	45.7	46.0	46.0	0.0	38.2	28.4
Proposed Park-and-Ride Facility	16.5	16.5	20.8	20.8	0.0	9.8	0.9
Contingency	32.2	32.2	22.8	22.8	0.0	0.0	0.0
Project Revenue	0.0	0.0	0.0	0.0	0.0	0.0	0.0
<b>TOTAL</b>	<b>329.5</b>	<b>329.5</b>	<b>329.5</b>	<b>329.5</b>	<b>0.0</b>	<b>260.2</b>	<b>136.3</b>

Expenditures are cumulative through September 2004.

Note: The Commitment value for the Stations and Special Conditions cost elements are higher than the Original Budget this period. The Original Budget requires an update to reflect changing project conditions at the element level. Staff is currently preparing a budget change recommendation to re-allocate costs to address work scope revisions. The Total Project Budget of \$329.5 million will remain unchanged.

### PROJECT COST ANALYSIS

The Original Budget of the Metro Orange Line, which includes an allowance of \$16.5 million for a proposed park-and-ride facility at the Western Terminus of the Orange Line, was adopted in February 2003 for a value of \$329.5 million.

**Current Budget:** The Current Budget remains unchanged this period.

**Current Forecast :** The Total Project forecast remains the same at \$329.5 million for the October period. The Project Contingency remains at \$22.8 million to cover unknown but anticipated changes.

**Commitments:** The commitments increased \$3.2 million primarily due to the following: \$2.8 million as a result of Design/Build Contract C0675 executed change orders; \$0.2 for a contract amendment to Environmental Services for additional stormwater mitigation, and \$0.2 million for Other Professional Services. The \$260.2 million in commitments to date represents 79% of the Current Budget.

**Expenditures:** Expenditures are cumulative through period ending September 2004. The expenditures increased \$6.2 million this period primarily due to the following: \$3.6 million for Design/Build Contract C0675, \$0.5 million for Environmental Mitigation; \$0.7 million for Real Estate settlements; \$0.6 million for Professional Services and Third Party Master Cooperative Agreements with the City of LA; and \$0.8 million for Agency costs. The \$136.3 million in expenditures to date represents 41.4% of the Current Budget.

## PROJECT COST STATUS

### PROJECT 800114 - METRO ORANGE LINE (BIKEWAY)

*\$ in Millions*

#### COST SUMMARY

Description	Original Budget	Current Budget	Previous Forecast	Current Forecast	Forecast Variance	Commitments	Expenditures
Guideways	5.8	8.0	7.9	7.9	0.0	6.2	1.1
Special Conditions	0.2	0.7	0.6	0.6	0.0	0.6	0.3
Professional Services	1.3	1.5	1.5	1.5	0.0	0.8	0.3
Contingency	0.8	0.4	0.6	0.6	0.0	0.0	0.0
<b>TOTAL</b>	<b>8.1</b>	<b>10.6</b>	<b>10.6</b>	<b>10.6</b>	<b>0.0</b>	<b>7.6</b>	<b>1.7</b>

Expenditures are cumulative through September 2004.

## PROJECT COST ANALYSIS

The same C0675 Design/Build Contractor as the Metro Orange Line Busway will construct the Metro Orange Line Bikeway Project. Construction activities for the Orange Line Bikeway are expected to occur concurrent with the construction effort of the Orange Line. The Orange Line Bikeway Project is segregated from the base scope of the Orange Line Project due to differences in funding sources.

The Original Budget of the Metro Orange Line Bikeway Project was adopted in February 2003 for a value of \$8.1 million.

#### **Current Budget**

The Current Budget of \$10.6 million was approved by the MTA Board in July 2004 to accommodate the bikeway enhancements and incorporates the usage of all grant funding available to the Project. The Current Budget remains unchanged this period.

#### **Current Forecast**

The Current Forecast remains the same this period.

#### **Commitments**

The commitments increased \$0.6 million primarily due to Design/Build Contract C0675 executed change orders. The \$7.6 million in commitments to date represents 71.8% of the Current Budget.

#### **Expenditures**

Expenditures are cumulative through period ending September 2004. The expenditures increased \$0.2 million this period due to Design/Build Contract C0675. The \$1.7 million in expenditures to date represents 15.6% of the Current Budget.

### PROJECT COST STATUS

PROJECT 800116 - METRO ORANGE LINE LADWP RECYCLED WATER PIPELINE

\$ in Millions

COST SUMMARY

Description	Current Estimate	Previous Forecast	Current Forecast	Forecast Variance	Commitments	Expenditures
Guideways	3.8	1.5	1.5	0.0	1.8	1.3
Special Conditions	0.0	0.0	0.0	0.0	0.0	0.0
Right-of-Way	0.0	0.0	0.0	0.0	0.0	0.0
Professional Services	1.2	1.4	1.4	0.0	0.9	0.9
Project Reimbursement	0.0	(2.9)	(2.9)	0.0	(2.2)	(2.2)
<b>TOTAL</b>	<b>5.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.0</b>	<b>0.5</b>	<b>0.0</b>

(1) Current Estimate based on September 2003 MTA Board action. Initial \$5.0 million authorized to further develop scope of work and life of project costs.

(2) Expenditures are cumulative through September 2004.

(3) Project reimbursement includes unpaid invoices totaling \$1,101,496. Actual reimbursement to date totals \$1,106,102.

### PROJECT COST ANALYSIS

On September 16, 2003, the Los Angeles Department of Water and Power (LADWP) Board of Commissioners approved a \$5.0 million budget under an existing Master Cooperative Agreement with MTA for partial funding for schedule-critical portions of the LADWP Recycled Water Pipeline. Subsequently, on September 25, 2003, the MTA Board approved a \$5.0 million initial budget for Project No. 800116 authorizing the issuance of Change Orders in the amount not to exceed \$2.5 million to the C0675 Design/Build Contractor for initial funding of design and construction of the LADWP Recycled Water Pipeline. Consequently, MTA and LADWP have determined that the LADWP Recycled Water Pipeline will not be incorporated into the Metro Orange Line Project and all work associated with the Recycled Water Pipeline should be terminated except for competing work related to incorporating a pipeline in the two bridges (Tujunga Wash and Bull Creek Bridges).

**Current Forecast**

The MTA Board adopted the project on the condition that LADWP reimburse MTA for all costs associated with design, construction and administration of the Recycled Water Pipeline Project. The MTA is proceeding with authorized scope and has billed for and received reimbursement from LADWP for the Pipeline scope of work.

The current forecast remained the same this period.

**Commitments**

The commitments increased \$0.3 million due to C0675 Design/Build executed change orders.

**Expenditures**

Expenditures are cumulative through period ending September 2004 and remained the same this period.

## FINANCIAL/GRANT STATUS

### Project 800112 – Metro Orange Line (Busway Only)

OCTOBER 2004		STATUS OF FUNDS BY SOURCE							
\$ in millions									
SOURCE	(A)	(B)	(C)	(D)	(D/B)	(E)	(E/B)	(F)	(F/B)
	ORIGINAL BUDGET	TOTAL FUNDS ANTICIPATED (1)	TOTAL FUNDS AVAILABLE	COMMITMENTS \$	%	EXPENDITURES \$	%	BILLED to \$	FUNDING SOURCE %
FEDERAL RSTP	17.5	17.5							
STATE TCRP	47.0	47.0	47.0	47.0	100%	32.1	68%	32.1	68%
STATE STIP	0.3	0.3	0.3	0.3	100%	0.3	100%	0.3	100%
PROPOSITION C	166.7	166.7	163.6	166.7	100%	77.3	46%	77.3	46%
PROP C (STIP REPLACEMENT)	98.0	98.0		46.2					
UNBILLED ACCRUALS						26.6			
<b>TOTAL</b>	<b>329.5</b>	<b>329.5</b>	<b>210.9</b>	<b>260.2</b>	<b>79.0%</b>	<b>136.3</b>	<b>41.4%</b>	<b>109.7</b>	<b>33%</b>

(1) Based on August 2003 Adopted Short Range Transportation Plan  
NOTE: Expenditures are cumulative through September 2004.

## STATUS OF FUNDS ANTICIPATED

**STATE TCRP:** Cumulative to date, \$47 million of State TCRP funds are available for draw down. The California Transportation Commission (CTC) allocated \$12.3 million in January 2001 and \$34.7 million in June 2002. At the October 2003 CTC Meeting, the CTC approved MTA's request for an AB 1335 (Letter of No Prejudice) for \$98 million of TCRP funds should they become available.

**STATE STIP:** Due to the suspension of the TCRP program, MTA processed a STIP amendment to secure \$98 million of STIP substitute funding to replace TCRP funds previously committed to the project. On April 3, 2003, the CTC approved the MTA's request for the STIP amendment.

**FINANCIAL/GRANT STATUS**

**Project 800114 – Metro Orange Line Bikeway Project**

OCTOBER 2004

STATUS OF FUNDS BY SOURCE

in \$ millions

SOURCE	(A)	(B)	(C)	(D)	(D/B)	(E)	(E/B)	(F)	(F/B)
	CURRENT BUDGET	TOTAL FUNDS ANTICIPATED	TOTAL FUNDS AVAILABLE	COMMITMENTS \$	%	EXPENDITURES \$	%	BILLED to \$	FUNDING SOURCE %
TEA (FED)	6.0	6.0	6.0	4.3	72%	1.3	21%	1.3	21%
TEA-21 (FED)	1.8	1.8	1.7	1.2	69%		0%		0%
STIP (FED)	0.5	0.5	0.5	0.4					
CITY OF LA	2.4	2.4	2.4	1.7	72%	0.3	12%	0.3	12%
UNBILLED ACTUALS						0.1			
<b>TOTAL</b>	<b>10.6</b>	<b>10.6</b>	<b>10.6</b>	<b>7.6</b>	<b>71.8%</b>	<b>1.7</b>	<b>15.6%</b>	<b>1.5</b>	<b>15%</b>

NOTE: Expenditures are cumulative through September 2004.

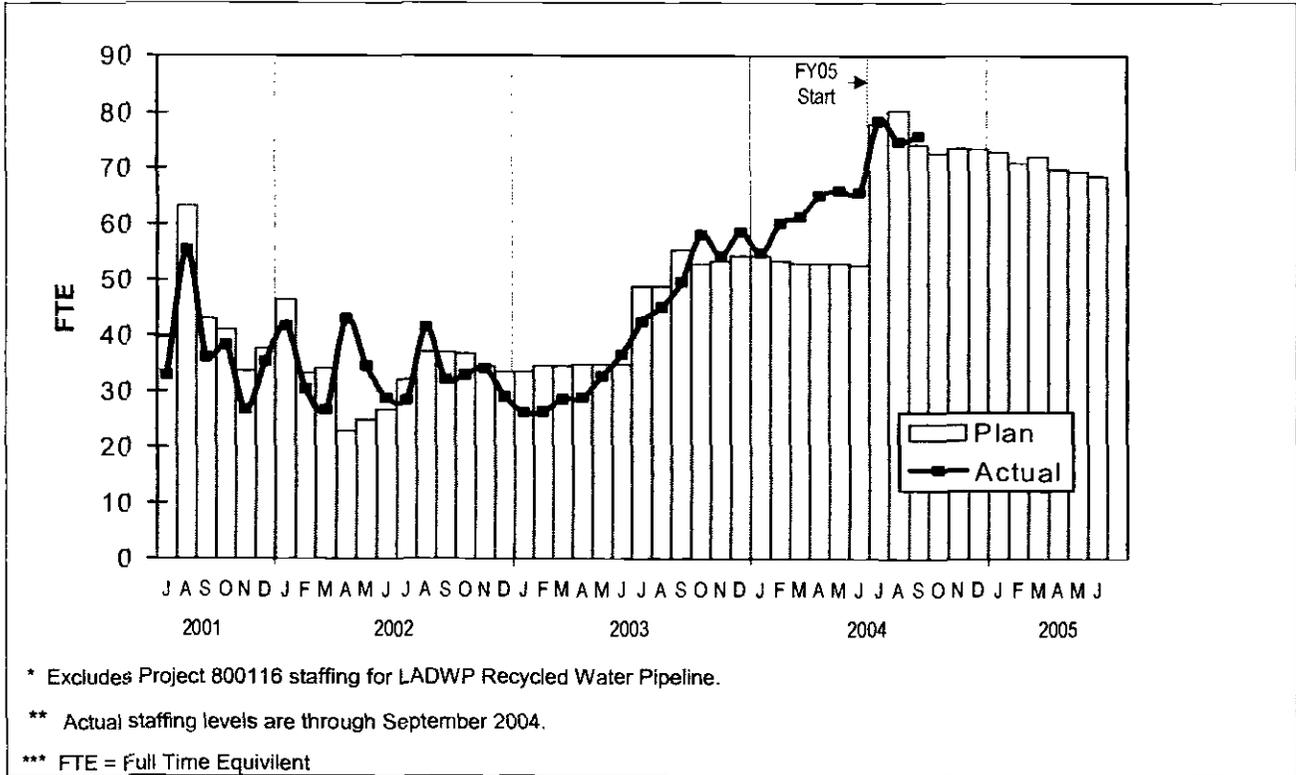
**STATUS OF FUNDS ANTICIPATED**

**FEDERAL FUNDS:** The transfer of Federal Funds from the Federal Highway Administration to the FTA has been completed. On May 29, 2003, MTA submitted grant application #CA-90-X970-03 to the FTA for a total amount of \$8,174,226. The FTA grant was executed on August 27, 2003 and is now available for drawdown.

**CITY OF LA:** The funding agreement for the local match between MTA and the City of Los Angeles was reviewed by the MTA and sent back to the City of Los Angeles for execution. The funding agreement was executed on April 24, 2003 and is now available for drawdown.

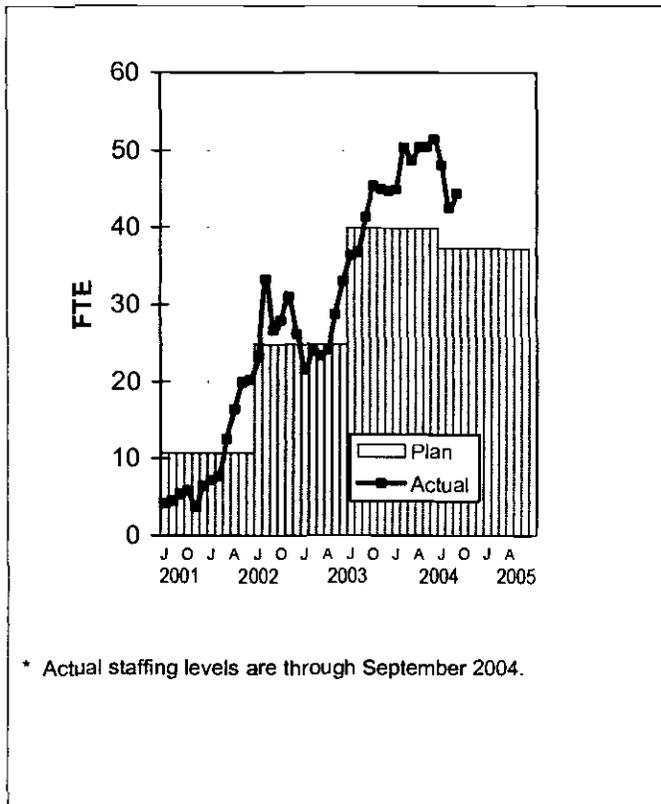
## STAFFING STATUS \*

### TOTAL STAFFING STATUS

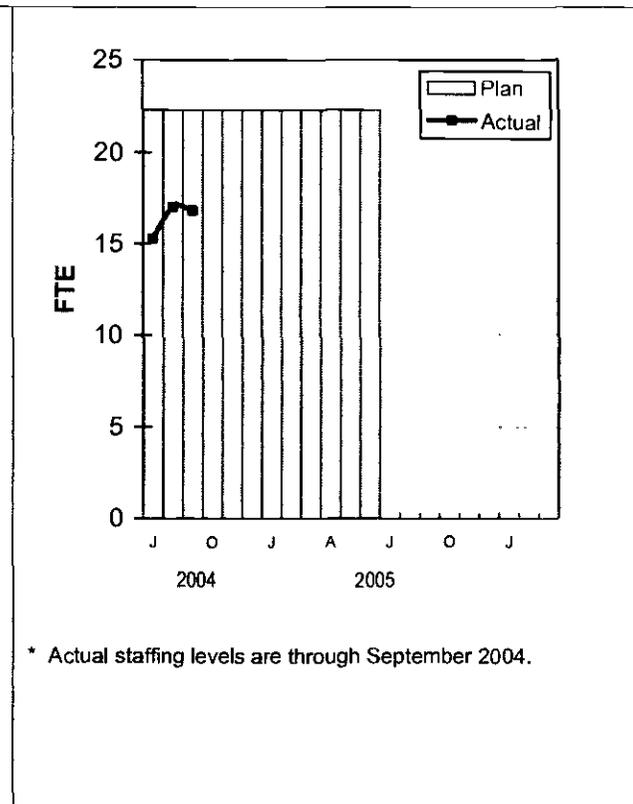


## STAFFING STATUS

### DIRECT AGENCY STAFFING



### INDIRECT AGENCY STAFFING



#### Direct Agency Staffing

For the first quarter of fiscal year 2005, agency FTE's have been exceeding the plan due to the following:

- Use of MTA Engineering to design portions of the Canoga Station Park and Ride with busway extension instead of using only Design consultant staffing.
- Real Estate Department support for acquisition of property for the Canoga Station Park and Ride.
- Environmental Department support for the assessment and handling of potential contaminated soils related to Canoga Station parcel acquisition.
- Planning Department support to address Court ordered revision to Project EIR.
- Increased workload for Engineering and Procurement Departments associated with design submittal review and ongoing changes.

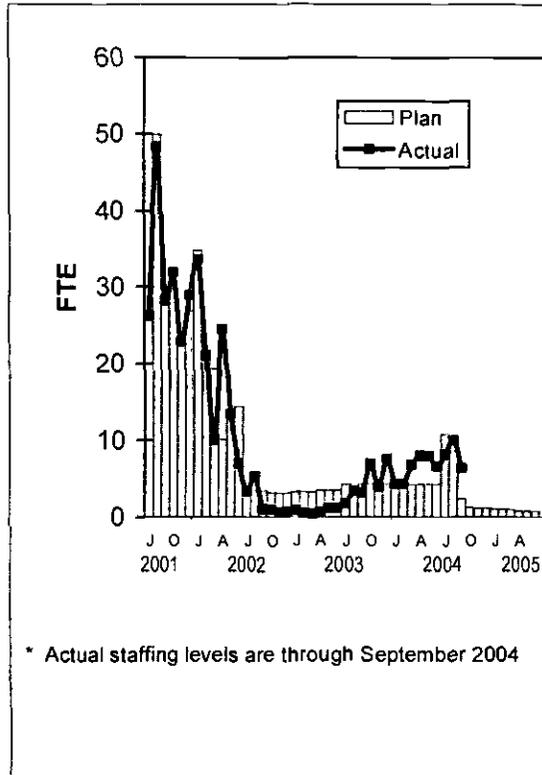
For the second quarter of FY05, the trend indicates a leveling of direct agency charges in the range between 41-44 FTE's. Reduced FTE's are expected due to the completion of the Canoga Station Park and Ride procurement package and the Planning Department completion of the revision to the project EIR by December 2004.

#### Indirect Agency Staffing

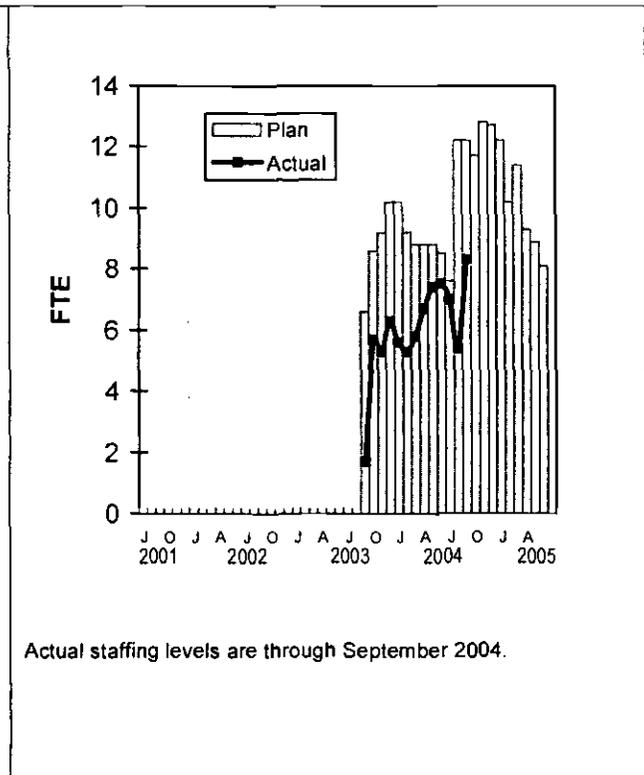
Beginning FY05, the major capital projects began tracking the planned FTE allocation for overhead as defined by the MTA Office of Management and Budget (OMB). There are a total of approximately 69 FTE's in overhead of which 33% or 22.7 FTE's are allocated to the Metro Orange Line Project.

## STAFFING STATUS

### DESIGN CONSULTANT



### CONSTRUCTION MANAGEMENT SUPPORT SERVICES CONSULTANT



### Design Consultant Staffing Analysis

July 2002 through December 2002 represents Bid Support Phase only. January 2003 through July 2005 represents Design Support During Construction.

### Construction Management Support Services Consultant Staffing Analysis

Contract NTP was issued September 15, 2003. Consultant staffing is lower than plan due to use of MTA Construction Division employees when available rather than mobilization of additional consultant forces.

### REAL ESTATE STATUS

- The number of leases to be terminated for the project is 101 with 99 available to the C0675 Design/Build Contractor. The following two parcels were scheduled to be available to the C0675 Contractor on July 1, 2003 and have been permitted to remain either partially or completely as noted below without impacting the Project (Parcels 1502 and 1503 still require a design review).
  1. Parcel 1502 Allegheny Properties (required for construction of Sepulveda Park-and-Ride). The lease on a portion of this parcel is being extended on a month-to-month basis pending design review at the tenant's request to retain a portion of the lease area.
  2. Parcel 1503 Chesapeake Properties (required for construction of Sepulveda Park-and-Ride). A small portion of the lease on the western edge of this parcel is being extended on a month-to-month basis pending review at the tenant's request to retain a portion of the lease area.
- Under New Acquisitions, nine parcels were originally required and certified as full takes. However, one parcel (Parcel 1813) was decertified as not required for the Project. All eight parcels have been acquired.

#### REAL ESTATE STATUS – LEASES

	Total Number	Received Courtesy Letters	Received Relocation Plan Letter	Recvd 90 Day Termination Notice	Unlawful Detainer Action	Relocation Completed	Available for Demolition	Available for Construction
Leases	101	101	101	101	25	58	97	99

#### REAL ESTATE STATUS – NEW ACQUISITIONS

Contract	No. of Parcels	Certified		Just Comp Approved		Offers Made*		Agreements Signed		Condemnation		Parcels Available		Parcels projected to be unavailable by need date
		Plan	Actual	Plan	Actual	Plan	Actual	Plan	Actual	Plan	Actual	Plan	Actual	
<b>TOTAL</b>	8	8	8	8	8	8	8	8	8	2	2	8	8	0

The parcels will be purchased by MTA Real Estate.

\* Offers made contingent to MTA Board approval.

## **ENVIRONMENTAL STATUS**

- Completed Sepulveda Park and Ride sampling.
- Completed Canoga Park and Ride agronomic sampling on MTA portion of the lot.
- Conducted monthly stormwater audit.
- Continued the removal, transport and disposal of additional heavy metals impacted soils at various locations along the right-of-way.
- Continued daily air monitoring and sampling where active soil disturbance occurred.
- Received SCAQMD Rule 403 Notice of Violation inquiry letter from Chief Prosecutor.
- Continued agronomic testing along the right-of-way for soil suitability for plant growth.

## **COMMUNITY RELATIONS STATUS**

- Notified residents, elected officials and the media about ongoing intersection, station and soundwall construction.
- Held a Landscape Advisory Committee Meeting and a community landscape exhibition on Chandler Blvd. planting.
- Addressed concerns and mitigated impacts to businesses caused by construction.

## **QUALITY ASSURANCE STATUS**

- Only one out of 27 Quality Action Requests (QAR) remains to be completed.
- QAR No. 8 regarding Washington Group International, is due to be completed on November 30, 2004. Report concerns Software Verification/Validation Plan for design deliverables.

### SAFETY STATUS

- Participated in weekly progress meetings with Construction Management and Contractor personnel to discuss safety related issues and construction schedule for Contract C0675 Design/Build project.
- Increased safety monitoring due to contractor's accelerated schedule and traffic control issues.
- Conducted monthly field audit. Corrections were made and audit was accepted.
- Participated in partnering meeting with Construction Management and SOJV personnel.
- SOJV reported No Recordable and No Lost Time Incidents. Project to Date (14) Recordable and (8) Lost Time Incidents.

### SAFETY STATISTICS

Contract Number	Contractor	Work Hours	Cases				Days						Incident Rates				
			Total Recordable Cases	Cases with days away from work, job transfer, or restriction			Days away from work			Days of job transfer or restriction			Total Days Lost	Total Recordable Cases	Cases with Days Away	Restricted or Transferred Only	Total Days Lost
				Total	Days Away	Restricted or Transferred Only	Current	Carry Over	Total	Current	Carry Over	Total					
<b>Project To Date</b>																	
C0675	SO, JV (Design)	120,835	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0	0.0	0.0
C0675	SO, JV (Build)	332,215	15	10	6	4	230	108	338	112	0	112	179	9.0	6.0	2.4	107.8
C0675	SO, JV (composite)	453,050	15	10	6	4	230	108	338	112	0	112	179	6.6	4.4	1.9	79.0
C0676	Brutoc	15,247	1	0	0	0	0	0	0	0	0	0	0	13.1	0.0	0.0	0.0
C0739		0	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0	0.0	0.0
	<b>Subtotals</b>	<b>468,297</b>	<b>16</b>	<b>10</b>	<b>6</b>	<b>4</b>	<b>230</b>	<b>108</b>	<b>338</b>	<b>112</b>	<b>0</b>	<b>112</b>	<b>179</b>	<b>6.8</b>	<b>4.3</b>	<b>1.7</b>	<b>76.4</b>
	MTA Const. Mgmt	49,118	0	0	0	0	0	0	0	0	0	0	0	0.0	0.0	0.0	0.0
	<b>Totals</b>	<b>517,415</b>	<b>16</b>	<b>10</b>	<b>6</b>	<b>4</b>	<b>230</b>	<b>108</b>	<b>338</b>	<b>112</b>	<b>0</b>	<b>112</b>	<b>179</b>	<b>6.2</b>	<b>3.9</b>	<b>1.5</b>	<b>69.2</b>

### ART DEVELOPMENT STATUS

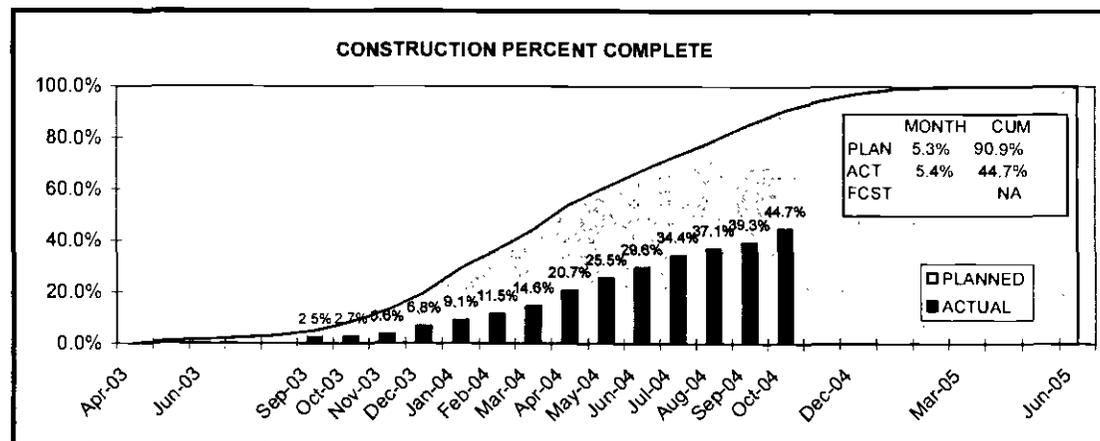
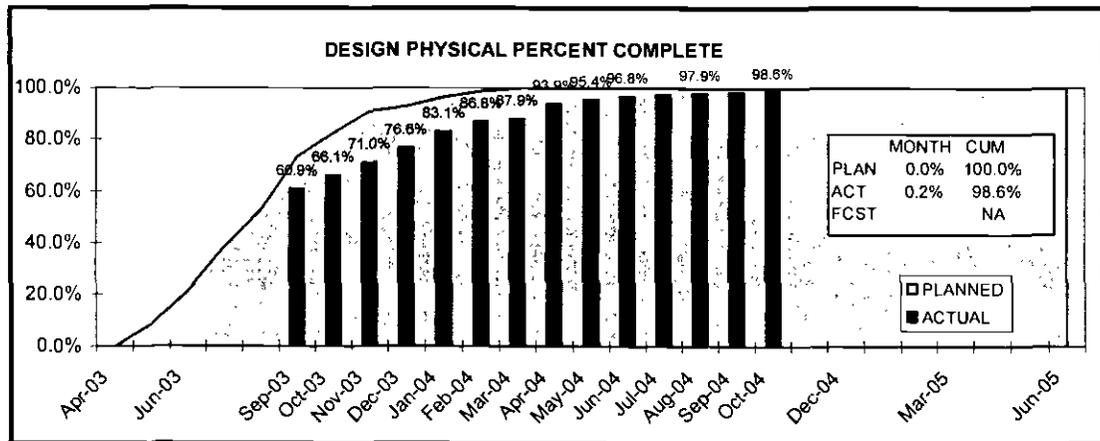
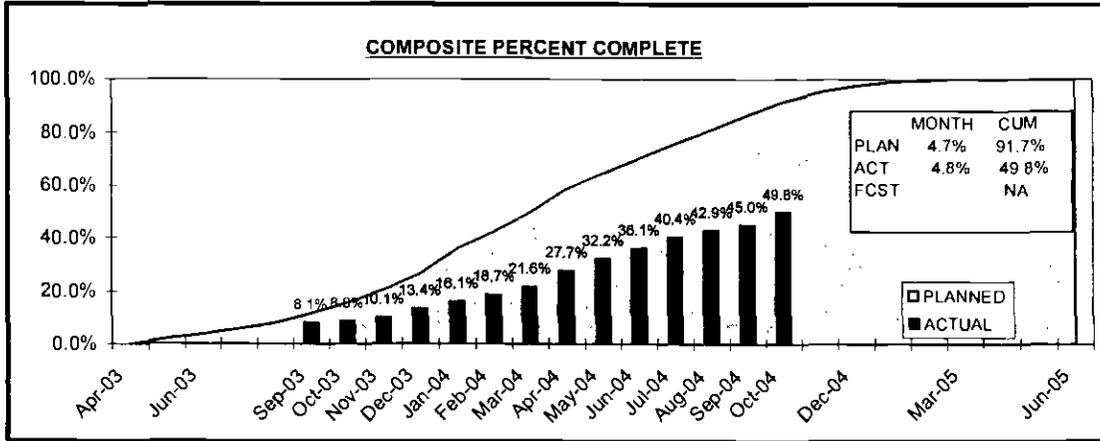
- Completed review of the Orange Line AFC Landscape Drawings and approved planting plans for Art Alignment Tree.
- Reviewed Alignment Landscape Art Plans at the Landscape Committee Meeting.
- Presented design materials and concepts for Metro Orange Line artists at the Chandler Blvd. Community Meeting.

## CONTRACT CONSTRUCTION STATUS

<p><b>Description: Design/Build</b> <b>Contractor: Shimmick Construction Co., Inc./</b> <b style="text-align: center;">Obayashi Corp, A Joint Venture</b></p>	<p><b>Contract No.: C0675</b></p> <p><b>Status as of: October 31, 2004</b></p>																																				
<p><b>Progress/Work Completed:</b></p> <ul style="list-style-type: none"> <li>· Submitted Landscape design - AFC submittal.</li> <li>· Submitted Intersection Group 7 design - 100% submittal.</li> <li>· Completed review Bikeway segment 3 design - 100% submittal.</li> <li>· Colfax Avenue and DeSoto Avenue were opened to traffic.</li> <li>· Completed rough grading along Busway, west of the L. A. River.</li> <li>· Completed intersection phase 1 work at Hazeltine Avenue and at Oxnard Street.</li> <li>· Completed fine and base grading work west of the L.A. River.</li> </ul> <p><b>Areas of Concern:</b></p> <ul style="list-style-type: none"> <li>· The Contractor's October 2004 schedule submittal shows all Milestones behind schedule.</li> <li>· Landscaping - Due to the unanticipated arsenic found within the MTA right of way, plant survival may not meet Contractual requirements. The MTA has determined that additional arsenic and herbicide testing is necessary to identify those areas where soil additive amendments may be necessary to assure plant survivability. The MTA has issued a change order to the Contractor for implementation of a testing and soil amendment program.</li> <li>· Suspension of Work - On August 3, 2004 the MTA issued to the Contractor an order of suspension. This originated from a California Court of Appeal directive to stay from carrying out the Orange Line on behalf of the Appellant group Citizens Organized for Smart Transit. The order of suspension was rescinded on August 26, 2004. The MTA and the Contractor are assessing schedule impacts from a gradual build up of resources from the Contractor and its Subcontractors. Mitigation measures will be required to maintain the Orange Line Revenue Operation Date.</li> </ul> <p><b>Non-Compliance Report Status</b></p> <ul style="list-style-type: none"> <li>· Contractor has received 49 notices of non-compliance from the City of Los Angeles.</li> <li>· The City has forwarded resolution of 17 notices to the Contractor.</li> <li>· The other 32 notices are still open.</li> </ul>	<p><b>Major Activities (In Progress):</b></p> <ul style="list-style-type: none"> <li>· Submittal Reviews are ongoing. Remaining submittals include:                             <ol style="list-style-type: none"> <li>1. Communications design - 100% submittal.</li> <li>2. Intersection Group 7 design - 100% submittal.</li> <li>3. Landscape design - AFC submittal.</li> </ol> </li> <li>· Continue retaining walls under the I-405 freeway.</li> <li>· Continue with Intersection Groups 3 and 4 construction.</li> <li>· Continued installation of soundwall pilasters on the west end of the alignment</li> <li>· Continue Station canopy foundation work on the east end of the alignment.</li> <li>· Continue fabrication of communication equipment and steel canopies for Stations.</li> <li>· Continue installation of irrigation system along Chandler Boulevard.</li> </ul> <p><b>Major Activities Next Period:</b></p> <ul style="list-style-type: none"> <li>· Continue irrigation installation along Chandler Blvd.</li> <li>· Start installation of 60 in. drainage pipe west of Hazeltine Ave.</li> <li>· Complete work at the following intersections: Wilbur, Tampa, White Oak and Ethel.</li> <li>· Continue curb/gutter and asphalt paving along Busway Segment 2.</li> <li>· Complete retaining wall construction under the I-405 freeway.</li> <li>· Continue soundwall installation on the west end of the alignment.</li> <li>· Complete asphalt paving along the Chandler Blvd.</li> <li>· Continue Station Work on the east end of the alignment.</li> <li>· Continue Park &amp; Ride rough and fine grading work.</li> <li>· Start rough grading work along the Bikeway at the west end of the alignment.</li> <li>· Submit Intersection Group 7 design - AFC submittal.</li> <li>· Submit Division 8 Crane/Catwalk design - AFC submittal.</li> <li>· Submit Communication design - AFC submittal.</li> <li>· Submit Bikeway Segment 3 design - AFC submittal.</li> </ul> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th></th> <th style="text-align: center;">Original Contract Dates</th> <th style="text-align: center;">Time Extension</th> <th style="text-align: center;">Current Contract</th> <th style="text-align: center;">Forecast</th> <th style="text-align: center;">Variance CD's</th> </tr> </thead> <tbody> <tr> <td>Milestone 1 -Available for UFS Installation</td> <td style="text-align: center;">01/01/05</td> <td style="text-align: center;">0</td> <td style="text-align: center;">01/01/05</td> <td style="text-align: center;">06/13/05</td> <td style="text-align: center;">-163</td> </tr> <tr> <td>Milestone 2-MTA Division 8 Work Complete</td> <td style="text-align: center;">02/15/05</td> <td style="text-align: center;">0</td> <td style="text-align: center;">02/15/05</td> <td style="text-align: center;">10/26/05</td> <td style="text-align: center;">-253</td> </tr> <tr> <td>Milestone 3-Systems Infrastructure &amp; Equip Installed/Tested</td> <td style="text-align: center;">05/12/05</td> <td style="text-align: center;">0</td> <td style="text-align: center;">05/12/05</td> <td style="text-align: center;">12/16/05</td> <td style="text-align: center;">-218</td> </tr> <tr> <td>Milestone 4-Contract Substantially Complete</td> <td style="text-align: center;">06/16/05</td> <td style="text-align: center;">0</td> <td style="text-align: center;">06/16/05</td> <td style="text-align: center;">12/16/05</td> <td style="text-align: center;">-183</td> </tr> <tr> <td>Milestone 5-Reliability Demonstration Testing Period</td> <td style="text-align: center;">ROD + 365CD's</td> <td style="text-align: center;">0</td> <td style="text-align: center;">ROD + 365CD's</td> <td style="text-align: center;">ROD + 365CD's</td> <td style="text-align: center;">0</td> </tr> </tbody> </table>		Original Contract Dates	Time Extension	Current Contract	Forecast	Variance CD's	Milestone 1 -Available for UFS Installation	01/01/05	0	01/01/05	06/13/05	-163	Milestone 2-MTA Division 8 Work Complete	02/15/05	0	02/15/05	10/26/05	-253	Milestone 3-Systems Infrastructure & Equip Installed/Tested	05/12/05	0	05/12/05	12/16/05	-218	Milestone 4-Contract Substantially Complete	06/16/05	0	06/16/05	12/16/05	-183	Milestone 5-Reliability Demonstration Testing Period	ROD + 365CD's	0	ROD + 365CD's	ROD + 365CD's	0
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<p><b>Schedule Summary:</b></p> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 60%;">Date of Award:</td> <td style="text-align: right;">04/03/03</td> </tr> <tr> <td>Notice to Proceed:</td> <td style="text-align: right;">05/02/03</td> </tr> <tr> <td>Original Contract Duration:</td> <td style="text-align: right;">776</td> </tr> <tr> <td>Current Contract Duration:</td> <td style="text-align: right;">776</td> </tr> <tr> <td>Elapsed Time from NTP:</td> <td style="text-align: right;">548</td> </tr> </table>	Date of Award:	04/03/03	Notice to Proceed:	05/02/03	Original Contract Duration:	776	Current Contract Duration:	776	Elapsed Time from NTP:	548	<p><b>Cost Summary:</b> <span style="float: right;"><b>\$ In millions</b></span></p> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 70%;">1. Award Value: *</td> <td style="text-align: right;">150.72</td> </tr> <tr> <td>2. Executed Modifications:</td> <td style="text-align: right;">4.00</td> </tr> <tr> <td>3. Approved Change Orders (NTE):</td> <td style="text-align: right;">5.56</td> </tr> <tr> <td>4. Current Contract Value (1 + 2 + 3):</td> <td style="text-align: right;">160.28</td> </tr> <tr> <td>5. Pending Change Orders (NTE):</td> <td style="text-align: right;">0.80</td> </tr> <tr> <td>6. Incurred Cost:</td> <td style="text-align: right;">91.80</td> </tr> </table>	1. Award Value: *	150.72	2. Executed Modifications:	4.00	3. Approved Change Orders (NTE):	5.56	4. Current Contract Value (1 + 2 + 3):	160.28	5. Pending Change Orders (NTE):	0.80	6. Incurred Cost:	91.80														
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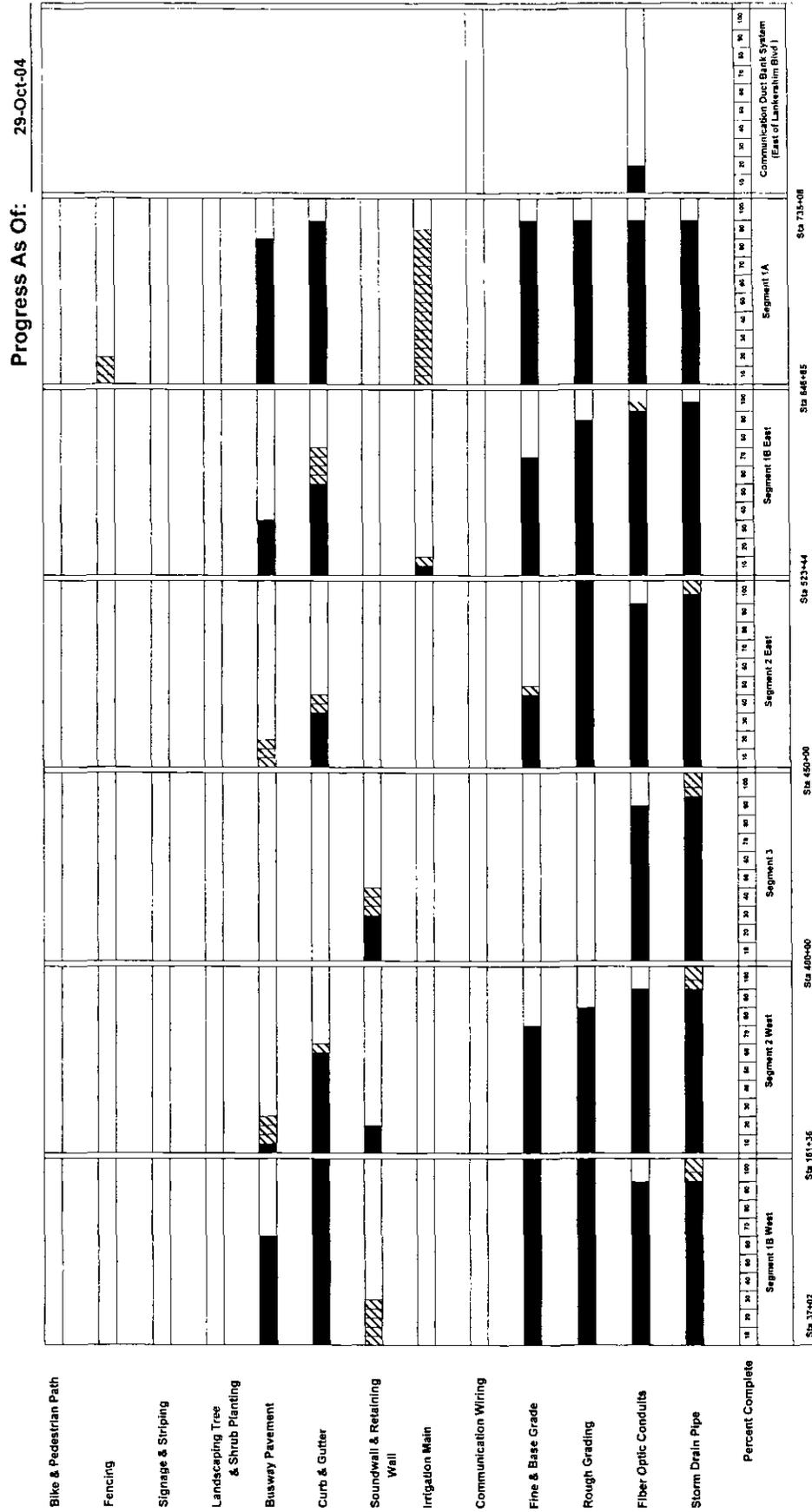
\* Includes Options E.2, E.3, E.4, E.5, E.6 (exercised after award) and E.8

## CONTRACT C0675 PHYSICAL PERCENT COMPLETE



\* Plan is based on approved Contract C0675 Baseline Schedule

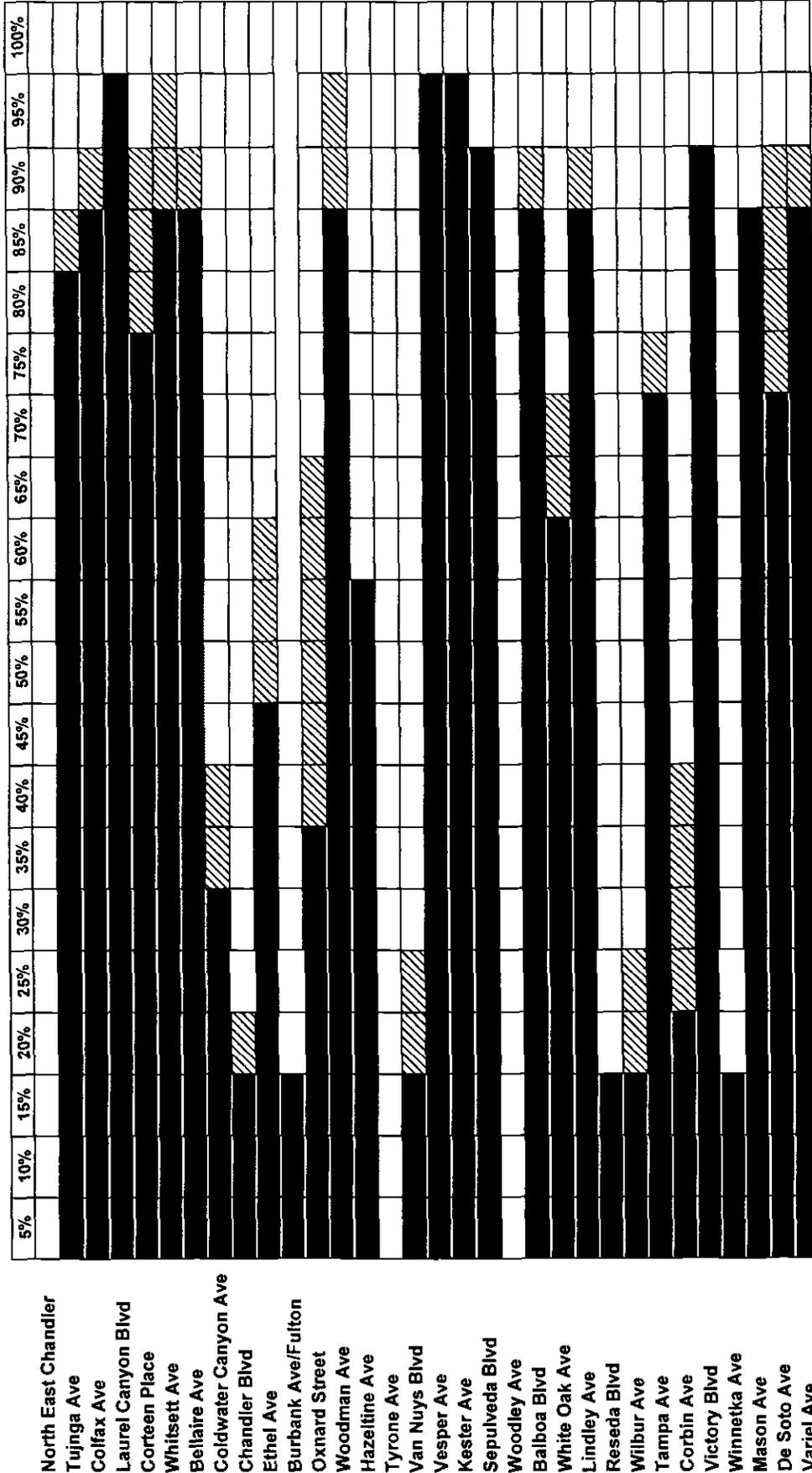
Metro Orange Line Project - Busway Segments  
 Station 37+08 to Station 735+08/Communication Duct Bank System  
 Summary - Busway Segment Percent Complete



Actual This Period  
 Cumulative Prior Period  
 NOTE: STATIONING NOT TO SCALE

Metro Orange Line Project - Intersections  
Summary - Percent Complete

Progress as of: 29-Oct-2004



Actual This Period  
 Cumulative Prior Period

## CHRONOLOGY OF EVENTS

May 15, 2001	The MTA released the draft environmental study of a proposed 14-mile Bus Rapid Transit (BRT).
July 26, 2001	The MTA Board of Directors adopted the 14-mile Bus Rapidway system, also called a "busway," as the locally preferred alternative for the San Fernando Valley Metro Rapidway Corridor.
February 14, 2002	The MTA issued the final environmental report for the 14-mile Bus Rapid Transit.
February 28, 2002	The MTA Board voted to certify the final environmental report for the Bus Rapid Transit system and approved a solicitation for a Design/Build delivery system for the Project. The action paved the way for the project's final design phase.
April 2, 2002	Citizens Organized for Smart Transit (COST), an unincorporated association located in Los Angeles, filed a lawsuit against the MTA in which COST asserted numerous challenges to the adequacy of the EIR under CEQA and sought an order from the Superior Court setting the Project aside.
June 28, 2002	The MTA completed the Preliminary Engineering and Design Development efforts. The Design/Build Invitation for Bid package for Contract No. C0675 was assembled and advertised.
July 12, 2002	The MTA issued Addendum No. 1 for Contract No. C0675.
July 19-22, 2002	The MTA conducted job walks for potential bidders providing the opportunity to view current project conditions.
July 25, 2002	The MTA issued Addendum No. 2 for Contract No. C0675.
August 23, 2002	The MTA completed final design of the Los Angeles River Bridge. The final design was completed to mitigate possible construction and schedule risks associated with a limited dry season construction restriction within the river channel.
August 28, 2002	San Fernando Valley Metro Rapidway Project held its first Community Transit meeting. The meeting, held in a relatively informal style, included planners, schedule makers, and schedule checkers from the sector office. The meeting's format included plenty of time for attendees to speak to the planners and schedulers about specific issues with specific lines or stops.
August 29, 2002	The MTA issued Addendum No. 3 for Contract No. C0675. This included the option for constructing the Los Angeles River Bridge.
August 29, 2002	Contract No. EN069, CH2M Hill Constructors, Inc., started demolition of the Los Angeles River Bridge.
September 9, 2002	Two bidders submitted technical bids, first step of the two-step bid process for Contract C0675.
September 11, 2002	The MTA advertised Contract No. C0676 Los Angeles River Bridge.
September 19, 2002	Substantial completion of the Los Angeles River Bridge demolition.

## CHRONOLOGY OF EVENTS

October 31, 2002	The MTA issued Notice of Technical Acceptance to two Contract No. C0675 bidders, Shimmick-Obayashi, a Joint Venture and Granite-Brutoco, a Joint Venture.
November 2002	Addendums No. 4, 5, and 6 were issued for Contract No. C0675 to clarify issues identified during the technical evaluation phase.
December 2, 2002	Received price bids from the two contractors for Contract No. C0675 Design/Build.
December 5, 2002	MTA received a single bid for Contract No. C0676 Los Angeles River Bridge.
December 5, 2002	MTA opened two price bids for Contract No. C0675 Design/Build for design and construction.
December 17, 2002	Notice of Intent to Award Contract No. C0675 sent to both contractors.
December 17, 2002	The California Transportation Commission (CTC) issued letter deferring "Traffic Congestion Relief Program" (TCRP) funds until its meeting on February 27, 2003. This resulted in the suspension of Contract No. C0675 contract award pending further notification of funding status from the CTC.
December 20, 2002	A trial was held to hear the lawsuit brought by the Citizens Organized for Smart Transit (COST) opposed to the Project. The judge ruled in favor of MTA.
December 23, 2002	Los Angeles Department of Water and Power (LADWP) agrees to reimburse MTA for actual costs incurred to review and update Los Angeles River Bridge design to incorporate a future reclaimed waterline.
December 27, 2002	A Notice to Award was issued to Brutoco Engineering and Construction Corporation for Contract No. C0676 Los Angeles River Bridge.
January 14, 2003	The Superior Court denied COST's petition for a writ of mandate and granted judgment for MTA. COST filed an appeal of the Superior Court's decision. In addition, COST filed a petition with the Court of Appeal, seeking a temporary stay of the Project.
January 17, 2003	MTA held groundbreaking ceremony for Contract No. C0676.
January 17, 2003	CTC held workshop to discuss funding issues.
January 27, 2003	The Court of Appeal summarily denied the petition from COST seeking a temporary stay of the Project, allowing construction to proceed pending the appeal.
January 28, 2003	Notice to Proceed was issued to Brutoco Engineering and Construction Corporation for construction of the new Los Angeles River Bridge. The decision to award a separate contract instead of exercising option in Contract No. C0675 Design/Build allowed critical work to commence during the first dry construction period starting April 15, 2003.
February 27, 2003	CTC put the project funding issue on the April 3, 2003 CTC meeting.

## CHRONOLOGY OF EVENTS

February 27, 2003	The MTA Board adopted the Project Budget and Schedule. The Project Revenue Operations Date calculated as full Notice to Proceed for Contract No. C0675 Design/Build plus 27 months contingent upon resolution of CTC funding issues. A budget of \$329.5 million was adopted for the busway and \$8.1 million was adopted for the bikeway.
April 3, 2003	CTC approved funding plan for the San Fernando Valley Metro Rapidway Project.
April 3, 2003	MTA awarded Contract No. C0675 to Shimmick Construction Co./Obayashi Corporation, a Joint Venture for the design and construction of the San Fernando Valley Metro Rapidway. Total value of the Contract was \$150.4 million.
April 24, 2003	MTA Board adopted San Fernando Valley Metro Rapidway Project as the official name of the Project.
May 2, 2003	Notice to Proceed (NTP) was issued to Shimmick Construction Co., Inc./Obayashi Corporation, a Joint Venture for Contract No. C0675.
May 20, 2003	The LADWP Board of Commissioners certified the Mitigated Negative Declarations for the West Valley Water Recycling Project (known as the Recycled Water Pipeline Project).
July 9, 2003	MTA and Contractor staff complete move into an Integrated Project Management Office for Contract No. C0675.
September 15, 2003	NTP was issued to Carter & Burgess for Contract MC067 Construction Management Services.
September 16, 2003	The LADWP Board of Commissioners approved a \$5.0 million budget under an existing Master Cooperative Agreement with MTA for partial funding for schedule-critical portions of the LADWP Recycled Water Pipeline Project.
September 25, 2003	The MTA Board approved a \$5,000,000 initial budget and authorized the issuance of change orders in the amount not to exceed \$2,500,000 to the C0675 Contractor for initial funding of design and construction of the LADWP Recycled Water Pipeline.
October 15, 2003	C0676 Contractor completed on time all in-channel work required to meet contract milestone date planned for October 15, 2003.
November 6, 2003	C0675 Contractor began first excavation and installation of drainage pipe at the east end of the Rapidway on Chandler Boulevard between Laurel Canyon and Coldwater Canyon.
November 17, 2003/ November 20, 2003	Held two of four planned community meetings in the San Fernando Valley to collect feedback on proposed landscape plans for the Rapidway.
November 21, 2003	First temporary lane closure at Laurel Canyon Intersection for installation of drainage pipe across intersection.
December 1, 2003	Successful completion of Contract C0676 Los Angeles River Bridge, Brutoco Engineering and Construction Corporation, two weeks ahead of schedule and under budget.

## CHRONOLOGY OF EVENTS

December 3, 2003/ December 6, 2003	Held remaining two of four planned community meetings in the San Fernando Valley to collect feedback on proposed landscape plans for the Rapidway.
January 14, 2004	LADWP advised MTA to not proceed with the Recycled Water Pipeline Project and finish current authorized scope of work.
January 22, 2004	MTA Board of Directors approved changing name of project to Metro Orange Line.
January 22, 2004	Suspension of work issued to C0675 Design/Build Contractor at westside of busway between De Soto Avenue and Corbin Avenue due to presence of contaminated soil.
January 29, 2004	Suspension of work issued for C0675 Design/Build Contractor at east end of busway east of Colfax/Chandler intersection due to presence of contaminated soil.
February 4, 2004	Removed suspension of work issued to C0675 Design/Build Contractor at westside of busway between De Soto Avenue and Corbin Avenue due to presence of contaminated soil.
February 11, 2004	Removed suspension of work issued to C0675 Design/Build Contractor at east end of busway east of Colfax/Chandler intersection due to presence of contaminated soil.
February 11, 2004/ February 16, 2004	First weekend full street closure at Balboa Blvd./Victory Blvd. for construction of new intersection including demolition, paving, signage and striping.
March 24, 2004	First asphalt busway paving on Chandler Boulevard between Colfax and Laurel Canyon Boulevard (Segment 1A).
March 22, 2004	LADWP issued notice to cease and desist construction activities at west end of Project due to C0675 Design/Build Contractor striking and damaging an LADWP underground 230,000 volt line.
April 2, 2004	LADWP lifts cease and desist notice issued March 22, 2004 with conditions.
May 27, 2004	MTA Board approved property acquisitions for new Warner Center Park and Ride facility pending resolution of environmental issues.
July 19, 2004	The California Court of Appeal reversed the December 2002 decision of the Los Angeles Superior Court, which had rejected a challenge to the Environmental Impact Report (EIR) for the Orange Line. The Court of Appeal rejected each of the grounds except one. The court found that the MTA should have studied the possibility of multiple east-west Rapid Bus lines in the San Fernando Valley as an alternative to the Orange Line, and the failure to do so renders the EIR invalid. The Court of Appeal decision did not enjoin further construction on the Project.
July 20, 2004	Start of first station construction at Laurel Canyon Station with C0675 Design/Build Contractor beginning drilling and placement of CIDH piles.
July 22, 2004	The MTA Board of Directors approved an increase in the Current Budget for the Bikeway portion (Project 800114) of the Orange Line, from \$8.1 million to \$10.6 million.
July 30, 2004	Superior Court denies COST request for Stay of Project citing lack of jurisdiction. COST states it will go to California Court of Appeal.

## CHRONOLOGY OF EVENTS

July 30, 2004	MTA filed a petition for rehearing of July 19, 2004 action with California Court of Appeal.
August 2, 2004	California Court of Appeal issues a temporary stay halting construction of the Orange Line Project.
August 3, 2004	MTA issued a suspension of work to the C0675 Design/Build Contractor due to the temporary stay of the Project by the California Court of Appeal.
August 19, 2004	The temporary stay issued by the California Court of Appeal on August 2, 2004 expired. The California Court of Appeal denied MTA's request for rehearing on the validity of the EIR.
August 26, 2004	The suspension of work to C0675 Design/Build Contractor was lifted and work resumed.
August 26, 2004	MTA filed an appeal to the California Supreme Court regarding validity of the Project's EIR.
September 23, 2004	California Supreme Court issued an order denying the Petition of the MTA that the Supreme Court review the decision of the Court of Appeal which had invalidated the Environmental Impact Report. In the same order the Supreme Court also denied the request of the plaintiff, COST, that construction on the Project be halted.
September 28, 2004	Superior Court denies a request by COST for a temporary restraining order to again shut down construction. Schedules hearing for October 22, 2004.
September 28, 2004	Superior Court denies a request by COST for a temporary restraining order to again shut down construction. Schedules hearing for October 22, 2004.
October 22, 2004	Superior Court issued a writ to MTA as directed by the Court of Appeal decision regarding the EIR.
October 23, 2004	MTA released the draft Revised EIR for a 30-day public review and comment period. The revised EIR studies three multiple Rapid Bus route alternatives.
October 28, 2004	MTA Board adopted resolution to vacate the February 28, 2002 certification of the earlier EIR and directed staff to return on December 13, 2004 with Revised Final EIR, public comments, responses to public comments and a recommendation to select a Preferred Alternative.
October 29, 2004	COST requested the Court of Appeals to shut down construction, which was denied.

## PROJECT PHOTOS



Low floor CNG-powered articulated vehicle manufactured by North American Bus Industries (NABI) at NABI's facility in Anniston, Alabama.

## PROJECT PHOTOS



Forming Platform of Laurel Canyon Station.



Sound wall north of Victory Blvd.

## PROJECT PHOTOS



Caballero Creek bottom invert placement.



Fabrication of first sound wall panel.

### PROJECT PHOTOS



Working on retaining wall at I-405 freeway.



Installation of the cast in drilled hole piling system at Laurel Canyon Station.

## PROJECT PHOTOS



SOJV subcontractor Romero started rough grading,  
Phase 3 of intersection work at De Soto Avenue.

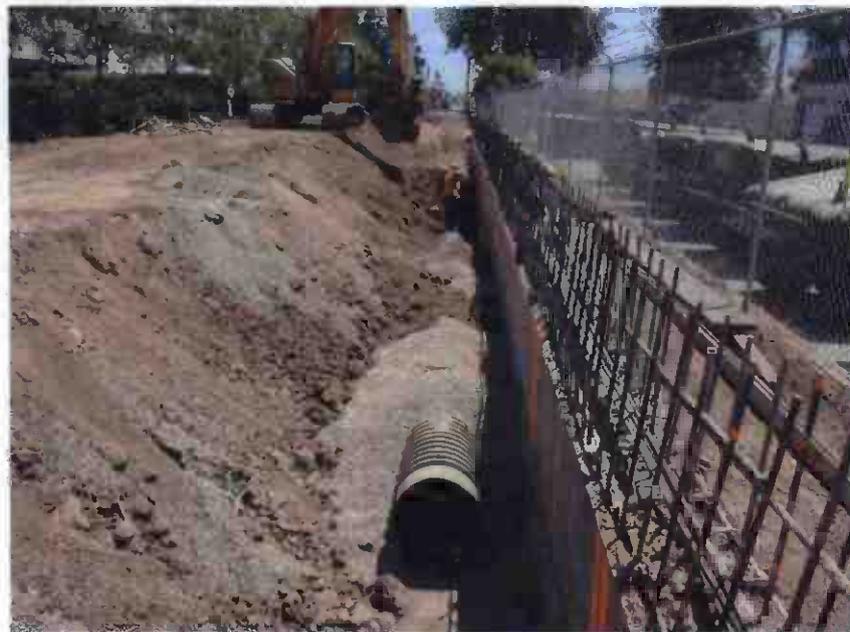


SOJV subcontractor Rainbow continuing installation of  
storm drain culvert near Mason Avenue.

## PROJECT PHOTOS



SOJV subcontractor Western Paving continuing paving near western end of Project.



SOJV subcontractor Rainbow installing storm drain pipe near Balboa Blvd.

## PROJECT PHOTOS



SOJV subcontractor Moore Electric continues installing traffic/light poles at Whitsett Avenue.



First course of asphalt looking west toward De Soto Avenue.

## PROJECT PHOTOS



Placement of crushed miscellaneous base material.



Paving operation along Chandler Boulevard.

## PROJECT PHOTOS



Paving operation along Chandler Boulevard.



Contract C0676 Los Angeles River Bridge Contractor completed bridge (December 2003).

## APPENDIX

### COST AND BUDGET TERMINOLOGY

#### Cost Descriptions

**ORIGINAL BUDGET** The Original Project Budget as established by Metropolitan Transportation Authority (MTA) Board of Directors at the time it authorizes Construction Project Management Division to commence full design and construction of the project (Project Adoption).

**CURRENT BUDGET** The Original Budget plus all budget amendments approved by formal MTA action. Also referred to as Approved Budget.

**COMMITMENTS** The total of actual contract awards, executed change orders or amendments, approved work orders of Master Cooperative Agreements, offers accepted for purchase of real estate, and other MTA actions which have been spent or result in the obligation of specific expenditures at a future time.

**INCURRED COST** The total value of work performed to date of services received, and acquired materials or properties.

**EXPENDITURES** The total dollar amount of funds expended by MTA for contractor or consultant invoices, third party invoices, staff salaries, real estate and other expenses that is reported in MTA's Financial Information System (FIS).

**CURRENT FORECAST** The best estimate of the final cost of the project when all checks have been issued and the project is closed out. Current Forecast is composed of actual costs incurred to date, the best estimate of work remaining, and a current risk assessment for each budgeted cost item.

#### Cost Element Descriptions

**CONSTRUCTION** Includes construction and procurement contracts. Costs associated with Guideways, Yards and Shops, Systems/Equipment, Stations and Buses.

**SPECIAL CONDITIONS** Includes work by outside agencies and utilities in design coordination, review, and relocation of utilities through Master Cooperative Agreements, environmental mitigation and compliance, insurance programs, safety program, art program, testing, start-up, and pre-revenue operations.

**RIGHT-OF-WAY** Includes purchase cost of parcels, easements, right-of-entry permits, escrow fees, contracted real estate appraisals and tenant relocation.

**PROFESSIONAL SERVICES** Includes design engineering, project management assistance, construction management support services, legal counsel, agency staff costs, and other specialty consultants.

**PROPOSED PARK-AND-RIDE FACILITY** Proposed park-and-ride facility at the Western Terminus of the Orange Line.

**CONTINGENCY** A fund established at the beginning of a project to provide for anticipated but unknown additional costs that may arise during the course of the project.

**PROJECT REVENUE** Includes all revenue receivable to the MTA as a direct result of project activities. This includes cost sharing of construction items, insurance premium rebates, and the like.

## APPENDIX

### LIST OF ACRONYMS

AFE	Authorization For Expenditure
BRT	Bus Rapid Transit (No longer valid see MRT instead)
CADD	Computer Aided Drafting and Design
CALTRANS	California Department of Transportation
CD	Calendar Day
CDFG	California Department of Fish and Game
CM	Construction Manager
CMAC	Congestion Mitigation Air Quality
CN	Change Notice
CO	Change Order
COE	Corps of Engineers
CPM	Critical Path Method
CPUC	California Public Utilities Code
CR	Camera Ready
CTC	California Transportation Commission
CUD	Contract Unit Description
D/B	Design/Build
D/B/B	Design/Bid/Build
DD	Design Development
DOT	Department of Transportation
DTSC	Department of Toxic Substances Control
DWP	Department of Water and Power
EIR	Environmental Impact Report
EIS	Environmental Impact Statement
EPBM	Earth Pressure Balance Machine
FAR	Federal Acquisition Regulation
FD	Final Design
FEIR	Final Environmental Impact Report
FIS	Financial Information System
FTE	Full Time Equivalent
GDSR	Geotechnical Design Summary Report
IFB	Invitation for Bid
IPO	Integrated Project Office
JV	Joint Venture
LA	Los Angeles
LABOE	Los Angeles Bureau of Engineering
LACFCD	Los Angeles County Flood Control District
LACMTA	Los Angeles County Metropolitan Transportation Authority
LADOT	Los Angeles Department of Transportation

**APPENDIX**  
**LIST OF ACRONYMS (Continued)**

LADPW	Los Angeles Department of Public Works
LADWP	Los Angeles Department of Water and Power
LAUSD	Los Angeles Unified School District
LNTP	Limited Notice To Proceed
LONP	Letter Of No Prejudice
LRTP	Long Range Transportation Plan
MIS	Major Investment Study
MPSR	Monthly Project Status Report
MR	Metro Rapidway
MRT	Metro Rapid Transitway (replaces BRT used prior to December 2002)
MTA	Metropolitan Transportation Authority
N/A	Not Applicable
NEPA	National Environmental Protection Act
NTE	Not to Exceed
NTP	Notice To Proceed
OCIP	Owner-Controlled Insurance Program
P3	Primavera Project Planner® (scheduling software)
PC	Project Control
PE	Preliminary Engineering
PEER	Permit Engineering Evaluation Report
PIP	Project Implementation Plan
PM	Project Manager
PMA	Project Management Assistance
PMIP	Project Management Implementation Plan
PMOC	Project Management Oversight Consultant
PMP	Project Management Plan (manual)
P&P	Policies & Procedures
PR	Project Report
PSR	Project Study Report
PUC	Public Utilities Commission
QA	Quality Assurance
QAR	Quality Assurance Report
QC	Quality Control
QPSR	Quarterly Project Status Report
RAC	Review Advisory Committee
RAG	Rail Activation Group
RFC	Request For Change
RFP	Request For Proposal
ROD	Record Of Decision
ROD	Revenue Operations Date
ROM	Rough Order of Magnitude

**APPENDIX**  
**LIST OF ACRONYMS (Continued)**

ROW	Right-Of-Way
RWQCB	Regional Water Quality Control Board
SCE	Southern California Edison
SCRRA	Southern California Regional Rail Authority
SFV	San Fernando Valley
SHA	State Highway Account
SHPO	State Historic Preservation Office
SIT	System Integration Testing
SOJV	Shimmick Obayashi Joint Venture
SOV	Schedule Of Value
SOW	Statement Of Work
SP	Special Provision
STIP	State Transportation Improvement Program
STP	Surface Transportation Program
STV	STV Incorporated
TBD	To Be Determined
TCRP	Traffic Congestion Relief Program
TRACS	Transit Automatic Control System
UFS	Universal Fare System
USDOT	U.S. Department Of Transportation
VE	Value Engineering
WBS	Work Breakdown Structure
WGI	Washington Group, Incorporated
WP	Work Package