

# The Cornfield Arroyo Seco Specific Plan



November 2010  
Draft



*Credit: Hargreaves and Associates from California State Parks  
Future Image of Los Angeles State Parks.*

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**NEIGHBORHOOD COUNCILS**

Arroyo Seco Neighborhood Council  
Chinatown-Alpine Hill Neighborhood Association  
Eagle Rock Neighborhood Council

Elysian Valley Riverside Neighborhood Council  
Glassell Park Neighborhood Council  
Greater Cypress Park Neighborhood Council  
Historic Cultural Neighborhood Council  
Lincoln Heights Neighborhood Council  
Silver Lake Neighborhood Council  
Solano Canyon Neighborhood Council

Woodbury University

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Patrick Johnston

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Alpine Recreation Center  
American Institute for Architects  
Arroyo Seco Foundation  
Audubon Society  
California Endowment's Center for Healthy Communities  
Center for Sustainable Cities  
Chinatown Advisory Committee  
Chinatown Redevelopment Commission  
Chinatown Service Center  
Chinese American Citizens Alliance  
Chinese Chamber of Commerce  
Creative Environments  
Cypress Park Recreation  
Cypress Park Youth & Family Center  
Downey Recreation Center  
El Pueblo de Los Angeles  
Farm Lab  
Friends of Los Angeles River (FoLAR)  
Goodwill Industries  
Homeboys Industries  
Lincoln Park Recreation Center  
Los Angeles & San Gabriel Watershed Council  
Natural Resources Defense Council  
North East Trees  
Puerta del Sol  
San Antonio Winery  
Santa Monica Mountains Conservancy  
The City Project  
Tree People  
Tujunga Watershed Stakeholders  
William Mead Residents Association  
Young Nak Church

**SCHOOLS**

Albion Street Elementary  
Ann Street Elementary School  
Cal State University, Northridge  
Cathedral High School  
Cal Poly Pomona  
Franklin High School  
Occidental College- Urban Environmental Policy Institute  
University of California, Los Angeles  
University of Southern California





# Vision



## PURPOSE

- To increase access to open space.
- To promote a mixed of uses.
- To provide economic growth opportunities for existing and emerging clean technologies.
- To re-connect historic communities.
- To reduce energy and water consumption.
- To promote a healthy watershed.
- To facilitate pedestrian and bicycle mobility.
- To provide access to a variety of transit options including frequent light rail and bus connections, shared vehicles and bicycles, and taxis.

The Cornfield Arroyo Seco Specific Plan (Plan) is the result of several years of planning and community engagement that involved community workshops, meetings and conversations with residents, employers, property owners, and community, business and environmental organizations, as well as staff input from numerous city, county, State, and federal departments.

The Plan incorporates an approximately 660 acre area that today is largely characterized by low-density light industrial buildings and publicly owned maintenance, utility and railway yards. The area is bounded by Chinatown and downtown Los Angeles to the southeast, Elysian Park and Cypress and Glassell Park to the north, and Lincoln Heights to the east. The Los Angeles River and Golden State Freeway both run north-south through the longitudinal center of the Plan while the Arroyo Seco and Pasadena Freeway run east-west through the northernmost section of the Plan area. Completing the infrastructure network is the Gold Line, a light rail line that cuts diagonally across the Plan area. While this abundance of infrastructure is largely responsible for the auto-centric and industrial character of the area it does provide residents and employees with access to multiple transportation options

The need for, and initial vision of, the Plan arose from a variety of planning and infrastructure activities that have occurred within the Plan area over the past six years. The primary activity that set the

stage for the Plan was the Los Angeles River Revitalization Master Plan (LARRMP) that was adopted by the City Council in 2007. The LARRMP re-imagined thirty-two miles of the Los Angeles River as a public amenity complete with continuous pedestrian and bicycle trails, mixed-use projects, parks and watershed management features. The LARRMP also identified much of the Plan area as an ideal location to showcase land use, watershed, open space and multi-modal connectivity innovations.

While the LARRMP established an early vision for the area, two major infrastructure projects were beginning to encourage unplanned redevelopment in the Plan area. The opening of the light rail Gold Line in 2004 and the promise of the new Los Angeles State Historic Park, on the parcel commonly known as the “Cornfield,” spurred the development of 900 residential units on what had been industrially zoned properties. While residential uses are not an unwelcome attribute adjacent to either transit or park land the area had not been previously planned to accommodate residential uses and many community amenities and support services were lacking. The City had also recently completed an industrial land use study that highlighted the negative economic impacts of industrial land conversions to residential uses and was therefore particularly sensitive to the impact of the potential loss of additional industrially zoned properties.

The various zone changes did however highlight the area’s vulnerability to change and illustrated the need for a comprehensive development plan to leverage the transit and park infrastructure improvements. Fortunately, the City’s Framework Element, adopted in 1995 and the Transportation Element, adopted in 1999, which make up part of the City’s General Plan, set forth a variety of policies to encourage the very mix of uses that was beginning to occur in an ad-hoc fashion within the Plan area. In particular, the General Plan encouraged the development of transit-oriented districts (TODs) in and around transit stations. These TODs were often envisioned as higher density places with a mix of land uses designed to accommodate pedestrians and bicyclists.

The General Plan policies, combined with the vision established in the LARRMP, set the stage for a model planning opportunity whereby transit-oriented strategies could be blended with the interests of watershed enhancements, open space and connectivity. In addition, during the initial months of the Plan’s conception, the US Green Building Council (USGBC)





## LEED-ND

Due to the timetable of the pilot program the draft Plan was submitted and accepted by the USGBC at the Pre-Approval stage as a Certified LEED-ND Plan in the Spring of 2010. The Plan achieved 31 of a possible 106 points. Because of the preliminary nature of the Plan a number of points were not yet able to be documented fully and therefore were not accepted. The Certified designation confirms that the Plan's numerous goals and objectives will reduce energy and potable water consumption, reduce ambient air temperature, increase pedestrian, bicycle, and transit mobility, encourage stormwater management, and promote a diversity of land uses and housing types. For a copy of the Plan's LEED-ND checklist and the Certification Letter from USGBC please refer to Appendix 02.

initiated a pilot phase of its newest Leadership in Energy and Environmental Design (LEED) rating system: LEED-Neighborhood Development (ND). The goal of the USGBC in developing the LEED-ND program was to expand the benefits of green building energy and conservation strategies to the neighborhood scale. The goals of LEED-ND seemed a perfect complement to the initial concepts for the Plan. The City, with the assistance of the Department of Water and Power, the Community Redevelopment Agency, and the non-profit Global Green, applied for and was accepted into the LEED-ND pilot program. The LEED-ND program proved a useful guide throughout the development of the Plan as a means to evaluate and set implementation standards for various conservation strategies.

With the vision and goals of the LARRMP in mind, coupled with the enthusiasm to remake this area as a sustainable model for the City, the desire to balance the needs of housing and jobs, a rich transit infrastructure, and the promise of an exciting new urban state park, the community got to work.

From the beginning, at the first workshop in September 2007 that was led by volunteer facilitators from the Western Justice Center, the community articulated a keen interest in seeing the Plan provide for safe and accessible passageways for pedestrians and bicyclists, additional open space, and access to the Los Angeles River and Arroyo Seco. The community also stressed the importance of providing for a mix of uses, especially near the transit stations and the State Park, that included affordable housing, along with community and employment uses. They also identified key industrial quarters that they wanted to protect, primarily along Main Street and the area currently defined as the Lincoln Heights Industrial Business Improvement District.

Due to the area's proximity to downtown, several universities, as well as transit and freeway infrastructure these industrial properties, along with the City's own vehicle storage and maintenance yards are well suited to attract and retain a portion of the existing light industrial sector as well as the emerging clean technology industry. While the City requires many of the yards to remain within easy access to the downtown area and surrounding communities a number of the properties are obsolete or dysfunctional. Redevelopment of these sites could attract key industrial employers, showcase examples of public private partnerships, increase site efficiencies, and demonstrate principles of sustainable development.



Merging the goals and objectives of the community with those of the General Plan and the LARRMP the Plan establishes zoning strategies and standards to transform the area into a collection of vibrant and mixed-use neighborhoods that will be a magnet for new jobs, residents, visitors, and a 24-7 vitality. Four new Districts replace the current segregated industrial and public facility zoning types. The new Districts will encourage living, working, shopping, education, open space, and recreational activities all within a short distance of each other. And, the new mixed-use Districts will appeal to employers and employees alike that are attracted to the nearby mix of living and playing options.

In meeting these goals the Plan will lessen dependence on automobiles, and thereby reduce vehicle emissions, while enhancing the personal health of residents, employees, and visitors and creating a safe and stable community. The Plan will also establish standards that will reduce the use of energy and potable water, capture stormwater, improve the ecology and hydrology of the Los Angeles River Watershed and Arroyo Seco, create connections from the community to the River and Arroyo Seco, and support the LARRMP. The open space requirements established in the Plan will provide places for people to socialize, including parks, sidewalks, courtyards, and plazas that are combined with shops and services as well as provide adequate public recreational open space within walking distance of residents and employees.

As the Plan area's population and employment increase, many more trips will begin and end within the Plan area. Walking to work or to a store, bicycling to the Los Angeles State Historic Park or the Los Angeles River, or taking transit from home to work will become an integral part of the Plan's lifestyle. The Plan's land use pattern will be intense and diverse, allowing many destinations to be reached within a short walk or bike ride and closely integrated with the transportation system. Trips outside of the Plan area will be facilitated by easy access to the Gold Line, one of the numerous bus lines, hailing a taxi, or borrowing a car or bicycle through "car or bike share" programs.

**Five-Sub Areas**

The Plan area is divided into five sub-areas, which serve as an organizational tool for referencing the distinct geographical areas. Existing barriers and infrastructure elements such as the Los Angeles River, Arroyo Seco, major street corridors and freeways were utilized to establish the boundaries of the five areas. While these boundaries currently divide the Plan area, implementation of the Plan over time will begin to erase the boundaries and transform the subareas into connected viable public places.

The images and text on the following pages provides descriptive and illustrative concepts for each of these five sub-areas. The numbers on each block of the map are for reference purposes only.

**High Speed Rail**  
During the development of the Plan, California voters supported a statewide effort to plan and develop a high-speed train system to connect major urban destinations. The Union Station location on the periphery of downtown Los Angeles is expected to be the central hub of the new system. While there is no high speed station planned for the Plan area all trains between Union Station and Northern California will pass through the Plan area. The recently established High-Speed Rail Authority (HSRA) is currently studying several alignment options in the Plan area and a final decision on the preferred alignment is not expected until 2011. The HSRA is aware of the goals of the Plan and the City will continue to provide input and mitigation suggestions. When a final alignment is selected the high-speed rail plan will be subject to environmental review to comply with federal and State law.





## AREA ONE

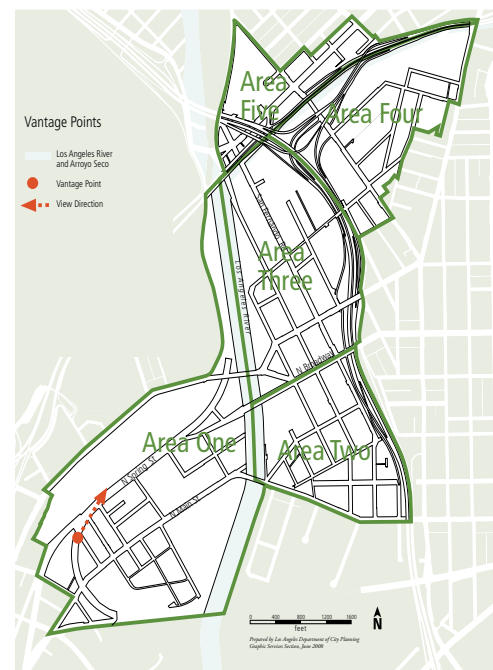
Sub-Area One links downtown and Chinatown to the River, the Los Angeles State Historic Park (Park) and Lincoln Heights. Much of the area will experience a transformation from a low intensity light industrial area to a mix of uses that includes new residences, intimate public spaces, and commercial space for light industrial, research and development, and creative industry.

The southwest portion of this sub-area, including the Department of Water and Power property and the properties along the north side of Main Street, will become an Urban Innovation district that will facilitate the development of light industrial uses.

The blocks on the southern side of Spring Street will act as a “front porch” for the Los Angeles State Historic Park (Park). The redesign of Spring Street will include a landscaped median, bicycle lanes, wide sidewalks and other pedestrian scaled street improvements. These improvements along with the introduction of retail shops and restaurants will reinforce active engagement between the Park and Spring Street. Sotello Street will link the Park and Spring Street with Main Street and the Urban Innovation district and the River to the south.



*Naud Street*





Before



After

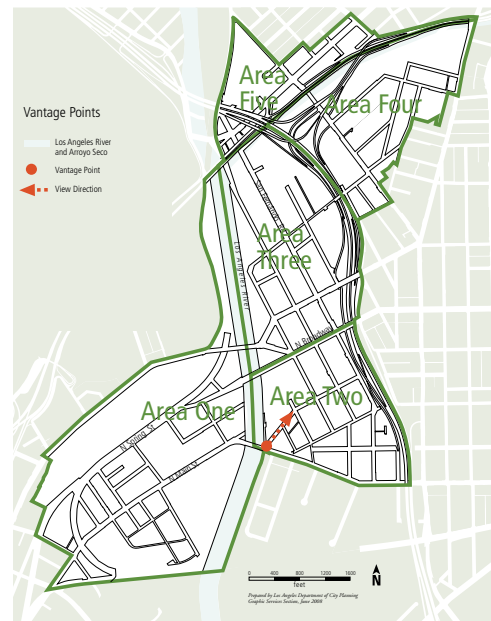
Spring Street

## AREA TWO

Area Two extends from the eastern edge of the River to Interstate 5, linking the southern portion of the Plan area to both Lincoln Heights and the industrial area to the south. The vast majority of this area is currently made up of single family and duplex style homes which will not be rezoned. Relative to the four other subareas of the Plan, area Two will experience minimal change.

An Urban Innovation district will line Main Street, providing a buffer from the sharp industrial edge south of Main Street and continuing the cluster of light industrial activity from west of the River. Downey and Albion Dairy Parks, the existing community, and the new Urban Innovation district will be linked into a stronger open space network that connects to the River. Pocket parks that treat stormwater during peak rain events will also provide new space for passive recreation and informal gatherings. Enhanced streets with wider sidewalks and trees will provide a pathway to the edge of the River and regional recreation networks.

At the northern edge of area Two, Broadway will be transformed, through new urban design and streetscape standards, into a public room, offering well-shaded places to sit, talk, shop, and wait for public transit. Beyond Broadway, public realm improvements along Avenue 19 and San Fernando Road will provide current residents with greater, more comfortable access to the opportunities and amenities that emerge as Area Two evolves.







Before



After

Mozart Street

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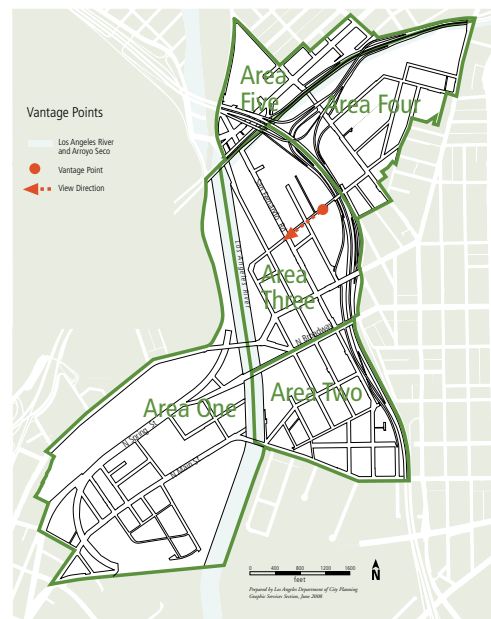


### AREA THREE

Area Three, informally dubbed “Rivertown” sits between Broadway, the River, Interstate 5, and the Arroyo Seco. What is currently a fragmented area with poor connections to surrounding districts and communities will become an attractive series of streets, parks, and waterfronts woven into the broader fabric of the Plan area and the City.

The central and northern portions of Area Three will become a hub for light industrial and research and development space—a combination of existing and future businesses that together form the critical mass of a successful urban district. The high volume of daytime activity created by the employment uses will be complemented by the evening and weekend activity generated by residents and visitors to institutions such as Young Nak Church.

A future new park on the existing Fire Department property is envisioned for the area. This new park will be within walking distance of every employee and resident within this area and will provide a central space for active and passive recreation, a place that will remain vibrant throughout the day and into the early evening. The sidewalks surrounding the park will become an extended “public room” accommodating a wide range of activities and gatherings for multiple generations.





Before



After

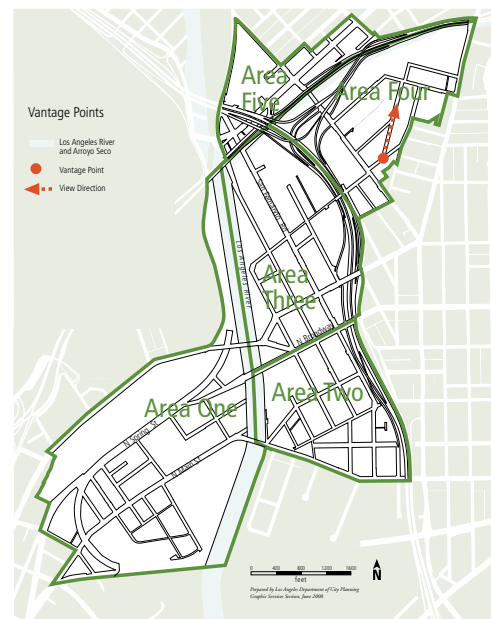
*Humboldt Street*

## AREA FOUR

Area Four is home to the Lincoln Heights/Cypress Park Gold Line Station and adjacent to the Heritage Square Station that sits just outside of the northeastern portion of this Area. New parks surrounded by a mix of light industrial and research and development uses, as well as existing residences, will create an anchor for the Urban Center, Urban Village and Urban Innovation districts surrounding the station.

To the northeast of Avenue 26 will be an Urban Innovation district with a mix of existing industrial tenants and companies engaged in the research, development, and production of clean technologies. This area will remain primarily industrial in character, but will become more hospitable to pedestrians and bicyclists. Formal and informal spaces for active and passive recreation will serve residents of surrounding neighborhoods as well as people working and living in Area Four.

The pedestrian overpass “gauntlet,” that currently links sub-area four to Cypress Street across the Arroyo Seco Parkway, will become a welcoming pathway as a result of a redesign of the overpass itself and the creation of defensible open spaces at its northern and southern entrances on either side of the freeway.







Before



After

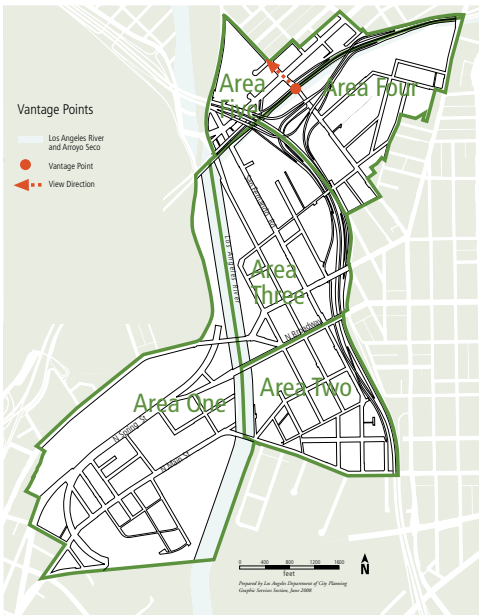
Humboldt Street



AREA FIVE

Area Five is framed by the Arroyo Seco and the River to the southeast, the River Center to the northwest, and Cypress Avenue to the east. The core of the area is Figueroa Street, which will evolve from an automobile-oriented arterial into a comfortable place to walk, sit, or wait for public transit. An Urban Center District will define Area Five in the future, providing a range of office, research and development, and light industrial jobs while enhancing the quality of the public realm through pocket parks, trees, bike lanes, and other features. New development and redevelopment will support the existing community and strengthen connections to Areas Three and Four to the south.

The River Center will remain an anchor for the northwest portion of Sub-Area Five, but will be connected to new employment opportunities made up of small blocks that provide a new link to Figueroa Street and the high concentration of transit travelers boarding and alighting at its intersection with Avenue 26. Shaded streets with wide sidewalks will seamlessly join this new development with Confluence Park and the River to the east and the western portion of Figueroa Street which will be a range of small to medium sized shops and offices, with occasional places for informal gathering and sitting.





Before



After

Avenue 26

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# 1

## Overview

### PURPOSE

- To transform an under served and neglected vehicular-oriented industrial and public facility area into a cluster of mixed-use pedestrian oriented and aesthetically pleasing neighborhoods.
- Increase access to open space.
- Provide economic growth opportunities for emerging clean technologies.
- Re-connect historical communities.
- To maintain and enhance the concentration of jobs, in both the public and private sectors.
- To provide a range of housing types and price levels that offer a full range of choices, including home ownership, for people of diverse ages, ethnicity, household sizes, and incomes.
- To provide shops and services for everyday needs, including groceries, day care, cafes and restaurants, banks and drug stores within an easy walk from home or work.
- To facilitate pedestrian mobility, encourage bicycle use, provide shared and unbundled parking spaces, and provide access to a variety of transit options including frequent light rail and bus connections, shared vehicles and bicycles, and taxis.
- To lessen dependence on automobiles, and thereby reducing vehicle emissions, while enhancing the personal health of residents, employees, and visitors.
- Provide “eyes on the street” to create a safe and stable community and to encourage interaction and identity.
- To respect historically significant buildings, including massing and scale while at the same time, encouraging innovative architectural design that expresses the identity of contemporary urban Los Angeles.
- To reduce the use of energy and potable water, capture stormwater, improve the ecology and hydrology of the Los Angeles River Watershed and Arroyo Seco, create connections from the community to the River and Arroyo Seco, and support the LARRMP.
- To provide places for people to socialize, including parks, sidewalks, courtyards, and plazas that are combined with shops and services.
- To provide adequate public recreational open space within walking distance of residents and employees, and to integrate public art and contribute to the civic and cultural life of the City.





## ESTABLISHMENT

**An ordinance establishing a Specific Plan**, known as the Cornfield Arroyo Seco Specific Plan, for a portion of the Central City North, Northeast, and Silverlake-Echo Park Community Plan areas.

THE PEOPLE OF THE CITY OF LOS ANGELES DO ORDAIN AS FOLLOWS

### Establishment of the Cornfield Arroyo Seco Specific Plan

**1.1.1.** The City Council hereby establishes the Cornfield Arroyo Seco Specific Plan applicable to that area of the City of Los Angeles shown within the solid line on the Plan Boundary and Block Numbers Map.

### Relationship to provisions of the Los Angeles Municipal Code

**1.2.1.** The regulations of this Specific Plan are in addition to those set forth in the planning and zoning provisions of the Los Angeles Municipal Code (LAMC) Chapter 1, as amended, and any other relevant ordinance, and do not convey any rights not otherwise granted under the provisions and procedures contained in that Chapter, except as specifically provided for herein.

**1.2.2.** Wherever this Specific Plan contains provisions which require lesser or greater restrictions or limitations on development than would be allowed or required pursuant to the provisions contained in Chapter 1 of the LAMC, the Specific Plan shall prevail and supersede the applicable provisions of that Code.

**1.2.3.** Site Plan Review Ordinance. Compliance with the provisions of this Specific Plan shall be considered compliance with the requirements of LAMC Section 16.05.

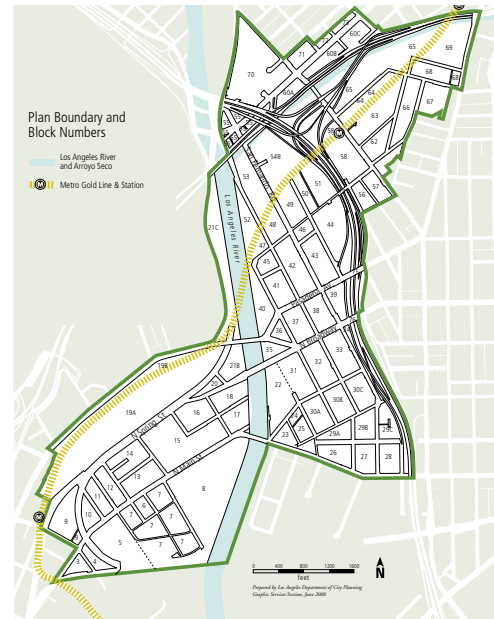
**1.2.4.** Los Angeles River Improvement Overlay (LA-RIO). Compliance with the provisions of this Specific Plan shall be considered with compliance with the requirements of LAMC Sections \_\_\_\_.

**1.2.5.** Landscape Ordinance. Compliance with the provisions of this Specific Plan shall be considered compliance with the requirements of LAMC Sections 12.40, 12.41, 12.42, and 12.43.

**1.2.6.** The procedures for the granting of adjustments, exceptions, and/or amendments to the requirements of this Specific Plan are set forth in LAMC Sections 11.5.7. E, F, and G.

### Application

**1.3.1.** The provisions of this Specific Plan shall apply to all Projects located on any lot located in whole or in part within this Specific Plan area.



### **Exceptions**

The provisions of this Specific Plan shall not apply to:

**1.4.1.** Any Project that has obtained a still-valid discretionary land use approval from the City prior to the operative date of this Specific Plan.

**1.4.2.** Demolitions.

**1.4.3.** Any Project where plans were accepted by the Department of Building and Safety for plan check prior to the operative date of this Specific Plan.

**1.4.4.** Any Project complying with an order issued by the Department of Building and Safety for the repair of an unsafe or substandard condition.

**1.4.5.** The restoration, repair, or remodeling of an existing building provided that the cost of the modification, in any one 24-month period, does not exceed 50 percent of the replacement value of the building or structure before the alterations or addition as determined by the Department of Building and Safety and does not increase the height, floor area, or occupant load of the original building; or,

**1.4.6.** The Interior remodeling of any other existing building, except for interior alterations to the ground floor that will result in the alteration of windows, display windows, entrances, storefronts or otherwise minimize ground floor transparency.

### **Exceptions for identified Historic Resources**

**1.4.7.** The following provisions of this Specific Plan shall not apply to projects for buildings identified as Historic Resources:

4.3.1, 4.4.1, 4.7.2, 4.7.7, 5.1.2, 5.4.1, 5.4.3, 6.8.6, 7.5.1, and 7.5.2

### **Prohibitions**

**1.5.1.** No grading permit, foundation permit, building permit, or use of land permit shall be issued for any Project that requests a Bonus Transfer of Floor Area, includes a Public Paseo, or is greater than 25,000 square feet on any lot located in whole or in part within this Specific Plan area, unless the Project complies with all applicable provisions of this Specific Plan and has received an Administrative Clearance from the Director of Planning.

### **Procedures**

**1.6.1.** Applications for an Administrative Clearance from the Specific Plan shall be filed at any public counter of the Department of City Planning, on a form provided by the Department, and include all information required by the instruction on the application and any additional submission requirements. The Director shall determine if the application qualifies for Administrative Clearance and whether the Project complies with all applicable Plan regulations.

## Fees

**1.7.1.** The application fee for an Administrative Clearance shall be as set forth in LAMC Section 19.01 E or 19.01 I. The fee in Section 19.01 E shall be charged for administrative clearance for new construction permits only. The fee in Section 19.01 I shall be charged for all other building permit sign-offs.

**1.7.2.** The application fee for a Transfer of Floor Area Rights of 50,000 or more than 50,000 square feet under either the Bonus or Transfer Programs shall be as set forth in Section 19.\_\_\_\_ for the Approval of Transfer of Floor Area Plan.

**1.7.3.** The application fee for a Transfer of Floor Area Rights of less than 50,000 square feet under either the Bonus or Transfer Programs shall be as set forth in Section 19.\_\_\_\_ .

### 1.8.1. Definitions

**Accessory Use.** A use that is customarily incidental to that of the main building/use of the land; and on the same lot with a main building or main use.

**Active Street.** A street where retail, cultural, office, and/or residential uses are encouraged at the ground floor level where adjacent to street frontage.

**Active Industrial Street.** A street where retail, office, lobby, meeting rooms or sales areas are encouraged at the ground floor level where adjacent to street frontage.

**Adult Entertainment.** Uses associated with “specified sexual activity” and/or “specified anatomical areas.”

**Ancillary.** A permitted use that is limited to 10 percent of the on-site principal use. May be located in a stand-alone building or structure separate from the principal use. More than one ancillary use may be permitted on a single site but in no case shall the combined maximum floor area of all ancillary uses exceed a floor area ratio of 1.0. Uses designated as Ancillary are intended specifically for the use and benefit of the employees and families, residents, or patrons of the nearby industrial/ commercial/recreational/transit/residential/ educational facilities such as food and beverage stores, health and personal care, recreational facilities, book stores, or similar uses.

**Animal Clinic / Kennels.** Uses where animals or pets are given medical or surgical treatment by an authorized licensing agent to treat injuries, illnesses and diseases of animals, including uses where small, domesticated animals and pets are cared for and boarded overnight for a limited amount of time.

**Architectural Feature.** Those purely aesthetic elements of the building, designed internal to the overall style of architecture, that are not habitable or otherwise to be counted toward floor area.





**Automobile Fueling and Service Stations.** Uses for fueling stations and car washes.

**Back of House.** Support and service areas usually not visible from the street.

**Base FAR.** The base floor area ratio (FAR) established for each district within the Plan area.

**Block.** A block is a tract of land bounded on all sides by streets or by a combination of streets, public parks, railroad rights-of-way, pier head lines or airport boundaries.



**Building Frontage Facade.** Those portions of the exterior of a building or structure that are closest to the frontage of the property.

**Building Height.** The height of a building shall be measured vertically from grade to the highest point of roof.

**Brownfield.** Abandoned or under used industrial or commercial facilities (including older gas stations and auto repair yards located on smaller sites adjacent to residential neighborhoods) that may be contaminated by low concentrations of hazardous waste or pollution and have the potential to be redeveloped into other uses once environmental remediation has been performed.

**Central Parking Structure.** A parking structure or surface lot accessible and available for use by the public.



**Commercial Office.** Uses that provide office space for professional services.

**Community Serving Uses.** Uses may include but are not limited to child care and other educational services, public library, fire station, medical services, or non-profit whose services directly benefit the community.

**Covenant.** A written document entered into by any and all owners of the property regarding the use or development of one or more lots, approved by the Director of Planning, and executed and recorded by such property owners in the Los Angeles County Recorder's Office. The covenant shall be in form to run with the land and shall be binding on any subsequent owners, heirs, successors (including but not limited to beneficiaries) or assigners. After recordation, a copy bearing the Recorder's number and date shall be furnished to the City Planning Department for its records.

**Curb Cut.** An inclined cut in the edge of a sidewalk to permit vehicular access to a driveway, garage, parking lot, or loading dock.



**Decibel A-Weighted Scale.** The common measurement of environmental and industrial noise.

**Designated Historical Resource.** A building, structure, landscape element, or natural feature listed in or formally determined to be eligible for the National Register of Historic Places, California Register of Historical Resources, or the City’s list of Historic-Cultural Monuments , or a Contributing Element in a City Historic Preservation Overlay Zone.

**Dual Pipe.** A system of plumbing installations used to supply both potable and reclaimed water to a home or business through separate pipes. Under this system, two completely separate water piping systems are used to deliver water to the user.

**Donor Site.** A site from which Floor Area Rights are transferred pursuant to the provisions of this Plan.

**Eligible Historic Resource.** A building, structure, landscape element, or natural feature identified through SurveyLA (the Los Angeles Historic Resources Survey Project) to be eligible for recognition as historically or architecturally significant either individually or as part of a district at the local, State, or national level.

**Entertainment and Multi-Purpose Cultural Facilities.** Uses designed to host public or private gatherings for cultural activities or entertainment.

**Floor Area.** The sum of the gross area of each floor of the building, excluding mechanical space, cellar space, floor space in open balconies, elevators or stair bulkheads and, floor space used for accessory parking that is located less than 23 feet above curb level.

**Floor Area Ratio.** The floor area ratio (FAR) is the principal bulk regulation controlling the size of buildings. FAR is the ratio of total building floor area to the area of its zoning lot (prior to any dedications).

**Floor Area Rights** means the right to construct additional floor area within a Project, pursuant to an approved Transfer Plan, in excess of the amount of floor area such Project would be allowed based on its lot area.

**Free-Standing Fast Food Establishment.** A single or multiple tenant free-standing structure designed solely for restaurant use which dispenses prepared food over a counter or by way of drive through service for consumption on or off the premises. This definition does not include cafeterias.

**Fully Shielded Fixture.** Outdoor lighting fixture shielded or constructed so that no light is emitted above the horizontal plane, and light rays are only emitted by the installed fixtures in such a manner



that do not direct light or light trespass onto adjacent property, on any other property within the line of sight (direct or reflected) of the light source, or to any member of the public who may be traveling on adjacent roadways or rights-of-way.

**Green Roof.** A roof of a building that is partially or completely covered with vegetation and soil, or a growing medium, planted over a waterproofing membrane.

**Greenway.** A new zoning district established by this Plan that provides for open space. The allowable uses are limited in this District.



**Heavy Manufacturing.** Uses that fabricate, assemble, process, extract, or treat predominantly raw materials; uses that require explosive or petroleum materials; or uses that produce noise, odor, dust, hazardous materials or other pollutants/nuisances that cannot be contained on site.



**Hospitals, Nursing and Residential Care Facilities.** Uses involved in providing medical, surgical, or assisted living care to patients and offering short and long-term overnight care.

**Holiday Lighting.** Seasonal displays of 60 days or less within one calendar year, using multiple low wattage bulbs (approximately 15 lumens or less) provided they do not constitute a fire hazard, create a nuisance, and are maintained in a safe condition.



**Hotels.** Housing built to accommodate the general and traveling public for a typical fee, generally limited to stays of less than 31 days.

**Identification Sign.** A wall sign that is limited to a company logo, generic type of business, or the name of a business or building.

**Illuminated Architectural Canopy Sign.** An exposed illuminated structure that is attached to the wall of a building with the face of the sign approximately parallel to the wall and with the message integrated into its surface.

**Inflatable Device.** A sign that is a cold air inflated object, which may be of various shapes, made of flexible fabric, resting on the ground or structure and equipped with a portable blower motor that provides a constant flow of air into the device. Inflatable devices are restrained, attached, or held in place by a cord, rope, cable, or similar method. The term inflatable device shall not include any object that contains helium, hot air, or a lighter-than-air substance.

**Information Sign.** A sign that is limited to a message giving directions, instructions, menus, or selections.

**Lamp.** The generic term for an artificial light source installed in the socket portion of a fixture, to be distinguished from the whole

assembly. Commonly referred to as a “bulb”.

**LEED®.** The Leadership in Energy and Environmental Design (LEED) Green Building Rating System™ is the nationally accepted benchmark for the design, construction, and operation of high performance green buildings. See the official website [www.usgbc.org](http://www.usgbc.org) for more information.

**Light Manufacturing and Assembly.** Uses that process, fabricate, assemble, treat, or package finished parts or products and/or whose noise, odor, dust, hazardous materials or other pollutants/nuisances capable of harming or disrupting adjacent uses can be contained on site.



**Light Trespass.** Light from any outdoor lighting onto neighboring property or property that is within a direct line from the light source that interferes with viewing of night sky, eliminates the ability to have darkness on the property, or shines on any area on these properties or structures.

**Los Angeles River Revitalization Master Plan (LARRMP).** Plan approved in May 2007 by the Los Angeles City Council, which describes a vision for the revitalization of the 32 miles of the Los Angeles River that are within the City boundaries of the City of Los Angeles.



**Lot Area.** The total horizontal area (in square feet) within the lot lines of a lot. (prior to any dedications)

**Lot Coverage.** Lot coverage is that portion of a zoning lot which, when viewed from above, is covered by a building.

**Lot Depth.** Lot depth is the mean horizontal distance between the front lot line and rear lot line of a zoning lot.

**Lot Line.** Any lot boundary line.

**Maximum FAR.** The maximum floor area ration (FAR) established for each district withiin the Plan area.

**Monument Sign.** A sign that is erected directly upon the existing or artificially created grade, or that is raised no more than 12 inches from the existing or artificially created grade to the bottom of the sign.

**Mural Sign.** A sign that is painted on or applied to and integral with a wall, the written message of which does not exceed three percent of the total area of the wall.

**North Facade.** North facades are defined as those facades between -22.5 and +22.5°N.

**Off-Site Sign.** A sign that displays any message directing attention to a business, product, service, profession, commodity, activity,



event, person, institution or any other commercial message, which is generally conducted, sold, manufactured, produced, offered or occurs elsewhere other than on the premises where the sign is located.

**On-Site Sign.** A sign that is other than an off-site sign.



**Parking.** Facilities and sites that provide free-standing, fee parking, as distinguished from parking provided on-site by a business or residential complex for its customers, employees, or residents on or adjacent to the business location or residential complex. This facility can be either a surface lot or a parking structure.

**Parkway Zone.** Sidewalk area reserved for street furniture, landscaping and access to parked cars.

**Partially Shielded Fixture.** A fixture employing a top shield to reduce upward light, but otherwise does not shield the lamp from view.

**Paseo or Pedestrian Walkway.** Walkway that is typically open to the sky and that provides pedestrian passage between structures, or through landscaping, or parking lots, which is distinguished by ground surface treatments that provide for pedestrian safety and ease of movement.

**Pedestrian Amenities.** Uses, services, or features typically available within, or adjacent to, a public right-of-way that assist and enhance the pedestrian experience. Amenities may include but are not limited to street furniture, wayfinding signage, kiosks, street lighting, street trees, coffee shops, and bookstores.

**Pedestrian Lighting.** Freestanding lighting fixtures not exceeding a height of thirty-six (36) inches from ground grade level.



**Personal Services.** Uses involved in personal service-oriented sales to the general public.

**Plan.** The Cornfield/Arroyo Seco Specific Plan.

**Pole Sign.** A freestanding sign that is erected or affixed to one or more poles or posts and that does not meet the requirements of a monument sign.

**Project.** The construction, erection, or addition to any building or structure, on a lot located in whole or in part within the areas shown in the map on page 2 of this Section which requires the issuance of a grading permit, foundation permit, building permit, or use of land permit.

**Projecting Sign.** A sign, other than a wall sign, that is attached to a building and projects outward from the building with one or more sign faces approximately perpendicular to the face of the building.

**Projection.** The distance by which a sign extends beyond the building line.

**Publishing, Motion Picture, and Broadcasting Industries.** Uses engaged in film, video, audio, and other media production; but excluding movie houses and theatres.



**Publicly Accessible Open Space.** Active or passive open space that is accessible to the public for a minimum of 10 hours per day or during all daylight hours whichever is more.

**Public Service Facilities.** Uses that provide government services to the public (except health-related services; See Hospitals, Nursing and Residential Care Facilities).

**Purple Pipe.** Pipes that are for the exclusive use of recycled water. Pipes are typically painted purple to distinguish them from pipes carrying potable water.

**Receiver Site.** A Receiver Site is a site that receives additional Floor Area Rights from a Donor Site pursuant to the provisions of this Plan.



**Repair and Maintenance Facilities.** Uses engaged in the repair or servicing of industrial, business or consumer machinery, equipment, products or by-products. Repair and service of consumer goods falls into the Personal Services category.



**Research and Development.** Uses engaged in scientific and technical research leading to the development of new products and processes, including development/testing activities and prototype fabrication.

**Retail Street.** A street where retail and community serving uses are encouraged at the ground floor level where adjacent to street frontage.



**Recreation Facilities.** Uses engaged with both indoor and outdoor recreational activity for the general public.

**Religious and Social Service Organizations.** Uses characterized by religious and sectarian activities as well as uses involved with ongoing social services.



**Residential-Multi-Family.** Structures containing more than one dwelling unit located on a single lot. A structure that provides multiple living units that may have separate sleeping areas and some combination of shared bath or toilet facilities. In addition, the structure may or may not have separate cooking facilities for each unit. Single Room Occupancy (SRO) residential structures, residential hotels and rooming houses are also included in this category.





**Residential-Single Family.** Dwelling units where no more than one dwelling unit is located on a lot. These units are usually detached, and occupied by a single household. An accessory unit (“granny flat”) or servant quarters may also be provided.



**Restaurants and Bars.** Uses involved in food, beverage and entertainment-oriented retail sales to the general public. Adult entertainment is not included in this category.



**Retail.** Uses involved with the sale or lease of new or used products to the general public.

**River Buffer Area.** A 300 feet buffer area adjacent to the Los Angeles River and Arroyo Seco. The 300 liner foot distance is calculated horizontally from the bottom of the channel perimeter.

**River Public Benefits** means amenities provided to the public such as affordable housing; public open space; historic preservation; recreational, cultural, community, and public facilities; storm water management; watershed protection and preservation; habitat restoration; flood control; streetscape improvements; public arts programs; or public transportation improvements with a demonstrable connection to improvements to the Los Angeles River and its environs.

**River Public Benefit Payment** means that dollar sum established by the application of the formula set forth in Section 2 of this Plan.

**River Public Benefit Trust Fund** means that certain interest-bearing Trust Account administered by the City Clerk’s Office designated as River Public Benefit Program Fund XXXX, from which funds may be distributed as set forth in of Section 2 of this Plan.



**Roof Sign.** A sign erected upon a roof of a building.

**Schools, Colleges, Tutoring, and Technical Training Programs.** Uses that includes public and private schools as well as institutions offering courses of general or specialized study leading to a degree or certificate.



**Server Farms.** Centers established for the exclusive purpose of storing computer and internet data.

**Setback/Street Line.** A setback is the portion of a building that is set back above the base height (or street wall or perimeter wall) before



the total height of the building is achieved. The position of a building setback in height factor districts is controlled by sky exposure planes and, in contextual districts, by specified distances from street walls.

**Sign.** A whole or part of a display board, wall, screen, or object used to announce, declare, demonstrate, display, or otherwise present a message and attract the attention of the public.

**Sign Area.** An area circumscribed by the smallest geographic shape created with a maximum of eight straight lines, which will enclose all words, letters, figures, symbols, designs, and pictures, together with all framing, background material, colored or illuminated areas, and attention-attracting devices, forming an integral part of an individual message except that:

1. Wall signs having no discernible boundary shall have the areas between letters or words intended to be read together included in any computation of surface area.
2. For spherical, cylindrical, or other three-dimensional signs the area of the sign shall be computed from the smallest two-dimensional geometrical shape or shapes, which will best approximate the greatest actual surface area visible from any one direction.
3. Sign support structures are excluded.

**Sign Face.** The surface upon which the sign message is placed.

**Specific Plan Area.** That area shown within the heavy lines on Map titled Plan Boundary and Block Numbers Map on page 2 of section 1.

**Stormwater.** Describes water that originates during precipitation events.

**Street Frontage.** The length of a lot line separating a lot from one street.

**Streetwall (or street edge).** The vertical face of one or more buildings adjacent within setback area and parallel to the public right of way.

**Supergraphic Sign.** A sign, consisting of an image projected onto a wall or printed on vinyl, or mesh or other material with or without written text, supported and attached to a wall by an adhesive and/or by using stranded cable and eye-bolts and/or other materials or methods, and which does not comply with the following provisions of L.A.M.C. Sections: 14.4.10, 14.4.16, 14.4.17, 14.4.18; and/or 14.4.20.

**Transfer.** Means the conveyance of unused allowable Floor Area of a lot from a Donor Site to a Receiver Site, which is approved in accordance with the requirements of this Plan.

**Transfer Plan** means a plan which identifies and describes the Donor Site(s), Receiver Site(s), amount of Floor Area Rights to be

transferred, and the River Public Benefit Payment.



**Trucking and Transportation Terminals.** Uses engaged in the dispatching, maintenance and long-term or short-term storage of large vehicles such as tractor-trailers, catering trucks, shipping vessels, helicopters, locomotives, and airplanes, among others.

**Urban Center.** A new zoning district established by this Plan that permits the integration of residential and employment uses within a single site. The inclusion of residential activities is limited as a proportion of the overall employment areas.

**Unused FAR.** FAR that a Donor Site does not need and has elected to transfer to a Receiver Site. An example might be an historic building that the property owner has elected to preserve and in which case the historic building is located within a district that allows a 3.0 FAR but the existing building utilizes only a 1.0 FAR. The difference of 2.0 FAR is considered Unused and may be transferred to a Project within the same district.

**Urban Agriculture.** Uses that engage in the growing of fresh produce and foods. Large-scale agriculture and animal husbandry are not allowed and are not included in this category.

**Urban Village.** A new zoning district established by this Plan that permits the integration of commercial, residential and industrial uses within a single site.

**Urban Innovation.** A new zoning district established by this Plan that permits a variety of industrial employment uses. The inclusion of commercial and residential activities are limited as a proportion of the overall employment area.

**Use.** Any activity, occupation, business or operation, listed in the Uses Table in Section 2, which is conducted in a building or on a tract of land.



**Utilities.** Uses that provide the transfer or delivery of power, water, natural gas, sewerage, stormwater runoff, telephone and related communication services.

**Wall Sign.** Any sign attached to, painted on, or erected against the wall of a building or structure, with the exposed face of the sign in a plane approximately parallel to the plane of the wall.



**Warehousing and Storage.** Uses that provide, hold, and distribute goods in large quantities, especially to retail sales establishments. Long-term and short-term storage of commercial goods and personal items are included.

**Waste Management and Remediation Services.** Uses that receive solid or liquid wastes (including hazardous waste) for on-site disposal, recycling, or transfer to another location, including uses that manufacture or produce goods or energy from the biological decomposition of organic material.



**Wholesale.** Uses engaged in the sale, lease, or rental of products primarily intended for industrial, institutional, or commercial businesses (not individual consumers). The uses emphasize on-site sales or order taking and often include display areas. Businesses may or may not be open to the general public, but sales to the general public are limited.



**Window Sign.** Any sign, except for a supergraphic sign, that is attached to, affixed to, leaning against, or otherwise placed within six feet of a window or door in a manner so that the sign is visible from the outside of the building.

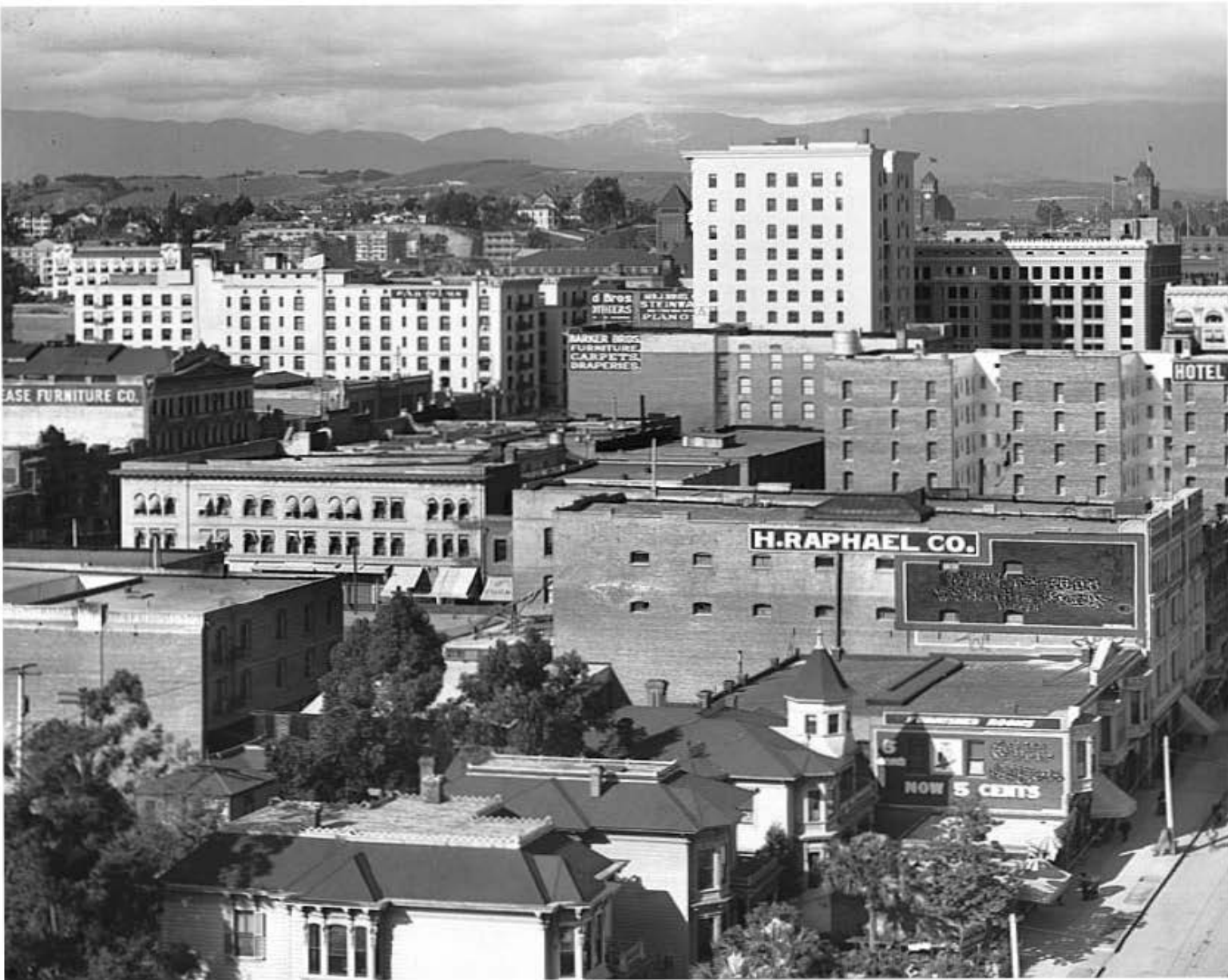
**Zoning Code.** The planning and zoning provisions of the Los Angeles Municipal Code (LAMC), Chapter 1 as amended.

**SEVERABILITY**

If any provision of this Specific Plan or its application to any person or circumstances is held to be unconstitutional or otherwise invalid by any court of competent jurisdiction, that invalidity shall not affect other Specific Plan provisions, clauses or application which can be implemented without the invalid provisions, clause or application, and to this end, the provisions and clauses of this Specific Plan are declared to be severable.







DRAFT

# 2

## Districts



### PURPOSE

- To protect existing light industrial areas from residential encroachment.
- To provide areas where residential, commercial, and light industrial uses can co-locate both horizontally and/or vertically.
- To provide residents, visitors, and employees with easy access to increased recreational opportunities.
- To facilitate the development of mixed-use and affordable housing projects.



## DISTRICTS

**2.1.1** The following four new districts are established by this Plan and are applied to property shown on the Land Use Districts Map.

**Greenway.** The Greenway District designates land primarily for recreation or open space. Limited development is permitted within this district if it provides for recreational, arts, educational, and/or community related activities.



**Urban Village.** The Urban Village District is a mixture of residential and non-residential land uses. The non-residential uses are expected to include a combination of supportive community retail services such as grocers and small scale along with industrial, creative and cognitive production, and crafts and artisan activities. The residential and community amenities are expected to encourage increased employment in the Urban Village and the surrounding districts.











**Urban Innovation.** The Urban Innovation District provides for a flexible range of light industrial job uses and research and development activities that benefit from close proximity to community, entertainment, and recreational activities.

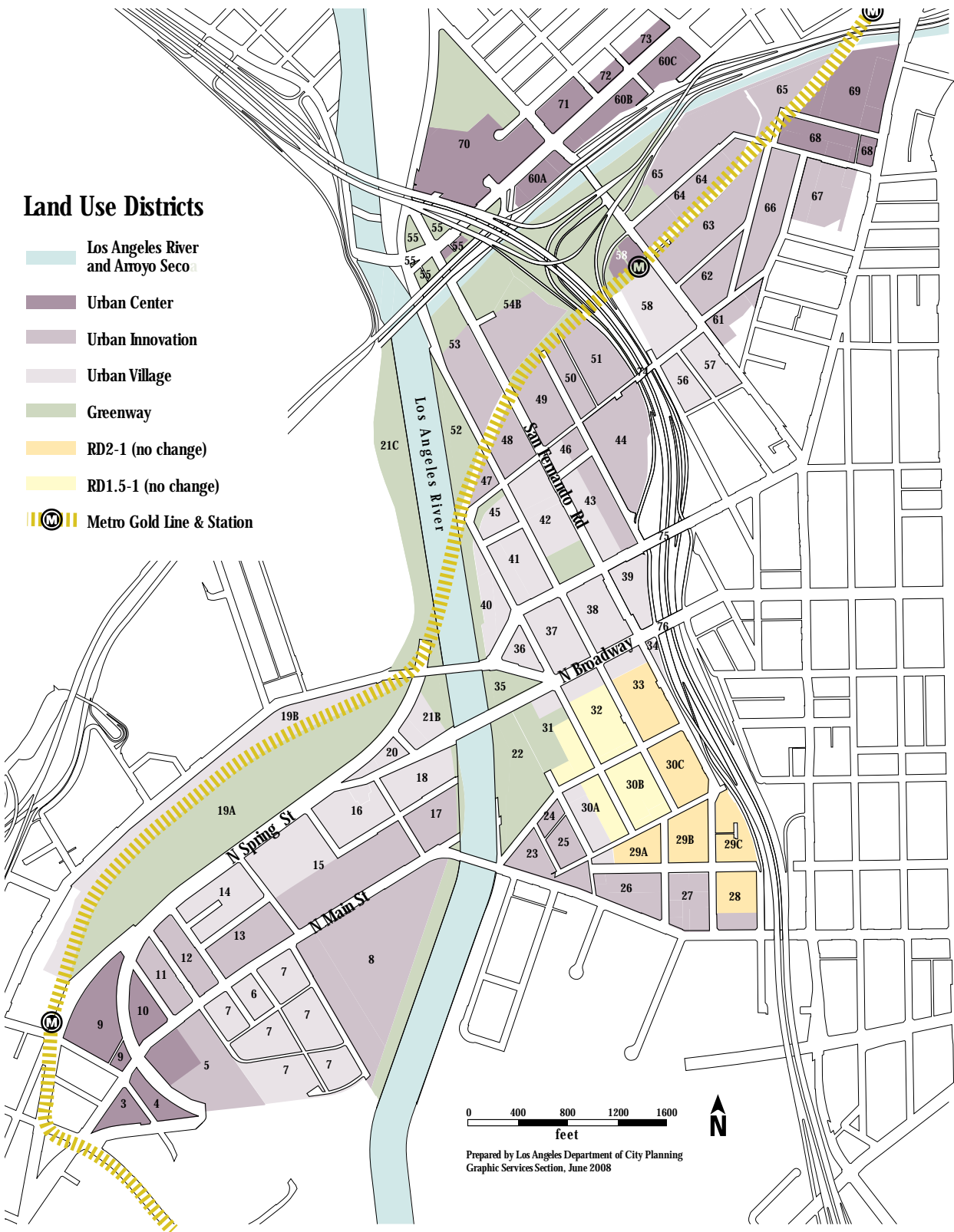


**Urban Center.** The Urban Center Districts are located immediately adjacent to each of the three transit stations and provide for a wide range of land uses including retail, offices, restaurants, light industrial, and to a limited extent residential. Lodging, entertainment, and civic uses are also encouraged.



# Land Use Districts

-  Los Angeles River and Arroyo Seco
-  Urban Center
-  Urban Innovation
-  Urban Village
-  Greenway
-  RD2-1 (no change)
-  RD1.5-1 (no change)
-  Metro Gold Line & Station



Prepared by Los Angeles Department of City Planning  
Graphic Services Section, June 2008

**USES**

**2.2.1.** Uses in each District are either permitted, prohibited, permitted up to a specified limit, permitted under certain conditions, or permitted with a Conditional Use Permit as described in the Uses Table below.

**2.2.2.** Accessory Uses that are customarily incidental to that of the main building/use of the land; and on the same lot with a main building or main use are implicitly allowed although not directly identified as permitted in the table.

**Uses Table**

<b>Use Classifications</b>	<b>Greenway</b>	<b>Urban Village</b>	<b>Urban Innovation</b>	<b>Urban Center</b>
Heavy Manufacturing	No	No	No	No
Light Manufacturing and Assembly	No	Yes	Yes	Yes
Repair and Maintenance Facilities	No	Yes <sup>2</sup>	Yes	Yes
Research and Development	No	Yes	Yes	Yes
Publishing, Motion Picture, and Broadcasting Industries	No	Yes	Yes	Yes
Trucking and Transportation Terminals	No	No	No	No
Urban Agriculture	Yes	Yes	Yes	Yes
Utilities	Yes	Yes	Yes	Yes
Warehousing and Storage	No	Ancillary <sup>4</sup>	Ancillary <sup>4</sup>	Ancillary <sup>4</sup>
Waste Management and Remediation Services	No	Yes <sup>9</sup>	Yes <sup>9</sup>	Yes <sup>9</sup>
Wholesale	No	Ancillary	Ancillary	Ancillary
Automobile Fueling and Service Station	No	Yes <sup>9</sup>	Yes <sup>9</sup>	Yes <sup>9</sup>
Commercial Office	No	Yes <sup>1</sup>	Ancillary	Yes
Parking	No	Yes <sup>6</sup>	Yes <sup>6</sup>	Yes <sup>6</sup>
Restaurants and Bars	Yes	Ancillary <sup>1,3</sup>	Ancillary <sup>1,3</sup>	Ancillary <sup>1,3</sup>
Retail	Ancillary	Ancillary <sup>1</sup>	Ancillary <sup>1</sup>	Ancillary <sup>1</sup>
Personal Services	No	Ancillary	Ancillary	Ancillary
Server Farms	No	Ancillary	No	Ancillary
Residential-Multi-Family	No	Yes <sup>1</sup>	Yes <sup>1</sup>	Yes <sup>1</sup>
Residential-Single Family	No	No	No	No
Hospitals, and Nursing and Residential Care Facilities	No	Yes	No	No

Use Classifications	Greenway	Urban Village	Urban Innovation	Urban Center
Hotels	No	Yes <sup>1,7</sup>	Yes <sup>1,8</sup>	Yes <sup>1,7</sup>
Entertainment and Multi-Purpose Cultural Facilities	Yes	Yes	No	Yes
Public Service Facilities	No	Yes	Yes	Yes
Recreation Facilities and Spectator Sports	Yes	Yes	Ancillary	Yes
Religious and Social Service Organizations	No	Yes	No	Ancillary
Schools, Colleges, and Technical Training Programs	No	Yes	Yes <sup>5</sup>	Yes

- (1) See Table 2.2.3 for limits on FAR or square footage.
- (2) Excludes truck repair.
- (3) Free-Standing Fast Food establishments permitted with a Conditional Use Permit.
- (4) Excludes personal storage.
- (5) Limited to Technical Training Schools or Programs.
- (6) Subject to area Parking Cap -See Section 6.
- (7) Residential hotels or rooming houses permitted with a Conditional Use Permit.
- (8) Residential hotels or rooming houses not permitted.
- (9) Permitted with a Conditional Use Permit.

**2.2.3.** The Limits Table provides further details as to the proportion of use that will be allowed.

**Limits Table**

Use Classifications	Greenway	Urban Village	Urban Innovation	Urban Center
Commercial Office	N/A	65% <sup>1</sup>	10% <sup>1</sup>	65% <sup>1</sup>
Retail/Restaurants/Bars	1,200 sf <sup>2</sup>	20,000 sf <sup>2</sup>	5,000 sf <sup>2</sup>	100,000 <sup>2</sup>
Residential Multi-Family	N/A	90% <sup>3</sup>	15% <sup>3</sup>	15% <sup>3</sup>
Hotels	N/A	150 rooms	100 rooms	200 rooms

1 - Floor area of Commercial Office shall not exceed the allowable percentage of the total gross floor area of all principal and ancillary uses combined.

2 - Limited square footage permitted for each establishment.

3 - Floor area of Residential Multi-Family shall not exceed the allowable percentage of the total gross floor area of all principal and ancillary uses combined. Construction shall only be permitted if built after or concurrent with on-site non-residential uses.



**FLOOR AREA RATIO**

**2.3.1.** A Base FAR (Base FAR) and Maximum FAR (Max FAR) is established for each parcel as set forth in the FAR Table below.









**2.3.2.** Additional FAR, up to the Max FAR, can be added to the Base FAR through either the Bonus FAR and/or Transfer of Floor Area (TFAR) Programs.

**2.3.3.** The proportion of uses established in Sections 2.2.1 to 2.2.3. shall remain applicable regardless of the projects resulting FAR.

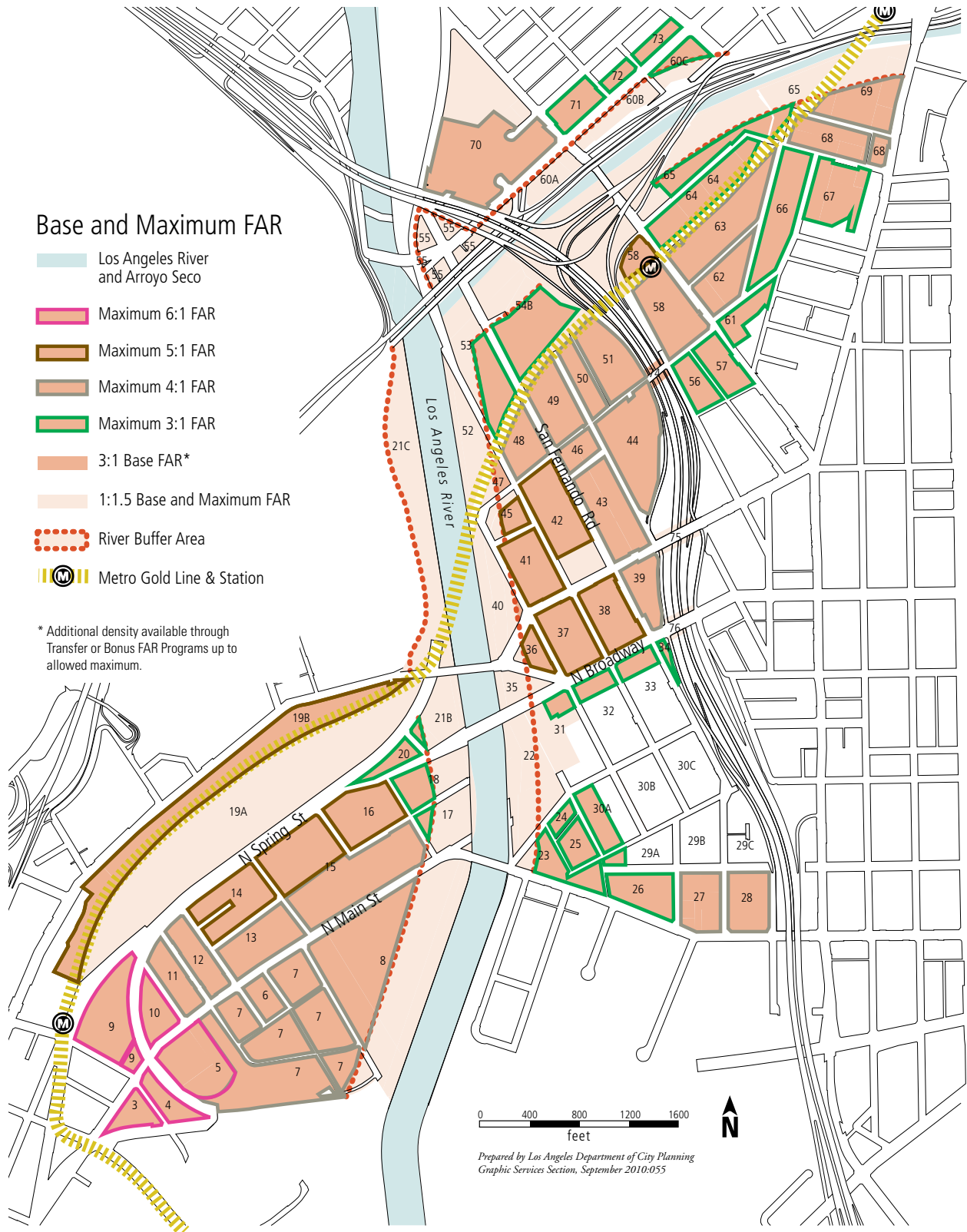
**FAR Table**

<b>Density</b>	<b>Greenway</b>	<b>Urban Village</b>	<b>Urban Innovation</b>	<b>Urban Center</b>
Base FAR	1.5	3	3	3
Base FAR within River Buffer Areas	1.5	1.5	1.5	1.5
Max FAR	1.5	5	4	6
Max FAR within River Buffer Areas	1.5	1.5	1.5	1.5

## Base and Maximum FAR

-  Los Angeles River and Arroyo Seco
-  Maximum 6:1 FAR
-  Maximum 5:1 FAR
-  Maximum 4:1 FAR
-  Maximum 3:1 FAR
-  3:1 Base FAR\*
-  1:1.5 Base and Maximum FAR
-  River Buffer Area
-  Metro Gold Line & Station

\* Additional density available through Transfer or Bonus FAR Programs up to allowed maximum.



0 400 800 1200 1600  
feet

Prepared by Los Angeles Department of City Planning  
Graphic Services Section, September 2010:055

## **ADDITIONAL FLOOR AREA RATIO**

Additional FAR in excess of the Base FAR shall be permitted by complying with the Bonus FAR and/or Transfer FAR (TFAR) strategies described below.

### **Bonus FAR Program**

**2.4.1.** A Project with a Base FAR of 3.0 may add up to an additional 1.0 FAR (limited to the Max FAR) by providing one of the following public benefits and submitting, as part of the Administrative Clearance Application, a Bonus FAR Plan on a form prescribed by the Director of Planning.

**a.** A Project may add 10 square feet of floor area (limited to the Max FAR) for each square foot of additional publicly accessible open space in excess of the required 15 percent.

**b.** A Project may add 20 square feet of floor area for each square foot of area provided for a community facility (including access and loading/unloading).

**c.** Public benefits may be provided on the same site as the Project or on a site within the Specific Plan Area.

**d.** The owner or owners of the property which is the recipient of the Bonus FAR area shall record an agreement in the Office of the County Recorder of Los Angeles County, as a covenant running with the land for the benefit of the City of Los Angeles, providing that such owner or owners shall continue to provide said public benefit (or a substitute benefit approved by the Director) so long as the building or use the public benefit is intended to serve is maintained.

**2.4.2.** Receiving Sites that participate in the Bonus FAR program may also participate in the TFAR Program.

### **Transfer of FAR (TFAR) Program**

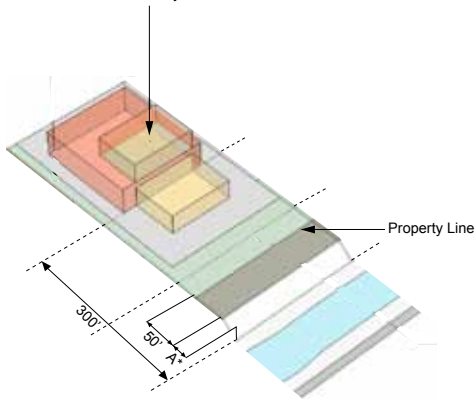
**2.5.1.** An existing parcel which has a total FAR that is less than the Base FAR may transfer its Unused FAR to a Receiver Site that is located within the same district.

**2.5.2.** An existing parcel within the Greenway District which has a total FAR that is less than the Base FAR may transfer its Unused FAR to a Receiver Site that is located within the Specific Plan.

**2.5.3.** Properties within the River Buffer Area may ..... any portion of their Unused FAR to another property within the same district but may not be a Receiver Site.

**2.5.4.** Any Project that participates in the TFAR Program shall submit, as part of the Administrative Clearance Application, a TFAR Plan on a form prescribed by the Director of Planning.

Unused FAR of the area within 360 Degrees from the River or Arroyo Seco can be transferred to the area beyond 300 feet



**2.5.5** The value of the TFAR shall be determined between the participants of the transfer unless the Donor Site is owned by either the City of Los Angeles or the Los Angeles River Revitalization Corporation (Corporation) in which case the River Public Benefit Payment described below will be used to establish the value and payment method.

**2.5.6.** Any transfer approved pursuant to this Section shall be evidenced by a recorded document, signed by the owner of the Donor Site and the owner of the Receiver Site and in a form designed to run with the land and satisfactory to the City Attorney. This document shall clearly set forth the amount of Floor Area Rights transferred, restrict the allowable floor area remaining on the Donor Site, and transfer the Floor Area Rights to the Receiver Site.

**2.5.7.** A River Public Benefit Payment (Payment) shall be provided when a Project receives density from a site owned either by the City of Los Angeles or the Corporation.

**a.** A Payment may be provided by any combination of the payment of monies to the River Public Benefit Trust Fund or by the direct provision of River Public Benefits by the Applicant; provided, at least 50 percent of such Payment consists of cash.

**b.** The Payment shall equal (a) the sale price of the Receiver Site, if it has been purchased through an unrelated third-party transaction within 18 months of the date of submission of the request for approval of the Transfer, or an Appraisal, if it has not, (b) divided by the Lot Area (prior to any dedications) of the Receiver Site, (c) further divided by the Base Floor Area Ratio Factor, (d) multiplied by 40 percent, and (e) further multiplied by the number of square feet of Floor Area Rights to be transferred to the Receiver Site.

Example: If Receiver Site with a Lot Area of 50,000 square feet (before any dedications) was purchased for \$2,500,000 (through an unrelated third-party transaction within 18 months of the date of submission of the request for approval of the Transfer), the River Public Benefit Payment under a Transfer Plan transferring 25,000 square feet of Floor Area Rights would equal: (a) \$2,500,000 (the purchase price), (b) divided by 50,000 (the Lot Area of the Receiver Site), (c) divided by the base FAR, for example, 3 (the Floor Area Ratio Factor), (d) multiplied by 40 percent, and (e) multiplied by 25,000 (the number of square feet of Floor Area Rights to be transferred) equals \$166,666.67 (or \$6.66 for each square foot of transferred Floor Area Rights).

(a)	Sale Price of Receiver Site (or appraisal)	\$2,500,000	\$2,500,000
(b)	Divided by Lot Area of Receiver Site	50,000	\$50 / SF
(c)	Divided by Floor Area Ratio	3.0	\$16.66 / SF
(d)	Multiplied by 40%	.40	\$6.66 / SF
(e)	Multiplied by SF of transferred FAR	25,000	<b>\$166,666.67</b>



c. For sites owned by the Corporation, the Corporation shall receive 100 percent of the cash portion of the Payment, and for sites owned by the City of Los Angeles, the Corporation shall receive 50 percent of the cash portion of the Payment. The Payment may be used by the Corporation for any purpose which the Corporation is authorized to undertake. The cash payment may be used to fund the operating costs of the Corporation.

d. The non-cash portion of the Payment, which shall not exceed 50 percent of the overall Payment, shall be provided as set forth in the Transfer Plan to the satisfaction of the Director.

**Payments.** Any Payment shall be provided as set forth in the Transfer Plan and as set forth below in this Subsection:

**2.6.1.** If the Transfer Plan specifies a single-phase Project on the Receiver Site, then the owner of the Receiver Site shall pay the Payment on or before the earlier of (a) the issuance of the building permit for the Project or (b) 24 months after the final approval of the Transfer and the expiration of any appeals or appeals periods.

**2.6.2.** If the approved Transfer Plan specifies a multi-phased Project on the Receiver Site, then the owner of the Receiver Site may elect to pay the Payment in any one of the three manners set forth below:

a. In total for all phases of the Project, on or before the earlier of (i) the issuance of the building permit for the first phase of the Project or (ii) 24 months after the final approval of the Transfer and the expiration of any appeals or appeals period for all phases of the Project;

b. Incrementally by each phase of the Project, proportionate to the Floor Area Rights utilized in each such phase, on or before the issuance of the building permit for each such phase, with the amount of each payment being subject to payment indexing in accordance with an executed agreement between the owner of the Receiver Site and the Corporation; or

c. Incrementally by each phase of the Project, proportionate to the Floor Area Rights utilized in each such phase, on or before the issuance of the building permit for each such phase, with the amount of each payment being recalculated as of the date that the building permit for each phase is issued in accordance with an Appraisal establishing the fair market value of the Receiver Site within six months prior to the issuance of the building permit for such phase.

#### **River Public Benefit Trust Fund**

**2.7.1.** Except as set forth below, funds held in the River Public Benefit Trust Fund (exclusive of funds paid to the Corporation) shall be disbursed:

a. As determined by a committee comprised of one representative from each of the following: the City Council Office for the City Council District in which the Receiver Site is located, and the Chair of the Ad Hoc Committee on the Los Angeles River (unless they are the same), the City Engineer, the Mayor's Office, the Chief Administrative Officer, the Chief Legislative Analyst, the Department of City Planning, the Community Redevelopment Agency (CRA/LA), and the Corporation in accordance with the procedure previously established for the Public

Benefit Trust Fund,

**b.** Within five years after receipt, and

**c.** For use on projects or programs providing a Public Benefit, as set forth in this Section, and consistent with the Los Angeles River Revitalization Master Plan. The above notwithstanding, the Corporation shall, as noted above, receive 50 percent of cash contributions to the River Benefit Trust Fund as set forth in Section 2.5.8, which funds may be used for any purpose the Corporation may legally perform.

**2.7.2.** The City shall establish an accounting of all River Public Benefit Payments. The accountings shall be transmitted annually to the Corporation, CRA/LA, and Commission for their review.

**2.7.3.** The records shall be available for public inspection.



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# 3

## Massing and Street Wall



### PURPOSE

- To provide spatial and proportional standards that reinforce the street as “a large public outdoor room.”
- To emphasize the public realm-streets and public spaces-more so than individual buildings.
- To ensure that development is designed with a pedestrian orientation.



## GUIDELINES

The guidelines included here are provided as suggestions and shall have no bearing towards a projects compliance with this Plan.

Treatment of the required setback area will vary depending upon the use for which the ground floor is primarily designed:

**G.3.1.a.** Adjacent to retail, the setback, if any, shall be primarily hardscape and may be used for outdoor dining and other commercial activities.



Zero setback with ground floor retail.

**G.3.1.b.** Adjacent to live-work, office, or industrial space, the setback area shall include a little landscaping, which may be in pots or raised planters.

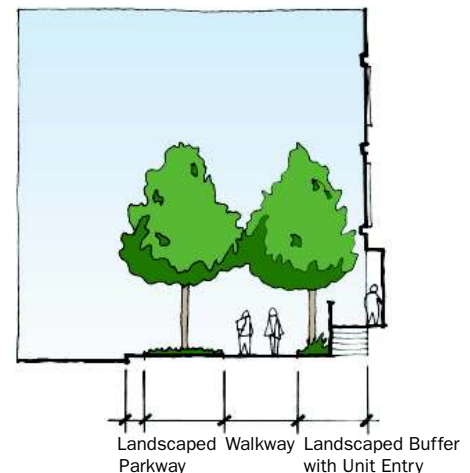
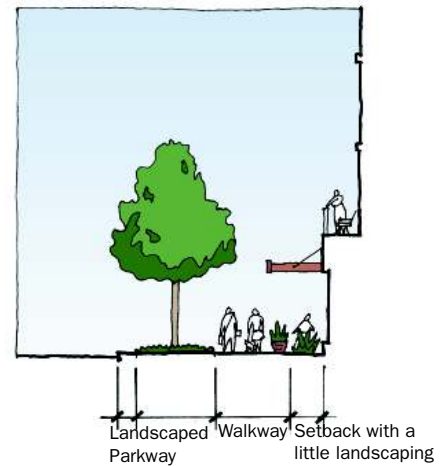
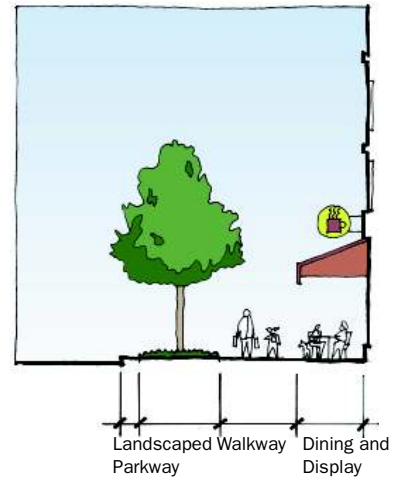


A small setback with a little landscaping next to professional office or live-work space.

**G.3.1.c.** Adjacent to ground-floor residential units with individual entries on the street the setback area should be primarily landscaped and may include walkways, porches, raised planters, other solid walls up to 3 feet above sidewalk elevation, and transparent fences (e.g. wrought iron, tubular steel, glass) up to a height of 5 feet above sidewalk elevation.



Housing with front yards and secondary entrances along the sidewalk.



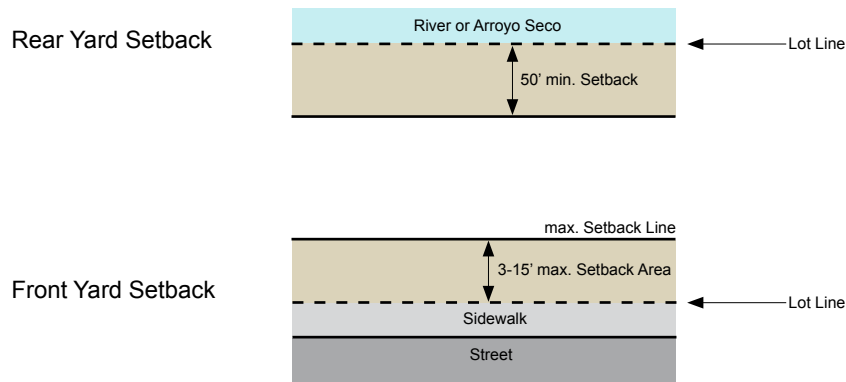
## REGULATIONS

### Setbacks

**3.1.1** The building setback along any lot line that abuts a street, flood control channel, rail corridor, or an adjacent side rear lot shall be as defined in the Building Setback Table below. The setback area for the building portion adjacent to the front lot line shall be further governed by the building's ground floor use.

**Building Setback Table**

Setback	Greenways	Urban Village	Urban Innovation	Urban Center
Front Yard/Ground Floor Use				
- Retail	N/A	0'-5' max	0'-10' max	0'-3' max
- Professional Office/Live Work	N/A	0'-10' max	0'-15' max	0'-5' max
- Industrial	N/A	0'-10' max	0'-15' max	0'-10' max
- Residential with Individual Entries on Street	N/A	0'-15' max	N/A	0'-10' max
Alley, Side or Rear Yard	0' min	0' min	0' min	0' min
River or Arroyo Seco	50' min	50' min	50' min	50' min
Rail Tracks	30' min	30' min	30' min	30' min



**3.1.2** The ground floor street wall (including entries and display windows) may be set back farther than the specified range, provided that structural columns and building walls above the ground floor are located within the specified range, as illustrated in the photographs below.



The Bradbury Building's columns and upper story walls are within a foot of the back of the required sidewalk, while entrances and display windows are setback a few feet.



Similarly, columns are at the property line, while the facade is set back a few feet.

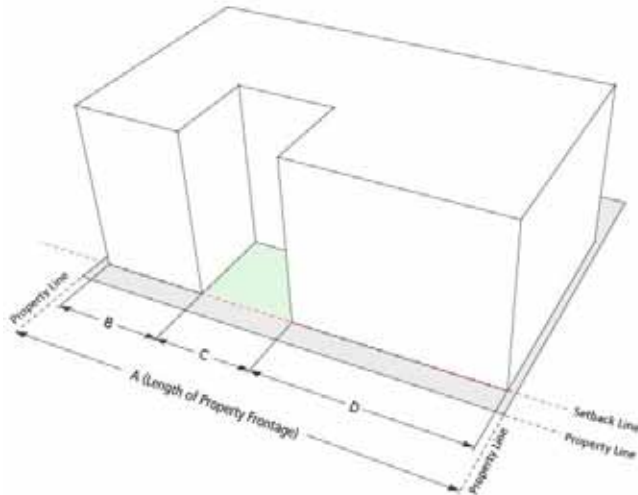
**Street Wall.** Design Street Walls to define the street and to provide a comfortable scale for pedestrians. Breaks in the street wall should be limited to those necessary to accommodate pedestrian pass-throughs, public plazas, entry forecourts, permitted vehicular access driveways, and hotel drop-offs.

**3.2.1.** A minimum percentage of the Street Wall shall be set along the setback line pursuant to the Building Street Wall at Setback Line Table and as illustrated in the Building Frontage Requirement Figure.

**Building Street Wall at Setback Line**

Minimum Percent of Building Street Wall at Setback	D1 Greenways	D4 Urban Village	D5 Urban Innovation	D6 Urban Center
Facing River or Arroyo Seco	NA	0%	0%	0%
Secondary Modified Street	NA	85%	80%	90%
Collector Modified Street	NA	75%	70%	80%
Local Modified Street	NA	65%	60%	70%

**Building Frontage Requirement Figure.**

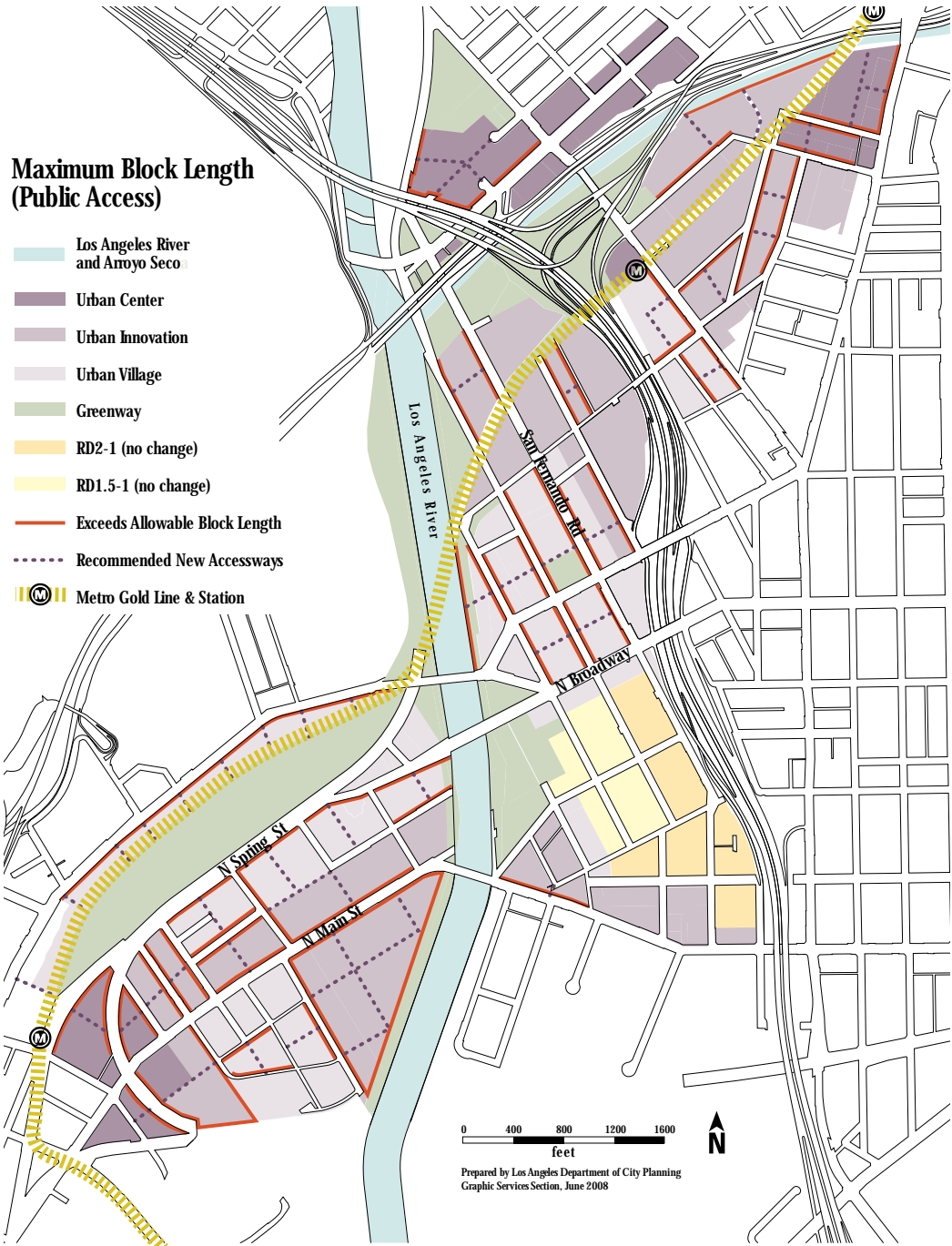


The street wall is largely defined by individual building massing

% of Building at Setback Line = (B+D) / A

**Block Length.** Break up long blocks to facilitate mobility. See the Block Length Map below for identification of blocks that are known to exceed the block length.

- 3.3.1. The block length in the three urban districts shall not exceed:
- Urban Village 450 linear feet.
  - Urban Innovation 600 linear feet.
  - Urban Center 500 feet.





**3.3.2.** A Projects that is in a block that is longer than the allowable block distances as defined on the previous page in 3.3.1 and that includes a lot with more than 300 feet of street frontage shall provide a through passageway that extends from the street to the nearest public right-of-way.

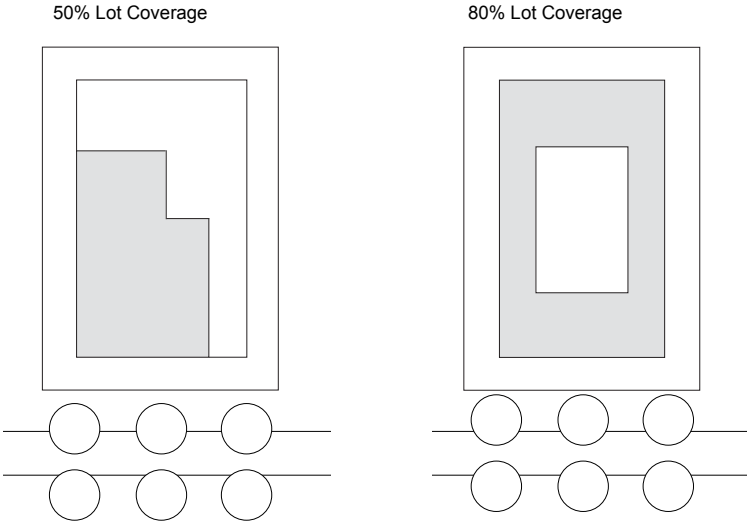
- a. The owner or owners of said lot on which a passageway shall be provided shall record an agreement in the Office of the County Recorder of Los Angeles, as a covenant running with the land for the benefit of the City of Los Angeles, providing that such owner or owners shall continue to provide said paseo as a publicly accessible pedestrian passageway so long as the building or use of the passageway is intended is maintained.
- b. Such a passageway shall permit unlimited 24 hour public access to pedestrians, bicyclists, and emergency vehicles.
- c. Design standards for the passageway can be found in Chapter 05 On-Site Open Space.
- d. The land area designated for the passageway may contribute to the 15 percent open space requirement defined in Chapter 05.

**Maximum Lot Coverage.** Design the building to provide for light, air, open space and stormwater infiltration.

**3.4.1.** Projects shall limit the percentage of building footprint relative to the overall site area as defined in Maximum Lot Coverage Table.

**Maximum Lot Coverage Table**

<b>Max Lot Coverage</b>	<b>Greenways</b>	<b>Urban Village</b>	<b>Urban Innovation</b>	<b>Urban Center</b>
Maximum Buildable Lot Coverage	25%	85%	85%	85%
Maximum Buildable Lot Coverage for projects within 300 linear feet of the bottom of adjacent edge of the River or Arroyo (River Buffer Area)	25%	50%	50%	50%



Examples of Lot Coverage (in gray) illustrating 50% and 80% lot coverage.

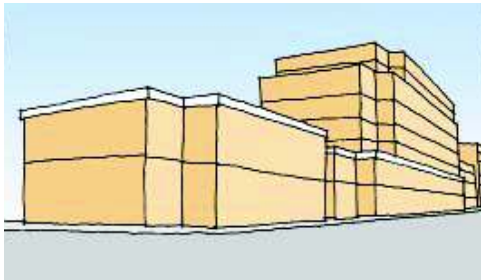


Examples of building overhang that does not interfere with street tree growth.

**Massing and Projections.** Design building massing to reinforce the street wall with well-scaled elements or structures that are sensitive to the neighborhood context. Sculpting a building's massing can also help avoid big bulky structures, and provide more visual variety than monotony. It is the well-balanced variety of building massing and textures of shadow, light, and materials that in total adds to the richness of the environment.

**3.5.1.** Break the facade of large projects into a series of appropriately scaled buildings so that no building shall be more than 300 feet in length at the base.

**3.5.2.** A building may cantilever over the sidewalk above a height of 40 feet in order to accommodate street trees at ground level.



Large half to full block projects should be massed to form a collection of appropriately scaled buildings that provide cohesion on a block

**Height**

**3.6.1.** 90 percent of a Street Wall shall comply with the minimum height as defined in the Building Height Map.

**3.6.2.** The Street Wall shall not exceed the maximum height established in the Building Height Map.

**3.6.3.** That portion of a building that exceeds the maximum Street Wall height, shall be located no closer than 20 feet from the Street Wall line.

**3.6.4.** There is no overall maximum height limit although height may be constrained by other conditions described herein.

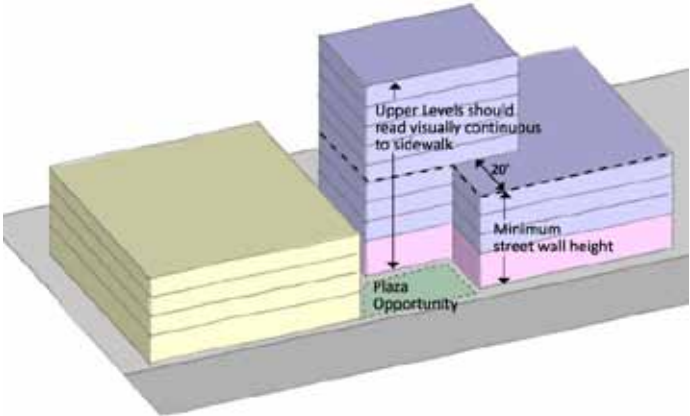
**3.6.5.** Parapet walls and other guard rails utilized to enclose roof terraces, gardens, or green roofs shall be permitted to exceed the maximum allowable height by no more than 42 inches.



3 - story street wall










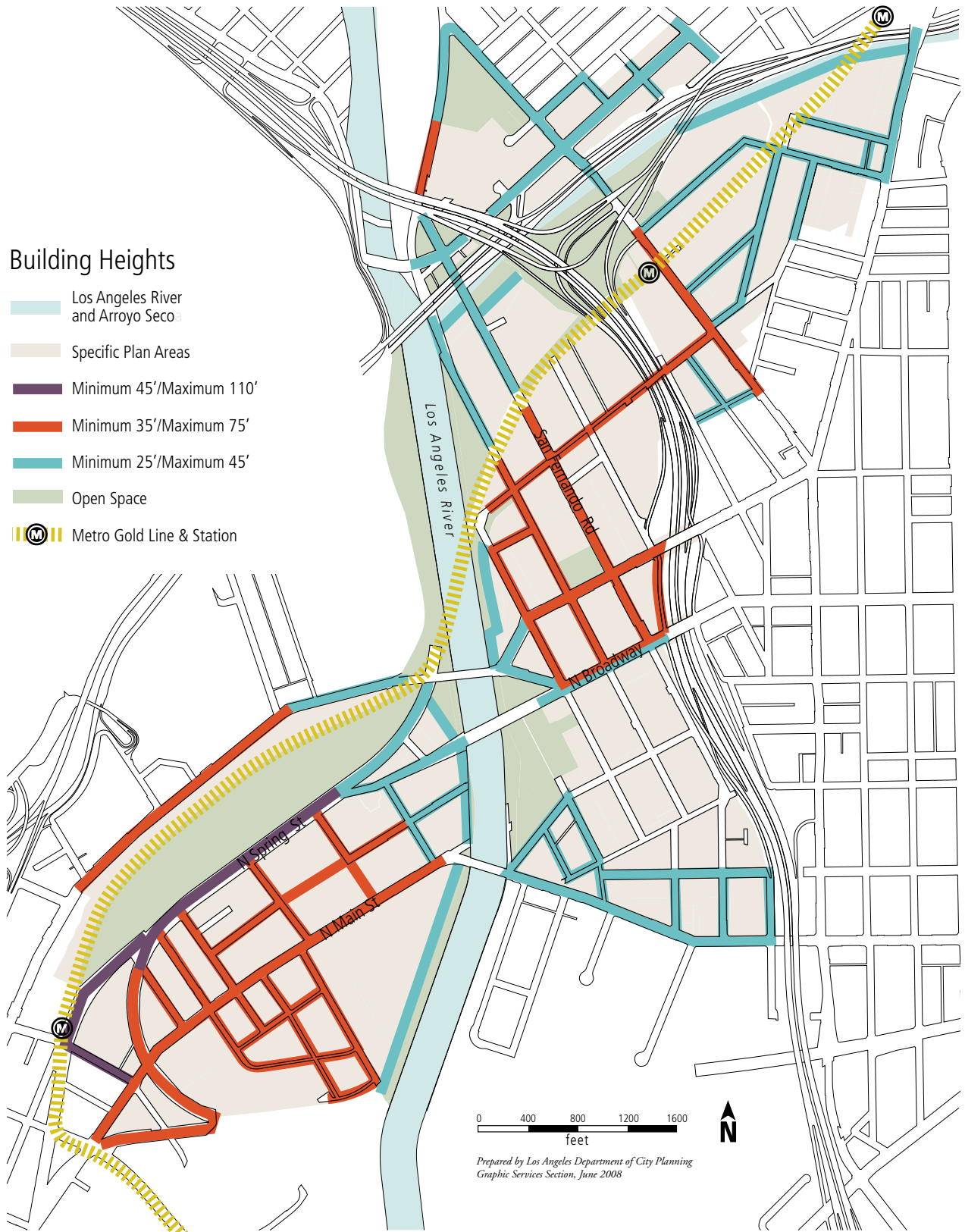
4 - story street wall



6 - and 7 - story street wall  
Walls above the ground floor that step back less than 15 feet from the ground floor street wall are part of the street wall, as illustrated above.

## Building Heights

-  Los Angeles River and Arroyo Seco
-  Specific Plan Areas
-  Minimum 45'/Maximum 110'
-  Minimum 35'/Maximum 75'
-  Minimum 25'/Maximum 45'
-  Open Space
-  Metro Gold Line & Station

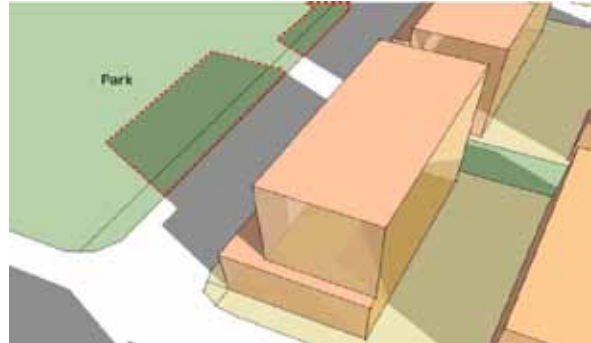
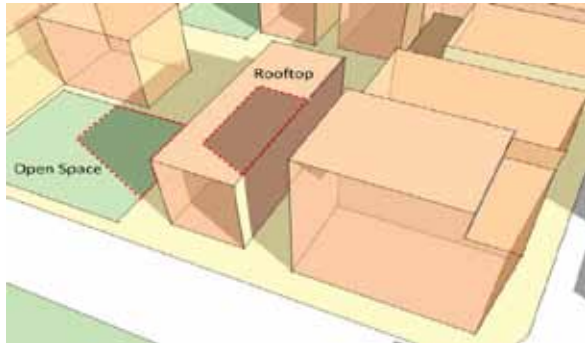


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Prepared by Los Angeles Department of City Planning  
Graphic Services Section, June 2008



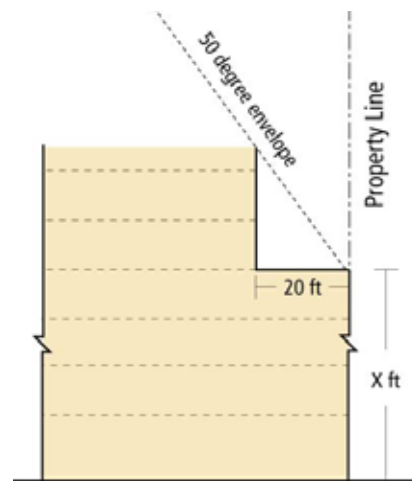
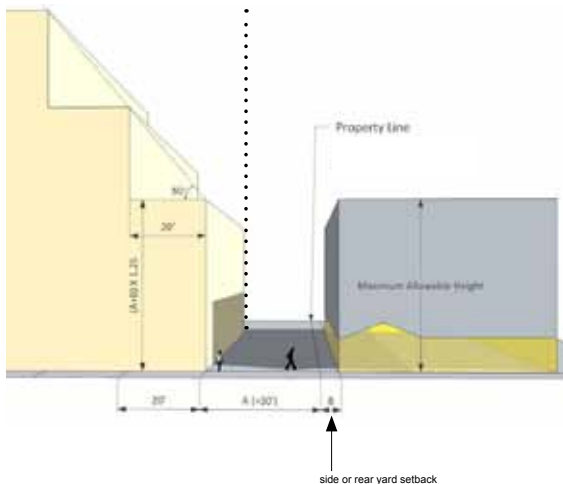
**3.6.6.** Building height and massing shall be designed such that there is no more than 1.5 hours of shadow projection on any parks, open spaces, and/or rooftop areas of abutting properties between 10am and 2pm on December 21.



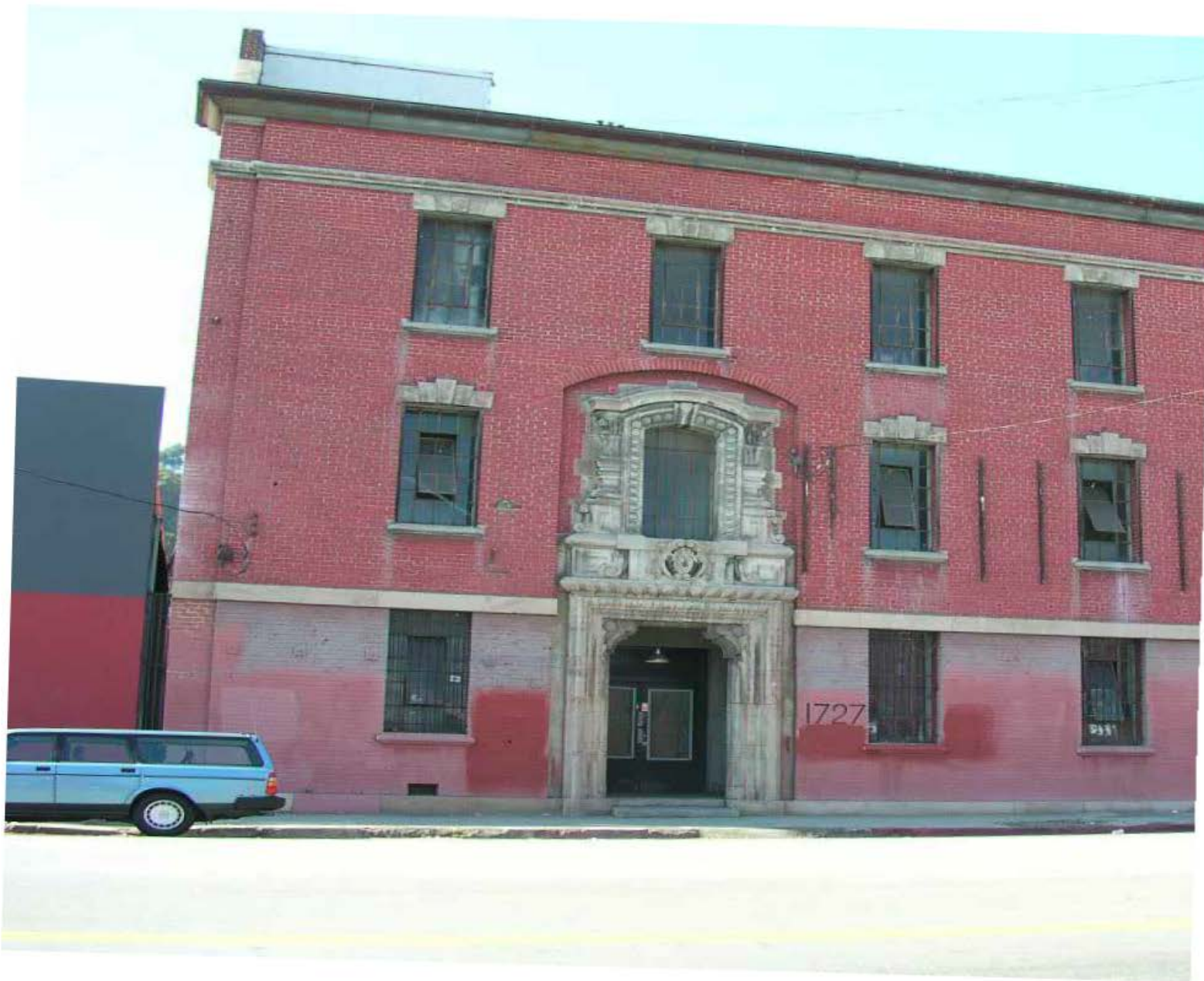
**Buffers.** Respect the smaller scale of adjacent low-density buildings.

**3.71** Projects located adjacent to the RD 1.5 and 2 zones in Sub-Area 2 or other low-density residential uses outside but immediately abutting the CASP boundaries shall:

- a. Provide an open space buffer of no less than 30 feet between the edge of the building and the property line of the low density use; and,
- b. At the buffer line and for a distance of 20 feet back from the buffer line, no building shall exceed a height of 125 percent of the buffer distance plus the side or rear yard setback (B) required by the zoning of the abutting property as illustrated below.
- c. Additional height is permitted within a 50 degree envelope as illustrated below.







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# 4

## Architectural Detail



### PURPOSE

- Maximize the advantage of the area's moderate climate by emphasizing the public realm-streets and public spaces-more so than individual buildings.
- Promote pedestrian-scaled architecture along the street.
- Promote fine-grained and well-articulated development while enabling desired development intensities to be achieved.



## GUIDELINES

The guidelines included here are provided as suggestions and shall have no bearing towards a projects compliance with this Plan.

### Materials

After establishing a building's overall massing and vertical and horizontal variation, it is important to develop a building's visual character at the level of material choices and detailing. The interplay of materials, windows, and other elements should support the larger design objectives articulated by the architect.

**G.4.1.a.** Buildings shall aim for a "timeless design" and employ sustainable materials and careful detailing that have proven longevity.

**G.4.1.b.** The material palette should provide variety and reinforce massing and changes in the horizontal or vertical plane.

**G.4.1.c.** Building details should reinforce the architect's design intentions and help set a standard of quality to guide the built results.

**G.4.1.d** Reflective materials or other sources of glare (like polished metal surfaces) shall be designed or screened to not impact views nor result in measurable heat gain upon surrounding windows either within or adjacent to a project.

### Layers

To provide visual variety and depth, layer the building skin and provide a variety of textures that bear a direct relationship to the building's massing and structural elements

**G.4.2.a.** The skin should reinforce the integrity of the design concept and the building's structural elements, and not appear as surface pastiche.

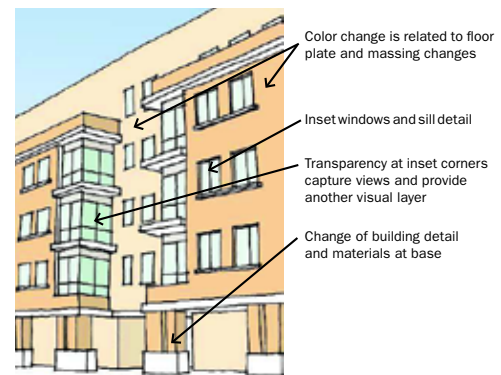
**G.4.2.b.** Layering can also be achieved through extension of two adjacent building planes that are extended from the primary façade to provide a modern sculptural composition.

### Color.

**G.4.3.a.** Design the color palette for a building to reinforce building identity and complement changes in the horizontal or vertical plane.



Layering with two adjacent planes that extend from the primary facade forming a modern composition.





### Windows and Doors

Provide high-performance, well-detailed windows and doors that add to the depth and scale of the building's façade.

**G.4.4.a.** Window placement, size, material, and style should help define a building's architectural style and integrity.

**G.4.4.b.** In buildings other than curtain wall buildings, windows shall be recessed (set back) from the exterior building wall, except where inappropriate to the building's architectural style. Generally, the required recess may not be accomplished by the use of plant-ons around the window.

**G.4.4.c.** Windows and doors shall be well-detailed where they meet the exterior wall to provide adequate weather protection and to create a shadow line.

**G.4.4.d.** Buildings with multiple uses shall provide a main entrance that reads differently from an entrance to a retail storefront, restaurant, commercial, and industrial use.

**G.4.4.e.** Entries shall be highly visible, well lit and avoid nooks, alcoves, and insets to reduce places where individuals might hide and limit potential security concerns.

**G.4.4.f.** Pedestrian entries shall include overhead protection that is integral to the architecture of the building such as canopies, awnings, or overhangs.



Windows should be well-detailed and have a recessed depth.

### Vertical Articulation

Both classical and modern buildings can exhibit basic principles of visual order in the vertical plane - often with a distinct base (street and pedestrian lower levels), a middle (core mid-section, and often consistent multiple floors of a mid-to high rise building), and a top (the upper level that distinguishes a building and defines how it "meets the sky"). Modern or contemporary building designs often layer this principle with more variation and syncopation to create interesting architectural compositions.

**G.4.5.a.** An identifiable break should be provided between the building's ground floors and upper floors designed for office, residential, or other uses. This break may include a change in material, change in fenestration pattern, or similar means.

**G.4.5.b.** Ground floors of buildings should have a different architectural treatment than the upper floors, and feature high quality materials that add scale, texture, and variety at the pedestrian level.

**G.4.5.c.** Street facing facades should incorporate a minimum of two continuous horizontal details refined to the scale of 24 inches or less within the first 10 feet of the building wall, measured vertically from street level.



Good examples of vertical variation from the street level base of lofts, to the middle, and at the top where the building meets the sky with a thin overhang.



Good examples of an identifiable break between ground level and the upper floors.

**REGULATIONS**

**Entrance**

Orient buildings to the street to promote sidewalk activity and reinforce the pedestrian environment along the sidewalk.

**4.1.1.** Primary entrances shall be located on a public street or on a courtyard, plaza, or paseo that is connected to and visible from a public street.

**4.1.2.** At least one functional pedestrian building entrance, which may be either a building or tenant/resident entrance, shall be provided every 75 feet, on average along all street frontages except on Local Industrial Modified Streets.

**4.1.3.** The primary entrance to each street or sidewalk-level tenant space that has its frontage along a public street shall be provided from that street.

**4.1.4.** The primary entrance to each street-level tenant that does not have its frontage along a public street shall be provided from a pedestrian paseo, courtyard, or plaza, which is connected to a public street.

**4.1.5.** Primary entrances shall not be permitted from a parking area.





Good example of individual unit entry several feet above the sidewalk with porch and windows that look onto the street.

### Residential Entries

**4.2.1.** If a residential unit's individual entry along the street is the unit's primary entry, it must be located at the same elevation as the sidewalk.

**4.2.2.** If a residential unit's individual entry along the street is a secondary entry, the entry and any private outdoor space for the unit may be several (but not more than 5) steps above the sidewalk elevation.

**4.2.3.** Ground floor residential units with individual entries shall include windows on the ground floor that look out onto the street.



### Ground Floor Frontage Uses

**4.3.1.** At least 75 percent of the ground floor frontage of a building located on a Retail Street identified on the Active Streets Map shall be designed specifically for and occupied by retail and community serving uses.

**4.3.2.** At least 50 percent of the ground floor frontage of a building located on an Active Street shall be designed to accommodate the following uses: retail, cultural, professional office, live/work units, residential units with individual entries along the street, and/or other active space such as recreation and meeting rooms, lobby or sales areas, or common rooms.



**4.3.3.** At least 25 percent of the ground floor frontage of a building located on an Active Industrial Street shall be designed to accommodate the following uses: lobby or sales areas, retail, professional office, and/or other active space such as meeting rooms.

**4.3.4.** The owner or owners of said lot on which the ground-floor uses are to be provided shall record an agreement in the Office of the County Recorder of Los Angeles County, as a covenant running with the land for the benefit of the City of Los Angeles, providing that such owner or owners shall continue to provide said ground-floor uses as long as the building or use the ground-floor uses are intended to serve is maintained.



**4.3.5.** All Projects shall provide information about local transit service at a primary entry point to the site or building. The information shall be prominently displayed and shall include phone numbers for transit, para transit, and taxis as well as brochures and maps for local bus and rail service.



## Ground Floor Transparency

**4.4.1.** Along Retail Streets transparent wall openings, such as storefront windows and doors, shall comprise at least 50 percent of a building's street level façade that is between 2 feet to 8 feet from the ground.

**4.4.2.** Along Active Streets and Paseos, transparent wall openings, such as storefront windows and doors shall comprise at least 35 percent of the street level façade that is between 2 feet to 8 feet from the ground.

**4.4.3.** An exception shall be made for older structures that are being renovated if the transparency requirement would render the building structurally infeasible or would compromise the historical integrity or original character of the building.

### Ground Floor Facade.

Vary the horizontal plane of a building to provide visual interest and enrich the pedestrian experience, while contributing to the quality and definition of the Street Wall.

**4.5.1.** To avoid blank walls that would detract from the experience and appearance of an active streetscape there shall be no blank walls (without doors or windows) longer than 50 feet along sidewalks on Active or Retail Streets. Walls with public art installations such as murals shall be exempt.

### Ground Floor Retail

**4.6.1.** Any and all ground floor retail space shall be located either along the Street Wall or along a courtyard or plaza, provided the retail frontage is not more than 60 feet from the back of sidewalk and is visible from the sidewalk.

**4.6.2.** Ground floor retail space shall be provided to a depth of at least 25 feet from the front façade and shall include an average 14 feet - 20 feet floor to ceiling height.

**4.6.3.** Where Retail Streets intersect other streets, the ground floor retail space shall wrap the corner onto the other streets.

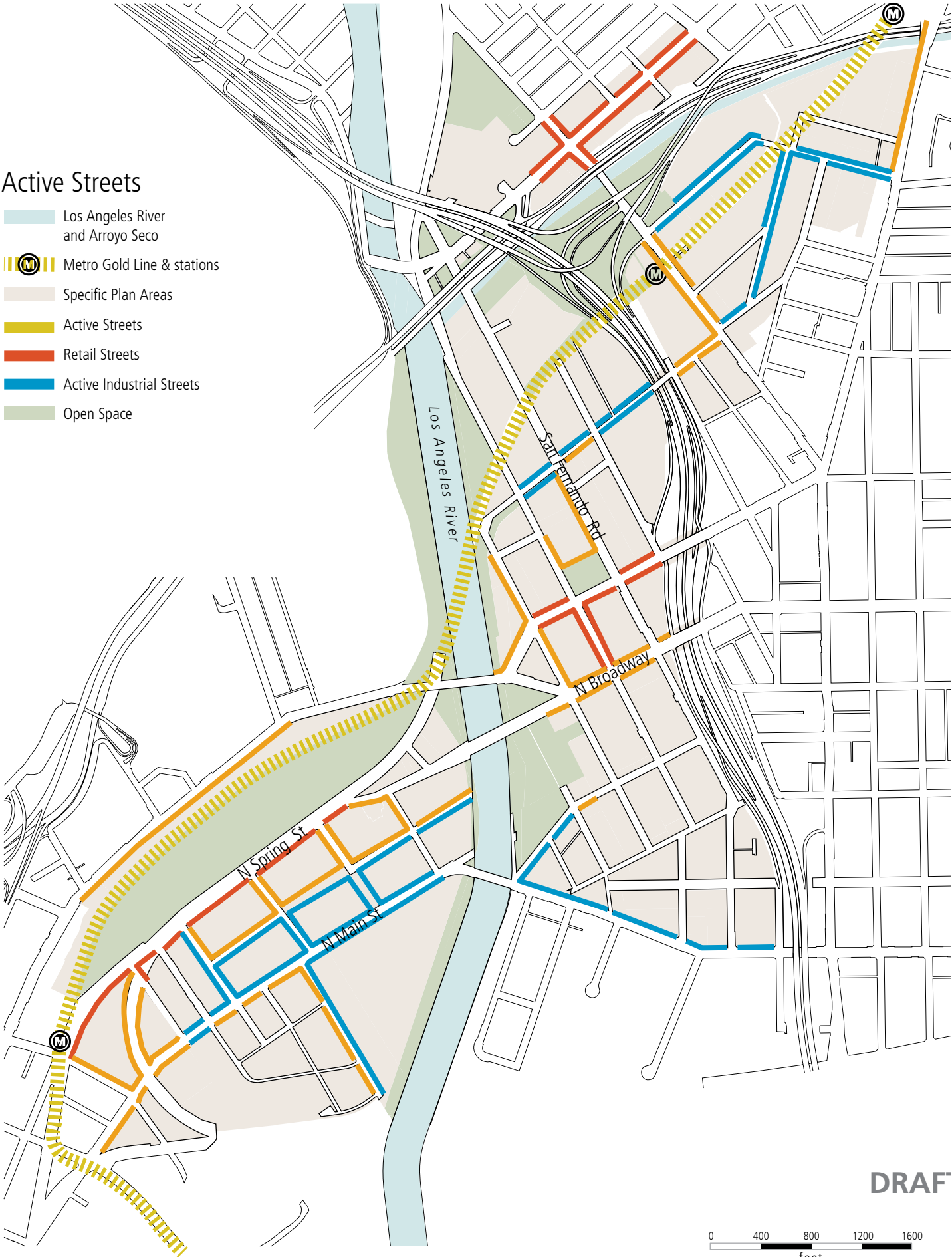


Bad examples of building facades that provide little to no visual relief and too much blank surface.



# Active Streets

-  Los Angeles River and Arroyo Seco
-  Metro Gold Line & stations
-  Specific Plan Areas
-  Active Streets
-  Retail Streets
-  Active Industrial Streets
-  Open Space



**DRAFT**



Prepared by the City of Los Angeles Planning Department • Graphics Services Section • Fall 2011

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## **Windows**

Window design can further contribute to the energy efficiency of a building.

**4.7.1.** To reduce interior heat gain and improve energy performance the window to wall ratio (exclusive of the ground floor) shall not exceed 40 percent on the E, W, SW, NW, SE, and NE facades unless an alternative façade design can demonstrate through building specific analysis that it provides the same or greater reduction in cooling loads of the building.

**4.7.2.** All windows on the aforementioned facades shall be shaded by 1' fins or overhangs or other architectural feature that provides the equivalent shading value unless an alternative design solution can be demonstrated through building specific analysis that it provides the same or greater reduction in cooling loads of the building.

## **Glazing**

Incorporate glazing that contributes to a warm, inviting environment while also reducing bird collisions by minimizing the reflection of surrounding habitat or sky.

**4.8.1.** All ground-floor window and door glazing shall be transparent and have a 0-10 percent reflectivity rating.

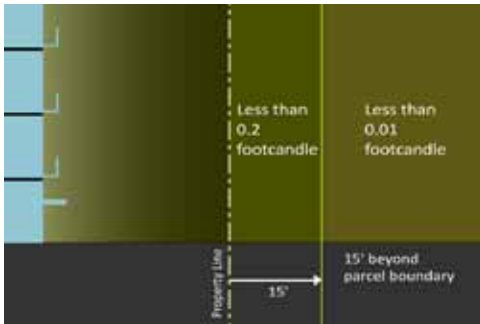
**4.8.2.** Glazing on the upper floors shall include one or more of the following: 0-10 percent reflectivity, etching, sandblasted patterns, fretting, or low-e patterning, shading devices, screen and other barriers to reduce birds' access to glass, and/or angle the glass between 20-40 degrees from vertical.



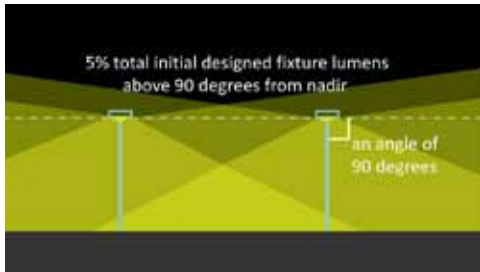
### Exterior Lighting

Provide well-designed, energy efficient, architectural and landscape lighting that contributes to a safe and inviting atmosphere without casting light into the night sky, adjacent properties, or sensitive habitat areas. All exterior lighting (building, landscape, and security) shall be integrated with the building design and should be of a character and scale that relates to the pedestrian and accentuates major architectural and special landscape features.

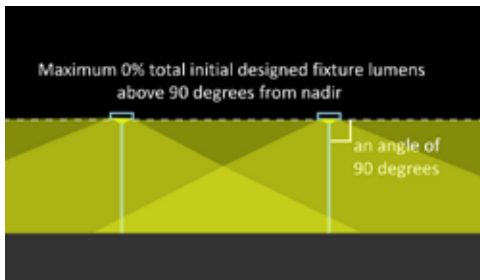
Light levels shall be measured with a photoelectric photometer, following the standard spectral luminous efficiency curve adopted by the International Commission on Illumination.



**4.9.1.** All projects in the Urban Center, Innovation, and Village Districts shall design all site and building mounted lighting such that it produces a maximum initial illuminance value no greater than 0.20 horizontal and vertical foot candles at the site boundary and no greater than 0.01 horizontal foot candles 15 feet beyond the site. No more than 5.0 percent of the total initial designed lumens shall be emitted at an angle of 90 degrees or higher from nadir (straight down).



**4.9.2.** All projects in the Greenway District shall design all site and building mounted lighting such that it produces a maximum initial illuminance value no greater than 0.01 horizontal and vertical foot candles at the site boundary and beyond. None of the total initial designed lumens shall be emitted at an angle of 90 degrees or higher from nadir (straight down).



**4.9.3.** Provide lighting along all vehicular access ways and pedestrian walkways.



**4.9.4.** All low pressure sodium, high pressure sodium, metal halide, fluorescent, quartz, incandescent greater than 60 watts, mercury vapor, and halogen fixtures shall be fully shielded in such a manner as to preclude light pollution or light trespass on any of the following; an abutting residential land use district; a lot zoned for residential use; and public right of way, park, or open space.

**4.9.5.** Lighting (exterior building and landscape) shall be directed away from properties and roadways, and shielded as necessary. In particular no lighting shall be directed at the window of a residential unit either within or adjacent to a project.



The cinema complex in Branford, Connecticut, before and after its lights were fitted with full cutoff shields to comply with the town's new regulations. The change kept the ground well lit while dramatically reducing glare and light trespass onto neighboring property.



#### **4.9.6. Exemptions**

The following outdoor lighting fixtures and activities are exempt from the requirements of this section:

- a. Fixtures producing light directly by the combustion of fossil fuels, such as kerosene lanterns or gas lamps.
- b. All neon, argon or krypton outdoor lighting fixtures.
- c. Emergency lighting operated by a public utility or agency during the course of repairing or replacing damaged facilities.
- d. Emergency lighting and fixtures necessary to conduct rescue operations, provide emergency medical treatment, or address any other emergency situation.
- e. Lighting fixtures within five feet of an entrance or exit door and/or alcove of a dwelling unit, not exceeding a height of eight feet and a wattage not exceeding 75 watts provided there is no light pollution, or light trespass, or when the lighting fixtures are regulated by a motion detector.
- f. Internally illuminated signs.
- g. Holiday lighting fixtures or displays.
- h. Architectural lighting whether it is freestanding or attached to a building which does not exceed an intensity of 60 watts.
- i. Pedestrian lighting which does not have an intensity greater than 60 watts.
- j. Vertical lighting for property displayed U.S. And State of California flags which does not exceed an intensity of 140 watts.

### **Minimizing Impacts on Neighbors**

In an urban setting such as the Plan area where many projects are viewed directly from adjacent properties where visitors, residents, and employees have clear site lines to roof and back-of house functions, it is important that new projects respect neighboring properties, and that the major mechanical systems, trash and recycling, antennas, glare lighting, and reflective materials are designed to limit adverse impacts.

**4.10.1.** Mechanical units shall be either screened from public view or the equipment itself shall be integrated into the architectural design of the building.

**4.10.2.** Ventilation intakes/exhausts shall be located at least 20 feet vertically and horizontally from a sidewalk and air flow shall be directed away from the public realm.

**4.10.3.** Recycling and trash facilities shall be screened from public view.

**4.10.4.** Exterior trash enclosures shall:

**a.** be designed to complement the primary building with a wall height that exceeds the disposal unit it is designed to contain by 18 inches,

**b.** have a solid roof to deter birds and to block views from adjacent properties,

**c.** shall be comprised of solid metal doors that accommodate a lock and shall remain closed when not in use, and

**d.** shall not be constructed of chain link or wood.



### Ground Floor Utilitarian Uses

Do not locate utilitarian uses along valuable street frontage areas.

**4.11.1.** “Back of house” uses shall not be located within the first 20 feet depth of the ground floor Street Wall.

**4.11.2.** Electrical transformers, mechanical equipment, water meters and other equipment shall not be located along the ground floor Street Wall unless screened from public view.

**4.11.3.** Electrical transformers, mechanical equipment, other equipment, enclosed stairs, storage spaces, blank walls, and other elements that are not pedestrian-oriented shall not be located within 100 feet of a corner.



Examples of poor equipment location choices. A primary opening to the courtyard garden is walled off with electric meters and irrigation equipment in plain view near a building entrance.



There are always exceptions: this security grill is not retractable, but could be approved given its aesthetic contribution.

### Security Grills and Roll-Down Doors and Windows

Balance the need for security doors and windows with the need to create an attractive, inviting environment.

**4.12.1.** Exterior roll-down doors and security grills are not permitted unless they are designed to be 75 percent transparent (open), or retractable and designed to be fully screened from view during business hours.

**4.12.2.** Any ground-level retail storefront windows must be kept open and visible (unshuttered) at night. A covenant shall be recorded binding future owners to comply with this provision.

**4.12.3.** Windows with security features shall not block out more than 30 percent of the natural light to the interior, and shall be designed as an architectural feature compatible with the building’s style.



Interior grills that are more than 75% open are less visible during non business hours and easier to screen from view during business hours.



Awnings can be used to conceal existing exterior roll-down doors during business hours.





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# 5

## Parks and Open Space



### PURPOSE

- Provide inviting, safe, and permanently accessible public open space.
- Provide a park, green plaza, or square at least 1/6 acre in area within 1/6 mile walking distance of all dwelling units and business entrances.
- Increase recreational opportunities for residents, employees, and visitors.
- Provide an active open space facility (e.g. general playfields, soccer, baseball, basketball, and other sports fields) of at least one acre within 1/2 mile walk distance of all dwelling units and business entrances.
- Provide publicly accessible open spaces that may be shared and that provide pedestrian linkages throughout the Plan area.
- Provide parks and open space that minimize demand for potable water resources.
- Provide areas for community-based and local food production to minimize the environmental impacts from transporting food long distances and increase direct access to fresh foods.
- Provide a location for the establishment of a weekly farmer's market.
- Provide open space areas that provide for native habitat and facilitate the movement of local species.
- Establish a clear hierarchy of common open spaces distinguished by design and function to create a connected pedestrian realm conducive to both active and passive uses.
- Provide adequate lighting to create a park environment where residents feel safe.
- Generate visual interest by creating focal points and meeting places to enhance the area's image.

**OPEN SPACE TYPOLOGIES**

**Alleys.** Alleys provide access to service activities and while not typically the most visible of public spaces they can facilitate physical connections between traditional open spaces.



**Community Gardens.** Community Gardens provide community members with local opportunities to tend individual plots and grow their own food.



**Courtyards.** Courtyards are common open space areas of a scale and enclosure that is conducive to social interaction at a smaller scale. A courtyard is typically contained on three sides by building and/or architectural features.



On-site open space should be designed to serve a building's residents.

**Entry forecourts.** Entry forecourts announce the function and importance of primary building entrances. They should provide a clear comfortable transition between exterior and interior space. An entry forecourt is typically contained on two sides by building and/or architectural features.



**Parks.** Parks provide a wide range of passive, active recreational, and picnic opportunities for multiple users.



**Paseos.** Paseos are extensions of the street grid located on private property. As outdoor passages devoted exclusively to pedestrians, they establish clear connections between streets, plazas and courtyards, building entrances, parking, and transit facilities. A paseo is typically contained on two sides by building and/or architectural features.



Biddy Mason Park is a paseo connecting Broadway and Spring Street.





**Plazas.** Plazas are common open space areas typically amenable to larger public gatherings. They are readily accessible from the street, as well as active building uses. A courtyard is typically contained on only one side by building and/or architectural features.



**Promenade.** A public area set aside as a pedestrian walkway.



**Residential Setbacks.** Building setbacks adjacent to residential buildings provide a transition between the public and private realm, allowing residents to have private spaces with visual access to the public realm.



**Roof Terrace.** Roof terraces and gardens can augment open space and are especially encouraged in conjunction with hotels or residential uses.



**Streets.** Streets are the most public of all open spaces. Streets communicate the quality of the public environment and the care a city has for its residents.



**Trails.** Trails provide opportunities for walking and hiking without the interruption of vehicular traffic.



## DESIGN GUIDELINES

The guidelines included here are provided as suggestions and shall have no bearing towards a projects compliance with this Plan.

**G.5.1.** Use landscape elements to provide shade and other functional and aesthetic objectives.

**G.5.2.** Incorporate amenities that facilitate outdoor activities such as standing, sitting, strolling, conversing, recreation, window-shopping, and dining; provide seating for comfort; and landscaping for shade and aesthetics.

**G.5.3.** Landscape elements should establish scale and reinforce continuity between indoor and outdoor space.

**G.5.4.** Landscape elements should provide scale, texture, and color. A rich, coordinated palette of landscape elements that enhances the Project's site identity is encouraged.

**G.5.5.** Open Space areas should assist with the Project's Stormwater requirements.

**G.5.6.** Projects should integrate artwork into the Project's design. Artwork should be placed in the most accessible and visible locations.

**G.5.7.** Planting within 100 feet of the top of bank of either the River and/or the Arroyo Seco should be established in a natural arrangement.



Parks, paseos, and courtyards. These spaces allow for closer, quieter contemplation of art, and can provide playful sequential elements.



Transit Hubs. Strategically located artworks can serve as beacons to attract people to transit, and to make a commuter's wait more interesting.



Plazas. Plazas should be activated with more prominent, dynamic artwork such as large sculptures, arbors, lighting, or water features which include adequate space for people to gather and amenities to make it inviting



Facades. An artist's sculpted or surface treatment can become a visual showcase that complements the architecture



## REGULATIONS

### Quantity

**5.1.1.** All Projects in the Greenway District shall provide 85 percent of the lot area as publicly accessible open space.

**5.1.2.** All projects in the Urban Village, Urban Center, and Urban Innovation Districts shall provide 15 percent of the lot area as open space.

**a.** Projects on a lot size equal to or greater than 15,000 square feet shall develop and maintain the 15 percent open space areas as publicly accessible open space.

**b.** Projects on a lot size less than 15,000 square feet shall develop and maintain the 15 percent open space area as usable outdoor space for the Project's residents and employees.

**5.1.3.** Projects occupying more than one lot and/or block may combine the individual open space requirement of each parcel into a single open space area equal to no less than the sum of the requirement of each parcel.

**5.1.4.** In the case of a Transfer of Floor Area Rights, a Project may comply with the provisions of this section by providing the required open space on either the Recipient or the Donor Site.

**5.1.5.** Parking areas, including access aisles, and driveways shall not qualify as usable open space.

**5.1.6.** Interior passive or active recreational spaces that are directly adjacent to the outdoors have a direct physical and visual connection to the nearest public right of way and are available for use by the public shall qualify as publicly accessible open space at a rate of 2:1. (1 square foot of interior space shall contribute to 2 square feet of publicly accessible open space)

**5.1.7.** Public alleyways, paseos, or new streets that are added to a project site may contribute to the 15 percent open space requirement.

**5.1.8.** Abandoned railway right of ways included within a redeveloped site shall be integrated as outdoor space and shall contribute towards the 15 percent public open space requirement.

**Uses**

**5.2.1.** Publicly Accessible Open Spaces shall be designed to serve at least one functional use listed below that includes but is not limited to:

Basketball Courts

Bicycle Rental Center

Community amenities

Community garden space

Farmers' Market

Information or newstand kiosk (as long as it does not exceed 1.5 percent of the open space area).

Off-leash Dog Park

Open air cafe (as long as it occupies no more than 20 percent of the open space).

Picnic Seating

Soccer Field

Softball Field

Tennis Courts

Trails, Alleys, Streets, Paseos for walking and bicycling

Transit Hub Amenities

Exercise Areas, Yoga, Pilates, and Tai Chi



**Access.** Landscape elements should support an easy transition between indoors and outdoors. Building entrances, well-sited and comfortable steps, shading devices, and/or planters shall delineate these traditional spaces.

**5.3.1.** All paths of travel shall conform to the standards of the Americans with Disabilities Act (ADA).

**5.3.2.** The relationship between open space and the adjacent street as well as the public access requirements for different types of open space are defined in the table below.

**Connections and Public Access Requirements Table**

Open Space Typology	Location	Connection to Street and Los Angeles State Park (when adjacent).	Public Access	Contributes to 15% open space requirement
Alley	Street Level	Direct connection required	Required	Yes
Community Garden	Street Level	Direct connection required	Required for garden members	Yes
Courtyard	Street Level or Above Grade	Not required	Not Required	No, unless public access is permitted.
Entry Forecourt	Street Level	Direct connection required	Not Required	No, unless public access is permitted
Parks	Street Level	Direct connection required	Required	Yes
Paseo	Street level	Direct connection required	Required	Yes
Plaza	Street Level	Direct connection required	Required	Yes
Promenade	Street Level, Below or Above Grade	Direct connections required	Required	Yes
Residential Setback	Street level	Private with visual access	Not Required	No
Roof Terrace	Rooftop	Not required	Not Required	No

**5.3.4.** When Public Access and Direct Connection are required as described in the table above the open space shall:

a. Be at the same level as the public sidewalk for at least one half of its frontage and a depth of 10 feet, and may not be more than three feet above or below the street curb level.

b. Be visible from an adjoining street(s) or adjacent parks.



**Dimensions and Boundaries.** Design urban open space areas so as to create the character of outdoor rooms contained by buildings.

**5.4.1.** All open space shall have a minimum area of 1,350 square feet with no horizontal dimension less than 15 feet when measured perpendicular from any point on each of the boundaries.

**5.4.2.** Blank walls longer than 100 feet are not permitted adjacent to the open space areas.

**Seating.** Open space should include permanent and temporary seating that is placed with consideration to sun and shade, and other factors contributing to human comfort.

**5.5.1** Provide one linear foot of seating for every 500 square feet of open space area. The flat top of walls and ledges may count as seating as long as they are no less than 15 inches in depth, between 15 inches and 20 inches in height, and have smooth surfaces to ensure comfort.



Seating is an essential element in most open spaces.



**Landscape.** Areas with vegetation shall offer relief from the urban hardscape.

**5.6.1.** A minimum proportion of all open space areas shall be landscaped as described in the table below.

**Landscaped Area Table**

<b>Open Space Type</b>	<b>Min. Planted Area</b>
Community Gardens	90%
Courtyards	25%
Entry Forecourt	10%
Parks	90%
Paseos	10%
Plazas	25%
Promenade	0%
Residential Setbacks	50%
Roof Terraces	25%



**Trees.** Deciduous trees should be planted as the most effective means of providing comfortable access to sun and shade.

**5.7.1.** Install trees at a minimum of one tree per 600 square feet of open space area.

**5.7.2.** Trees must have a minimum caliper size of 4 inches at planting and have a canopy of at least 10 feet at maturity.

**5.7.3.** A permeable surface shall be maintained below each tree for a distance of 2 feet from the trunk for every 1 inch of caliper. A tree with an expected 10 inches caliper at maturity would be required to have a 20 feet radial permeable surface below it.

**Plants and Shrubs.** Design landscaped areas to reflect the Los Angeles' area Mediterranean climate. Plants that provide habitat for native bird and butterfly species are encouraged. See the Plant Selection Appendix for guidance.

**5.8.1** 75 percent of the landscaped area shall be planted with indigenous native plants and shrubs. As identified in the County's Los Angeles River Master Plan Landscaping Guidelines and Plant Palettes.

**5.8.2.** 25 percent of the landscaped area shall be planted with drought tolerant plants.

**5.8.3.** Plants with similar water requirements shall be grouped together by common Hydro-zones.

**5.8.4.** All existing exotic weedy plants identified by the California Invasive Plant Council (CAL-IPC) shall be removed. Examples include the Mexican fan palm (*Washingtonia robusta*) and fountain grass (*Pennisetum setaceum*). See [www.cal-ipc](http://www.cal-ipc) for additional information on invasive plant species and management techniques.





### **Irrigation**

**5.9.1.** Irrigation systems shall be equipped with a Weather Based Irrigation Controller such that the system does not turn on during a storm event or when the soil has moisture level sufficient to support the plant species.

**5.9.2.** Irrigation systems shall be designed to the water needs of different parts of the landscape. This is referred to as Zoned Irrigation.

**5.9.3.** Any irrigation system shall be plumbed with a purple pipe to enable a connection to a recycled or gray water system once it is available.

**5.9.4.** All irrigation systems shall be either drip or subsurface.

### **Hardscape and Materials**

**5.10.1.** Hardscape materials shall have a Solar Reflectance Index (SRI) of at least 29.

**5.10.2.** No spikes, pointed railings, or other sharp objects shall be permitted.

### **Operations and Maintenance**

Property owners are encouraged to work together to retain maintenance providers that can service contiguous properties. A number of non-profit organizations and for-profit companies either specialize in or have developed a specialized unit knowledgeable in the maintenance of native landscaping.

**5.11.1.** Open space areas shall be maintained by pruning, weeding, and the use of supplemental irrigation and supplemental mulch as necessary. See instructions and guidelines in the book, *Care and Maintenance of Southern California Native Plant Gardens* written by Bart O'Brien, Betsey Landis, and Ellen Mackey.

**5.11.2.** Tree maintenance shall be provided by an arborist certified by the International Society of Arboreal Culture.

**5.11.3.** Open space areas shall be maintained free of litter. Litter receptacles shall be provided at a ratio of at least one cubic foot for every 2,000 square feet of open space, with an additional cubic foot for every 2,000 square feet of space if outdoor eating is present.

**5.11.4.** The owner or owners of a lot on which the publicly accessible open space is to be provided and maintained shall record an agreement in the Office of the County Recorder of Los Angeles County, as a covenant running with the land for the benefit of the City of Los Angeles, providing that such owner or owners shall continue to provide and maintain said publicly accessible open space as described in Section 5 of the Plan so long as the building or use the open space is intended to serve is maintained.



## ADDITIONAL GUIDELINES

### Plazas and Courtyards

**G.5.12.1.** Plazas and courtyards are encouraged to incorporate amenities beyond the minimum required, including permanent and/or temporary seating, to facilitate their use.

### Roof Terraces

**G.5.13.1.** Roof terraces shall incorporate trees and other planting materials in permanent and temporary planters that will provide shade, reduce reflective glare, and add interest to the space.



## ADDITIONAL REGULATIONS

### Community Gardens

**5.14.1.** Community gardens shall provide fencing, watering systems, and a secure storage space.

**5.14.2.** Community gardens must have solar access of at least 4 hours of summer sun between the hours of 10am and 4pm.

**5.14.3.** The Project shall identify the parties responsible for maintaining the garden's operation.



### Park Recreational Areas

**5.15.1.** Park-Recreational areas shall be designed to the specifications of the Department of Recreation and Parks

### Paseo.

**5.16.1.** Paseos shall be designed to:

- a. Be at least 20 feet wide;
- b. Have a clear line of sight from the street to the end of the passageway, gathering place, or focal element;
- c. Be at least 50 percent open to the sky or covered with a transparent material; and,
- d. Be lined with ground floor spaces designed for retail, especially restaurants, and/or cultural uses along at least 50 percent of its frontage;



### Off-Leash Dog Park

**5.17.1.** Off-leash dog parks shall use softscaping to capture and "scrub" animal fecal matter.

**5.17.2.** Softscaping needs to be laid to a depth of at least 6" with a drainage system installed prior to installation of the material. It could also be composed of Turf Type Tall Fescue grass, a hardy grass that can withstand extreme temperature along with heavy traffic and retain its aesthetic value, and therefore hold up to the play of dogs.



## **EXCEPTIONS**

**5.18.1.** Subdivision projects subject to the requirements of LAMC Section 17.12 shall conform to the regulations stated herein with the exception that the percentage of area provided as open space shall be as defined in LAMC Section 17.12.

**5.18.2.** An exception to the native plant and drought tolerant requirement shall be made for up to 10 percent of an area for horticulture such as herbs, fruit, or vegetables.

**5.18.3.** An exception to the native plant and drought tolerant requirement shall be made for up to 100 percent of an area established as a community food garden.



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# 6

## Parking and Access



### PURPOSE

- To manage and control the parking supply and demand.
- To avoid an oversupply of parking.
- To increase pedestrian, bicycle, and transit use, and reduce vehicular trips to, through, and within the area.
- To minimize the area's parking footprint and preserve land for other productive uses.
- To reduce the cost of parking typically associated with new construction.
- To provide vehicular access from side streets or alleyways to minimize driveways along Active Streets and to maintain building continuity and avoid vehicle and pedestrian conflicts.
- To create active ground floors around the base of parking structures that are adjacent to Active Streets.
- To screen parking to provide a safe, aesthetically pleasing, and secure environment for pedestrians.
- Provide adequate signage to public parking structures to aid visitors in finding them upon arrival and getting oriented to their surroundings.



## **GUIDELINES**

The guidelines included here are provided as suggestions and shall have no bearing towards a project's compliance with this Plan.

**G.6.1.** Parking lots shall be designed to assist the Project in conforming to the Parcel Level Stormwater requirements set forth in the Conservation Chapter 7.

**G.6.2.** Integrate the design of public art, signage and lighting with the architecture of a parking structure to reinforce its unique identity

## **REGULATIONS**

**Off-Street Parking.** Encourage the use of alternate modes of transportation by reducing the availability of off-street parking.

**6.1.1.** Residential Projects or those portions of Mixed-Use Projects that are residential shall:

- a. Provide, in a publicly accessible area, one shared vehicle parking space for every 25 units,
- b. Provide designated stalls for scooters, mopeds, and motorcycles at a ratio of one space for every 25 units,
- c. Provide a minimum of one bicycle parking space or locker for every two units, and
- d. Provide a maximum of one vehicle parking space per unit, exclusive of the shared vehicle and electric charging parking spaces.

**6.1.2.** Non-Residential Projects or those portions of Mixed-Use Projects that are non-residential shall:

- a. Provide a minimum of one share or carpool space for every 25,000 square feet.
- b. Provide designated stalls for scooters, mopeds, and motorcycles at a ratio of one space for every 25,000 square feet.
- c. Provide a minimum of one bicycle space or locker for every 2,500 square feet.
- d. Provide a maximum of one vehicle parking space per 1,000 square feet, exclusive of the shared vehicle parking spaces.

**6.1.3.** Project Open Space Areas and Public Parks shall:

- a. Provide a maximum of four parking spaces per acre. There is no minimum parking requirement.
- b. Provide a minimum of two bicycle parking spaces for every 15,000 square feet of open space or park area.

**6.1.4.** All Projects shall provide vehicle charging stations for a minimum of 5 percent of vehicle parking spaces.

**6.1.5.** The owner or owners of a lot on which the publicly accessible shared parking space(s) are to be provided shall record an agreement in the Office of the County Recorder of Los Angeles County, as a covenant running with the land for the benefit of the City of Los Angeles, providing that such owner or owners shall continue to provide said parking spaces for the use of a publicly accessible shared vehicle so long as the building or use the vehicle(s) are intended to serve is maintained.

**6.1.6.** All Projects shall unbundle the cost of parking from the cost of living and employment areas, either by charging a rent or lease fee, or selling the parking space separately. The owner or owners of a lot on which the parking is to be provided shall record an agreement in the Office of the County Recorder of Los Angeles County, as a covenant running with the land for the benefit of the City of Los Angeles, providing that such owner or owners shall continue to provide said parking spaces separate from the cost of the sale or lease of the living and/or employment areas so long as the building or use they are intended to serve is maintained.

**6.1.7.** Any parking space may be used for shared parking purposes. The purchaser or lessor of a parking space may rent the space to a secondary shared user for hours and/or days when the primary user of the space is not occupying the space.

**Parking Capacity.** Avoid an oversupply of parking.

**6.2.1.** Publicly accessible parking spaces (above and beyond the Project's maximum parking limit) may be developed, sold and/or rented for either short and/or long term periods of time, at the prevailing market rate, so long as they are not designated for a single property or use. The development of said spaces (exclusive of any spaces developed within the allowable Project maximum) shall not exceed the allotted Parking Cap established for the geographic Plan Area in which the Project is located.

**6.2.2.** A Parking Cap is established for each of the five geographic Plan Areas. The cap defines the maximum number of public parking spaces, above and beyond the maximum parking spaces permitted for each project, that can be built within each of the Plan Areas. The Parking Cap for Area 1 is\_\_\_\_, Area 2\_\_\_\_, Area 3\_\_\_\_, Area 4\_\_\_\_ and Area 5 is\_\_\_\_. The boundaries of each Area is illustrated on the Parking Cap Map on the following page.

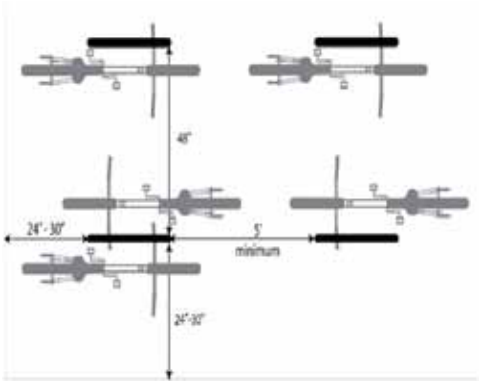
**6.2.3.** The Department of City Planning shall maintain a database of publicly accessible parking spaces within each Area.

**NOTE:**  
Maximum parking amounts will be filled in at the time of the release of the DEIR.

**Bicycle Parking Design**

**6.3.1.** Bicycle parking racks shall be provided in accordance with the amounts required in Sections 6.1.1.-6.1.3. and shall be:

- a. Located at a distance no greater than the vehicle parking spaces or 250 feet whichever is less,
- b. Located inside a parking structure or shall be located in other areas protected from the weather when automobile parking spaces are provided in a structure,
- c. Clearly marked and separated from auto parking by some form of barrier to minimize the possibility of a parked bicycle being hit by a car,
- d. Sufficient to accommodate a cycle at least six (6) feet in length and two feet wide and shall have a minimum of six feet of overhead clearance,
- e. Provided with some form of stable frame permanently anchored to a foundation to which a bicycle frame and both wheels may be conveniently secured using a chain and padlock, locker or other storage facilities which are convenient for storage and are reasonably secure from theft and vandalism,
- f. Placed no closer than 24 inches from a wall,
- g. Spaced such that there is at least 30" between the racks from side to side (measured from the center of the rack), and at least 48 inches between the racks from end to end (measured from the end points of each rack).
- h. Provide aisles, at least five feet in width to access bicycle parking spaces, and
- i. Provide display signage, which is clearly legible upon approach to all pedestrian building entrances that indicates the location of bicycle parking.



Example of bicycle parking layout

**NOTE:**  
Maximum parking amounts will be filled in at the time of the release of the DEIR.

### Maximum Parking Capacity

- Los Angeles River and Arroyo Seco
- Metro Gold Line & Station

Area Five  
00000  
Spaces

Area Four  
00000  
Spaces

Area Three  
00000  
Spaces

Area Two  
00000  
Spaces

Area One  
00000  
Spaces



Prepared by Los Angeles Department of City Planning  
Graphic Services Section, June 2008



**Parking Structure Design**

Good parking structure design can elevate the building's stature and contribute to the overall quality of the built landscape.

**6.5.1.** Parking structures shall have an external skin designed to improve the building's appearance and conceal ramps, walls, and columns. This can include heavy-gage metal screen, pre-cast concrete panels, laminated glass, or photovoltaic panels.

**6.5.2.** Parking structures that include parking at the ground level shall either line the perimeter with active uses and/or provide a low screen to block views of parked vehicle bumpers and headlights from pedestrians.

**6.5.3.** Vertical circulation cores (elevators and stairs) shall be located on the primary pedestrian corners and be highlighted architecturally so visitors can easily find and access these entry points.

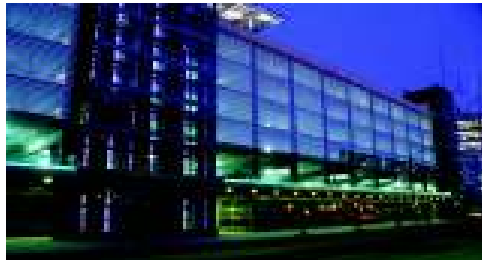
**6.5.4.** Automobiles on parking levels above the ground floor shall be screened from public view.

**6.5.5.** Parking structures that abut or are adjacent to any residential use shall:

- a. Contain solid decorative walls and/or baffles to block light and deflect noise along those sides closest to residential use,
- b. Contain solid spandrel panels at a minimum of 3 feet 6 inches in height, installed at the ramps of the structure, to minimize headlight glare,
- c. Construct garage floors and ramps using textured surfaces to minimize tire squeal,
- d. Not contain exhaust vents along sides closest to residential uses, and
- e. Not produce glaring light sources toward adjacent units.



Pre-cast panel and glass louver screening plus photovoltaic panels on top deck (upper), and metal screen with tower element marking the entry corner and vertical circulation (lower).



Examples of parking garage with a glass facade and backlighting that transcends function to provide an interesting architectural facade.

## **Parking Lot Design**

**6.6.1.** Parking lot area may not contribute towards the 15 percent Parks and Open Space Requirement.

**6.6.2.** The parking capacity of a surface parking lot shall be limited to no more than 10 percent of the maximum parking allowed for the specific project.

**6.6.3.** No at-grade parking space shall be located within the front yard.

**6.6.4.** Off-street parking facilities containing five or more spaces and not in a structure shall be effectively screened from abutting streets and lots. However, such screening shall not obstruct vehicle sight distances, entrances and exits and shall consist of one or a combination of the following:

- a.** A strip at least five feet in width of densely planted shrubs or trees which are at least two feet high at the time of planting and are of a type that may be expected to form, within three years after time of planting, a continuous, unbroken, year round visual screen, or
- b.** A wall, barrier, or fence of uniform appearance. Such wall, barrier, or fence may be opaque or perforated provided that not more than fifty percent of the face is open. The wall, barrier, or fence shall be at least four feet and not more than six feet in height.

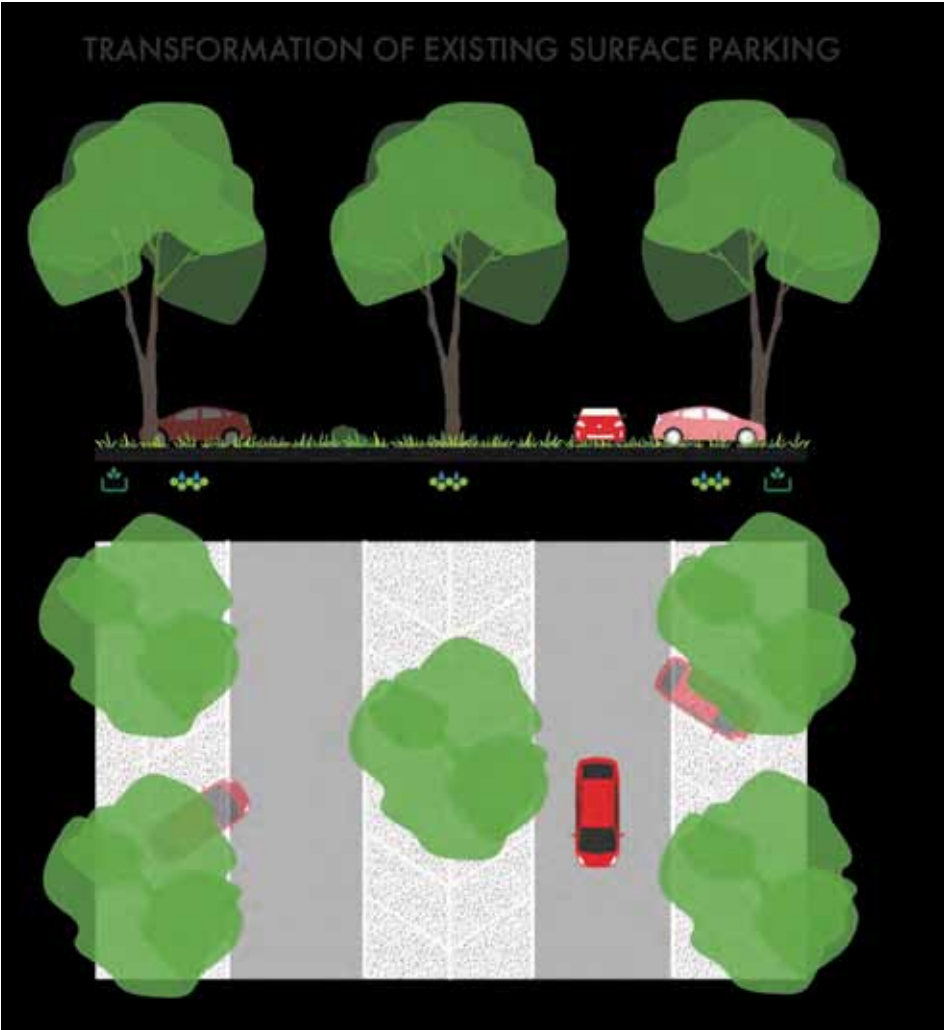
**6.6.5.** Provide any combination of the following strategies for 50 percent of the surface parking lot and driveways:

- a.** Shade within five years of occupancy,
- b.** Paving materials with a Solar Reflectance Index (SRI) of at least 29, or
- c.** Open grid pavement system.

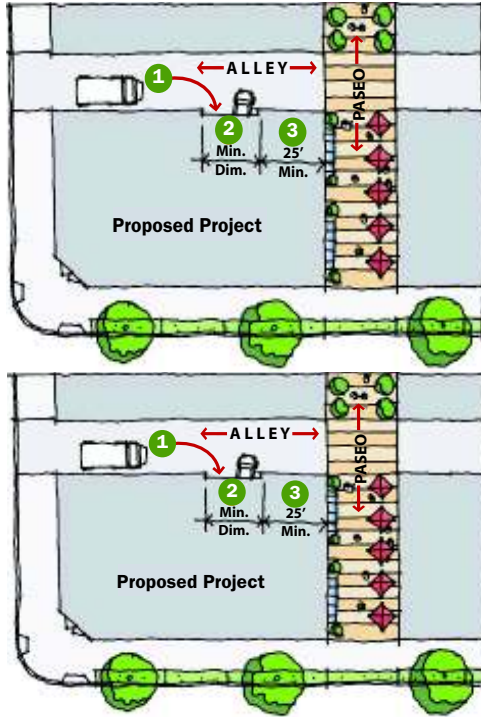
**6.6.6.** On grade, open parking facilities which contain five or more parking spaces shall be landscaped in accordance with the design regulations required by Sections 5.8.1.-5.11.3. and with the following requirements:

- a.** At least five percent of the interior area of the parking facility shall be landscaped. This does not include the perimeter planting provided for beautification or to satisfy screening requirements.
- b.** Each planting shall be at least 25 square feet in area and have no dimension less than five feet.
- c.** Each planting area shall contain at least one tree and the facility as a whole shall contain at least one tree for every ten parking spaces.
- d.** Trees used to satisfy parking lot landscaping requirements shall be a minimum of three inch caliper at planting and shall be suitable for location in parking lots.

- e. Existing trees shall be preserved wherever possible.
  - f. Existing and new trees shall be protected by bollards, high curbs or other barriers sufficient to minimize damage.
- 6.6.7.** Parking lots shall be designed to provide clear and designated paths of travel for pedestrians.
- 6.6.8.** Paths shall conform to the standards of the Americans with Disabilities Act.

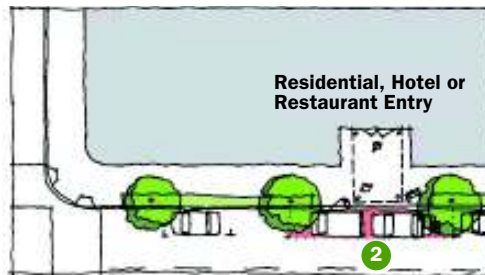


Credit: GREENFORM and connectiveissue



Vehicular Entries and Curb Cuts

1. Access to parking/services/loading shall be from the alley, and shared wherever feasible.
2. Curb cuts and parking/loading access into buildings shall be minimum width requirement by LADOT
3. Parking and loading access shall be a minimum of 25' from entrances, paseos, or outdoor gathering areas.



**Vehicular Access.** Limit the number and width of curb cuts and vehicular entries to promote street wall continuity and reduce conflicts with pedestrians.

**6.7.1.** No curb cuts are permitted from Secondary Modified and Collector Modified Streets except when no other street type is adjacent to the Project.

**6.7.2.** Local Modified, Local Modified Industrial Streets, and Alleyways shall provide the primary point of vehicular access for service and parking facilities.

**6.7.3.** Not more than two driveways shall be permitted per building, and at least 20 feet in distance should span between them.

**6.7.4.** Driveways shall not exceed the minimum width required by the Los Angeles Department of Transportation.

**6.7.5.** Parking and loading access shall be located a minimum of 25 feet from primary building entrances, pedestrian paseos, or public outdoor gathering area.

**6.7.6.** A vehicular exit from a parking structure within 5 feet of a sidewalk area, paseo, or trail shall feature a visual/audible alarm to warn pedestrians and cyclists of exiting vehicles.

**Drop-Off Zones.** Locate drop-off zones along the curb or within parking facilities to promote sidewalk/street wall continuity and reduce conflicts with pedestrians.

**6.8.1.** Drop-off Zones, including residential, hotel and restaurant drop-off areas shall be provided either:

- a. Within, or along the driveway access to the off-street parking facilities, or
- b. Alongside the required curb line where there is a full-time curbside parking lane, with no sidewalk narrowing.





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# 7

## Conservation



### PURPOSE

- Encourage preservation and rehabilitation of historic resources.
- Reduce energy demand.
- Recycle water and decrease demand for potable water.
- Reduce waste and use of new materials.
- Reduce demand on natural resources.
- Reduce impervious surfaces and improve on-site systems to control, treat, and infiltrate stormwater.
- Incorporate stormwater strategies to provide adequate treatment and flow attenuation.

## **GUIDELINES**

The guidelines included here are provided as suggestions and shall have no bearing towards a projects compliance with this Plan.

### **G.7.1. Adaptive Re-Use**

- a. Existing structures should be re-used whenever possible or be integrated into a new development to retain the architectural character of the area.
  
- b. When integrating an existing building into a new development the overall site design should consider access, parking, and landscaping so that the existing structure is cohesive with the new development.
  
- c. Existing 2-3 story structures should be studied and consideration should be given to adding additional floors to the existing structure prior to any decision to demolish the structure.
  
- d. When renovating an older structure, designers should consider ways to add fenestration to a public façade that is adjacent to a retail or active street.
  
- e. The branding and identity of the development should respect the original building.
  
- f. New signage, architectural features and lighting should complement the original building materials and be compatible with the spirit of the original building design.

### **G.7.2. Daylighting**

- a. Integrate natural lighting through clerestories, windows and skylights wherever possible to reduce the need for artificial lighting and energy consumption.
  
- b. Integrate shading systems with daylight openings to reduce heat gain in the warmest months to reducing cooling demand.

### **G.7.3. Appliances**

- a. All residential washers should be High Efficiency Clothes Washers with a water savings factor of 5.0 or less.
  
- b. All commercial washers should be High Efficiency Clothes Washers with a water savings factor of 7.5 or less.

### **G.7.4. Equipment**

All residential and non-residential spaces should use office and other miscellaneous equipment with a minimum of Energy Star rating.

## **REGULATIONS**

### **Historic Preservation**

**7.1.1.** Projects affecting designated and eligible historic resources shall comply with the Secretary of the Interior's Standards for Rehabilitation.

### **Plumbing and Plumbing Fixtures**

**7.2.1.** All faucets, not governed by City Ordinance 180822 shall not exceed 1.5 gallons per minute.

**7.2.2.** Residential shower stalls are not permitted to have more than one shower head per stall.

**7.2.3.** All residential units shall be either individually metered or sub-metered such that each unit is billed individually for its water use.

**7.2.4.** All Projects, which involve the installation of new internal rough plumbing system, shall install a dual plumbing system such that toilets and approved industrial uses can be served by recycled water.

**7.2.5.** Tankless and on-demand Water Heaters shall be installed in lieu of standard water heaters.

**7.2.6.** Conductivity Controllers or pH Conductivity Controllers shall be used when installing cooling towers.

**7.2.7.** Install a hot water on demand, re-circulation pump(s) to service any and all faucets requiring hot water.



### **Interior Lighting Design and Operations**

**7.3.1.** All non-residential buildings or portions thereof shall install lighting controls to extinguish or lower all unnecessary exterior and interior lights from 11pm to sunrise during the Spring migrations, from mid-March to early June, and the fall migration, from late August to late October.

**7.3.2.** All buildings shall schedule nightly maintenance activities to conclude before 11pm.

**7.3.3.** All non-residential buildings or portions thereof use gradual, “staggered switching” to turn on building lights at sunrise rather than instant light-up of the entire building.

**7.3.4.** All non-residential buildings or portions thereof shall install devices such as photo-sensors, infrared, and/or motion detectors to turn off lights when no occupants are present.

**7.3.5.** All commercial and industrial buildings or portions thereof shall design lighting layouts in smaller zones and avoid wholesale area illumination.

**7.3.6.** All non-residential perimeter space with a continuous depth of 20 feet shall have 20 percentage dimming ballasts and day lighting control.

**7.3.7.** All buildings shall include dimmers in lobbies, atria, and perimeter corridors for nighttime use.

## **Solar Systems and Roof Materials**

**7.4.1.** All projects shall install and maintain an onsite renewable energy generation system to provide a minimum of 20 percent of the Project's non-residential electrical needs and 10 percent of the Projects residential demand.

**7.4.2.** Use roofing materials that have a Solar Reflectance Index (SRI) equal to or greater than the values in the table below for a minimum of 75 percent of the roof surface of all buildings within the Project or install a green (vegetated) roof for at least 50 percent of the roof area of all buildings within the Project. Combinations of SRI compliant and vegetated roof can be used provided that they collectively cover 75 percent of the roof area of all buildings.

<b>Roof Type</b>	<b>Slope</b>	<b>SRI</b>
Low-Sloped Roof	< 2:12	78
Steep-Sloped Roof	> 2:12	29

## **Windows/Glazing**

See Sections 4.7.1-4.7.2. Architectural Details for regulations that will assist Projects in reducing internal heat gain.

## **Pools and Jacuzzis**

**7.5.1.** All pools shall be installed with a water-saving pool filter.

**7.5.2.** A leak detection system shall be installed on all swimming pools and Jacuzzis.

## **Stormwater Treatment and Flow Attenuation**

Projects shall be designed to manage and capture stormwater, in order of preference for infiltration, evapotranspiration, reuse, and/or high pollutant removal treatment of all of the runoff on site to the maximum extent feasible.

**7.6.1.** Residential projects of five units or more and non-residential projects shall develop and implement a Low Impact Development (LDI) plan that shall infiltrate, reuse, evapotranspire, or highly treat onsite stormwater through stormwater management techniques allowed pursuant to the Best Management Practices Handbook described in Appendix A-03.

**7.6.2.** The onsite stormwater management techniques shall be properly sized, at a minimum, to infiltrate, store for reuse, evapotranspire, or highly treat, without any runoff leaving the site to the maximum extent feasible, at least the volume of water that results from:

- a. The 85th percentile 24-hour runoff event determined as the maximized capture stormwater volume for the area using a 48 to 72-hour draw down time, from the formula recommended in Urban Runoff Quality Management, WEF Manual of Practice No. 23/ASCE Manual of Practice No. 87, (1998); or
- b. The volume of runoff based on unit basin storage water quality volume, to achieve 80 percent or more volume treatment by the method recommended in the California Stormwater Best Management Practices Handbook - Industrial/Commercial, (2003); or
- c. The volume of runoff produced from a 0.75 inch storm event.

**7.6.3** Pollutants of concern shall be prevented from leaving the development site for a water quality design storm event as defined above.

**7.6.4.** Hydro-modification impacts shall be minimized to natural drainage systems.

#### **EXCEPTIONS**

**7.8.1.** When the on-site stormwater requirements are technically infeasible, partially or fully, as defined in the LID Section of the Bureau of Sanitation's Development Best Management Handbook, the infeasibility shall be demonstrated in the submitted LID plan, shall be consistent with other City requirements, and shall be reviewed in consultation with the Department of Building and Safety.

**7.8.2.** If partial or complete onsite compliance of any type is technically infeasible, the project Site and LID Plan shall be required to comply with, at a minimum, all applicable Standard Urban Stormwater Mitigation Plan (SUSMP) requirements in order to maximize onsite compliance.







# 8

## Performance Standards

### PURPOSE

- To provide for a safe, clean, and healthy environment.
- To provide transit information.

**REGULATIONS**

**Compliance**

8.1.1. Prior to the issuance of a building permit or land use permit, the owner of the lot or lots shall execute and record a covenant and agreement, acknowledging that the owner shall implement each of the applicable regulations set forth in this Section. The covenant and agreement shall run with the land and be binding upon the owners, and any assignees, lessees, heirs, successors of the owners. The City’s right to enforce the covenant and agreement is in addition to any other remedy provided by the law.

**Air Quality**

8.2.1. All K-12 schools, residential, or residential portions of mixed-use Projects shall be located no less than the allowable minimum distances to existing industrial land uses, or industrial land uses in a new mixed-use development as defined for each industrial use by the California Air Resources Board (CARB).

8.2.2. All K-12 schools, residential, or residential portions of mixed-use Projects located less than 500 feet of a freeway shall disclose the unhealthful implications of residing within 500 feet of a freeway to residents who are purchasing or renting housing in these locations and shall implement mitigation measures to reduce exposure to air pollution.

**Maintenance and Delivery Standards**

8.3.1. All Projects shall be maintained in a clean, safe, and sanitary condition.

8.3.2. All Projects shall keep the site clear of weeds, rubbish, and all types of litter and combustible materials at all times.

8.3.3. All projects shall permit no loitering, camping, public begging, consumption of alcoholic beverages, use of illegal narcotics, or any other criminal activity on any premises.

8.3.4. All projects shall prevent standing water from accumulating anywhere on site.

8.3.5. Loading and unloading of vehicles shall occur either on site, within an alley, or a local modified, or local industrial modified street. Loading and unloading of vehicles from a Secondary street shall be permitted only when no other public right of way is adjacent to the project site.

8.3.6. Site cleaning, sweeping, trash collection, deliveries, and loading and unloading to the site are limited to the hours in the table to the right.

Hours	Greenway	Urban Village	Urban Innovation	Urban Center
Mon-Friday	6am-10pm	7am-7pm	24 Hours	6am-10pm
Sat., Sun., & Legal Holidays	8am-5pm	8am-5pm	24 Hours	8am-8pm

## Noise

**8.4.1.** Loudspeakers or public address systems are not permitted to be installed or operated within any portion of Projects in the Urban Village, Urban Innovation, or Urban Center Districts.

**8.4.2.** Loudspeakers or other public address systems are permitted in the Greenway District but their use is limited to the hours of 8am-7pm Monday-Thursday, 10am-10pm Friday-Saturday, and Noon-5pm Sundays and Legal Holidays. In any event the maximum noise level may not exceed 90 decibels on the A-weighted scale.

**8.4.3.** At the boundary line between two districts, the presumed ambient noise level of the quieter zone shall be used.

**8.4.4.** Maximum noise levels within Industrial Workspaces shall be as defined in the table below.

Noise levels between	Greenway	Urban Village	Urban Innovation	Urban Center
7am-10pm	55 dba lbn	55 dba lbn	70 dba lbn	65 dba lbn
10am-7pm	45 dba lbn	45 dba lbn	65 dba lbn	55 dba lbn

**8.4.5.** Maximum noise levels within Commercial Workplaces shall be as defined in the table below.

Noise	Greenway	Urban Village	Urban Innovation	Urban Center
7am-10pm	55 dba lbn	55 dba lbn	65 dba lbn	65 dba lbn
10am-7pm	45 dba lbn	45 dba lbn	60 dba lbn	55 dba lbn

**8.4.6.** Maximum noise levels within Residential Habitable Spaces shall be as defined in the table below.

Noise levels between	Greenway	Urban Village	Urban Innovation	Urban Center
7am-10pm	NA	50 dba lbn	50 dba lbn	50 dba lbn
10am-7pm	NA	45 dba lbn	45 dba lbn	45 dba lbn

## **Recycled Materials**

**8.5.1.** All Projects shall provide a recycling area that is clearly labeled, and easily accessible.

**8.5.2.** All Projects are required to develop a recycling program and contract for recycling pick-up if all recycled refuse is not re-used on site.

**8.5.3.** All recycled goods shall be placed or stored in Recycling Receptacles by the end of the business day and not be left in plain view on the site.

**8.5.4.** All recycling receptacles shall be kept covered, and made of durable, waterproof, rustproof, of incombustible construction, and of sufficient capacity to accommodate the materials collected.

**8.5.5.** The recycling area shall be kept free of litter, debris, spillage, bugs, rodents, odors, and other similar undesirable hazards.

**8.5.6.** Paper products and other lightweight materials shall be immediately placed into covered recycling receptacles.

**8.5.7.** All recycling receptacles and containers shall be kept in a secure location to prevent unauthorized entry and scavenging and theft of recyclable materials.

**8.5.8.** Recyclable materials, other than recyclable materials contained in reverse vending machine commodity storage bins, shall be emptied from recycling receptacles when full or every week, whichever comes first.





**Storage**

**8.6.1.** No materials or equipment shall be stored out of doors to a height greater than the height of the enclosing wall or fence.

**8.6.2.** Open air storage of merchandise or materials must be confined to a storage area completely enclosed by a solid, non-combustible wall (with self-closing gates).

**8.6.3.** Trash storage bins shall be located within a gated, covered enclosure at least six feet in height.



**Transit Information**

**8.7.1.** All Projects shall provide information about local transit service at a primary entry point to the site or building. The information shall be prominently displayed and shall include phone numbers for transit, para transit, and taxis as well as brochures and maps for local bus and rail service.



**Utilities**

**8.8.1.** All new utility lines, which directly service the lot or lots, shall be installed underground. If underground service is not available at the time the application is submitted and fees paid for plan check, then provisions should be made for future underground service to the satisfaction of the Bureau of Engineering, if determined necessary by the Department of Water and Power.

**8.8.2.** All utility boxes located within the public right-of-way shall be wrapped with a graphic image. See example to the left.

**Vibration**

**8.9.1.** Consistent with ASHRAE 200 and the LAMC Section 111.02 the Maximum Vibration Levels for Industrial, Commercial, and Residential uses shall be as defined in the table below.

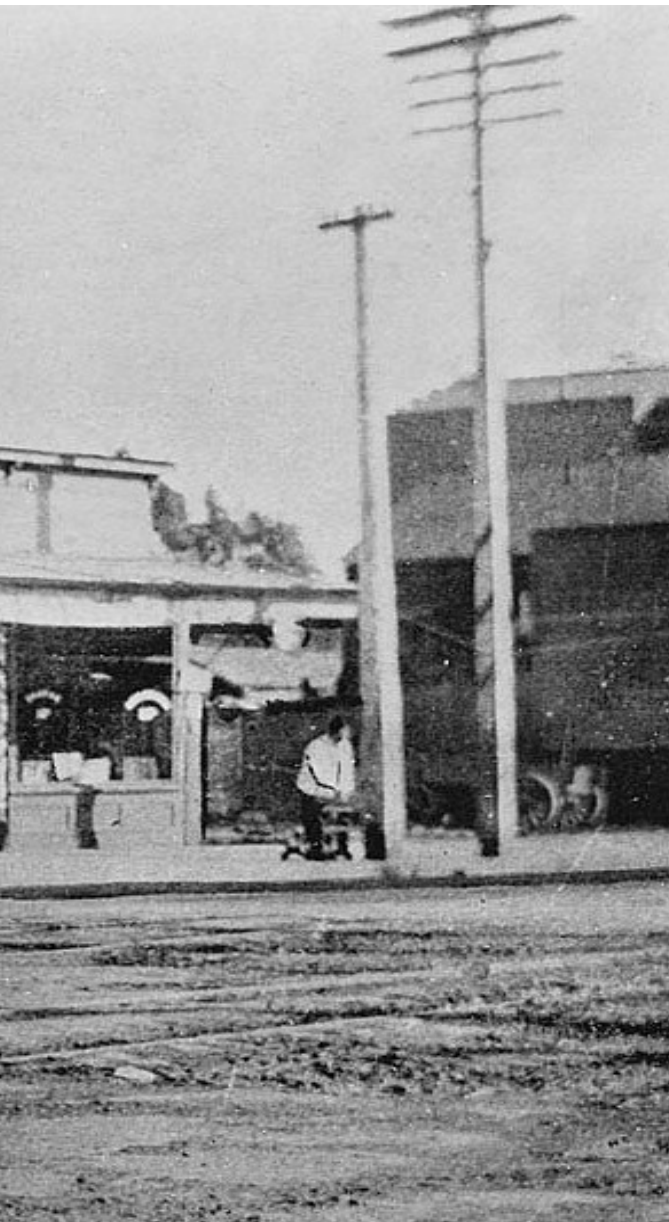
Use	Hours	Urban Village	Urban Innovation	Urban Center
Industrial	7am-10pm	16,000 pin/s	32,000 pin/s	16,000 pin/s
	10pm-7am	5,600 pin/s	32,000 pin/s	8,000 pin/s
Commercial	7am-10pm	16,000 pin/s	16,000 pin/s	16,000 pin/s
	10pm-7am	5,600 pin/s	16,000 pin/s	8,000 pin/s
Residential	7am-10pm	8,000 pin/s	8,000 pin/s	8,000 pin/s
	10pm-7am	5,600 pin/s	5,600 pin/s	5,600 pin/s



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# 9

## Signage



### PURPOSE

- To create strong building identity that is well integrated with the design of the architecture.
- To provide clear and attractive business identity.
- To attract visitors to publicly accessible open space areas.



## GUIDELINES

The guidelines included here are provided as suggestions and shall have no bearing towards a projects compliance with this Plan.

**G.9.1.a.** Signs should contribute to a lively, colorful, and exciting pedestrian atmosphere with signs and graphics that are compatible with the area.

**G.9.1.b.** Signage should be conceived as an integral part of the project design so as not to appear as an afterthought application.

**G.9.1.c.** The location, size, and appearance of building identification signs should complement the building and should be in character with the area.

**G.9.1.d.** Signs shall complement buildings with respect to style, design, materials, and colors. If illuminated, glare shall be carefully controlled, and if internally luminated, lighting sources shall be concealed.

**G.9.1.e.** Multiple signs within a Project should be related in their design approach and convey a clear hierarchy of information.

**G.9.1.f.** Signage should identify the main/visitor entrance or lobby, resident or visitor parking, community facilities, major amenities and commercial and industrial uses. These signs should be related in style and material while appropriately scaled for the intended audience.

**G.9.1.g.** Signs that hold multiple tenant information should be designed so individual tenant information is organized and clear within the visual identity of the larger campus or building.



Hierarchy of Signs. Examples of residential identity signage present at the most prominent corner. A related family or signs ranging from overall project identity to the parking garage are shown here.





Integrated Design. Examples of residential identity signage integrated into a sculptural seating and lighting element at the main entry and into an entrance canopy.

## REGULATIONS

### Prohibitions

**9.1.1.** The exposed unfinished backs and sides of all signs shall not be visible from a public right-of-way or greenway.

**9.1.2.** The following signs are prohibited; animated, blinking or scrolling signs; inflatable devices; off-site, supergraphics, pole signs, roof, and window signs.

**9.1.3.** Signs shall not obscure the architecture, windows, or window trim and molding.

**9.1.4.** No signs other than flags and banners, shall be located above the second story.

### Permitted signs

**9.2.1.** Each premise or business shall be permitted one identification sign limited to a maximum of 12 square feet in size and shall not exceed a depth of one foot. An additional sign is permitted if the premise abuts another street, alley, public parking area, park or open space. An identification sign may be located on any portion of an awning as long as the sign does not exceed the maximum allowable dimensions.

**a.** For projects that have multiple storefront tenants of similar size, all Identification signs shall be of the same type but may vary in respect to font style and color.

**9.2.2.** Building Identification signs shall be permitted one per building, and shall not exceed 16 square feet for one story-buildings. For each story above the first, the size of the sign may increase an additional four square feet. An additional Building Identification Sign shall be permitted for buildings located on a corner lot or where a second façade is visible to a publicly accessible park or open space. This secondary sign shall not exceed the size of the primary sign.

**9.2.3.** Each premise or business shall be permitted one Information sign limited to a maximum of six square feet in size and shall not exceed a depth of one foot. An additional sign of equal size is permitted if the premise abuts another street, alley, public parking area, park, or open space.

**9.2.4.** Each building, premise, or business shall be permitted one Street Address Information sign limited to a maximum of six square feet in size and shall not exceed a depth of one foot. An additional sign of equal size is permitted if the premise abuts another street, alley, public parking area, park, or open space.

**9.2.5.** Identification, Building Identification, Information, and Street



Address Information signs shall be permitted as wall, awning/canopy, or projecting signs.

**a.** Projecting signs shall not extend more than five feet beyond the property line, shall not be located lower than eight feet above the sidewalk grade or edge of roadway grade nearest the sign, and shall not extend above the top of the wall.

**b.** Wall signs that are made up of individual letters that use the wall of the building as background, the allowable sign area may be increased by 20 percent, provided there is no change in color between the background and the surrounding wall area. A wall sign shall not extend above the top of the wall of the building. Wall signs may extend no more than one foot beyond the face of the building.

**9.2.6.** Wall murals or mural signs are permitted with approval pursuant to LAMC Section 14.4.20.

**9.2.7.** One portable menu board sign may be permitted in the right-of-way for eating establishments, bakeries, florists, and similar businesses whose primary sales consist of perishable goods, provided that all of the following conditions are met:

**a.** The sign is removed at the end of each business day.

**b.** The sign's dimensions do not exceed two feet by four feet.

**c.** The sign does not interfere with pedestrian movement or wheelchair access.

**d.** The sign has a mounted base capable of keeping the sign upright in a moderate wind.

**e.** The sign is not illuminated.

**f.** The sign's permits have been secured from the appropriate City of Los Angeles departments.

**9.2.8.** Each publicly accessible park area shall be permitted one monument sign for every eight acres.

**a.** Each monument sign shall have a maximum of 75 square feet of sign face visible to the same direction of traffic and shall be limited to an overall height of eight feet above sidewalk grade or edge of roadway grade nearest the sign.

**9.2.9.** Each publicly accessible park area shall be permitted one flag or banner style sign for every acre or portion thereof.

**a.** Each flag/banner shall have a maximum of 24 square feet of sign face visible to the same direction and be affixed to a building or pole

at a height no lower than eight feet above sidewalk grade or edge of roadway grade nearest the banner.

**b.** Height of the top of the flag/banner shall be measured from the nearest sidewalk or edge of roadway grade to the top of the sign. The overall height limitation of the flag/banner shall be determined by the length of adjoining street frontage as follows:

1. 25 feet for lots having 50 feet of street frontage;
2. 35 feet for lots having more than 50 feet and less than 100 feet of street frontage; and
3. 42 feet for lots having at least 100 feet of street frontage.

**9.2.9.** Additional signs, beyond the Permitted Signs described above shall not be permitted.



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# 10

## Street Designations



### PURPOSE

- To connect the area to its neighboring communities, the City of Los Angeles, and the greater Los Angeles region through a safe, efficient, and accessible circulation network that embraces pedestrians, bicyclists, transit, truck traffic, and automobiles.
- To recognize the shared use of streets not only for moving traffic, but also as the front door to businesses that are the economic and fiscal foundation of the City and as public outdoor space for residents and workers.
- To develop an efficient yet balanced circulation system that defines different types of streets based on their transportation function and community role.
- To provide residents, employees, and visitors with a variety of transportation alternatives that result in a more efficient use of transportation resources.

## STREET TYPOLOGIES

Modified Street Standards foster a multi-modal circulation network.

Most of the existing streets within the Plan area will be designated with one of the new Modified Standards. Please refer to the Street Designation Table and Circulation Map to identify a street's new designation and its location within the street network. The Street Standards Table summarizes the dimensions and characteristics of each Modified Street type.

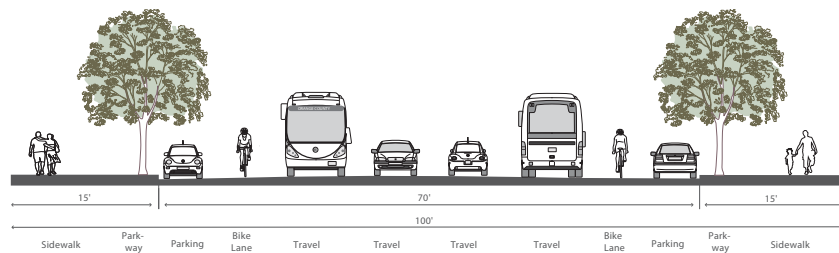
The sections below reflect the typical mid-block cross section. At the intersections turning lanes could typically be accommodated, where necessary, by removing the on-street parking.

The Modified Street Standards include:

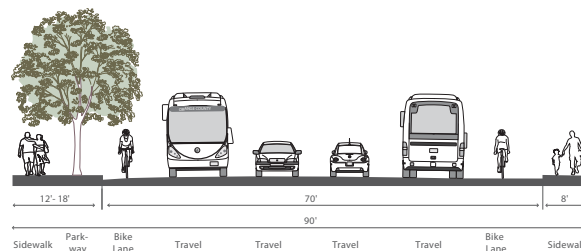
### Secondary Modified Street

A Secondary Modified Street emphasizes intra-city, multi-modal travel, and connect urban activity centers. A Secondary Modified Street has two lanes in each direction and carry a mix of local and regional traffic. Typical features include on-street parking, exclusive bicycle lanes and wide sidewalks, landscaping, and stormwater Best Management Practices (BMPs). To accommodate particular existing street characteristics or constraints there are five types (I-V) of Secondary Modified Streets.

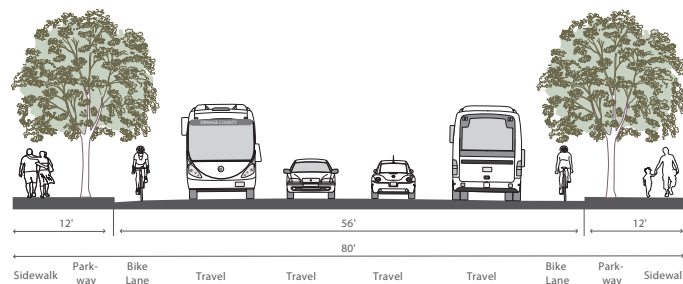
a. **Secondary Modified I Street.** The secondary Modified I Street includes all of the typical features.



b. **Secondary Modified II Street.** The Modified II Street provides on-street parking on only one side of the street.

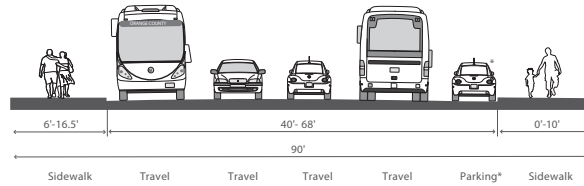


c. **Secondary Modified III Street.** No on-street parking is provided in the Modified III Street.

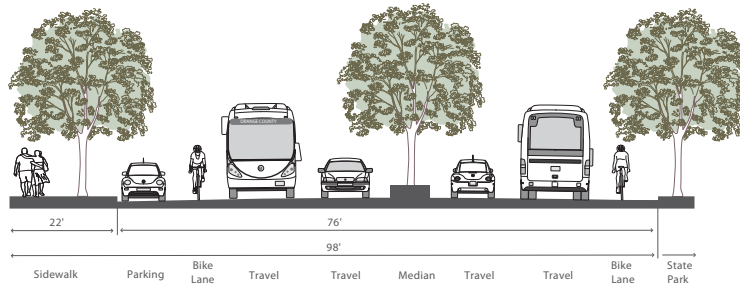




**d. Secondary Modified IV Street.** Due to roadway constraints neither on-street parking or bicycle lanes are included in the Modified IV Street and there may not always be room for a sidewalk on both sides of the street.

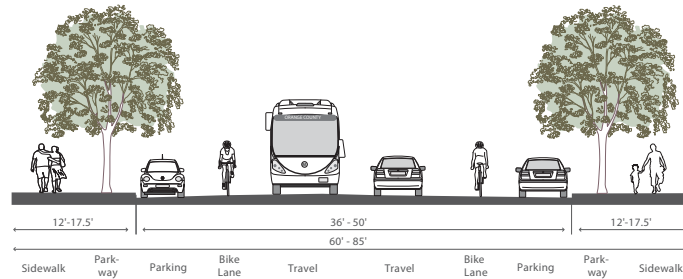


**e. Secondary Modified V Street.** The Modified V Street includes a median and parking on one side of the street.



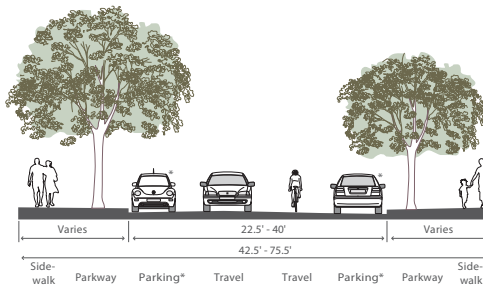
**Collector Modified Street**

A Collector Modified Street emphasizes multi-modal neighborhood travel and serves as a “Main Street” for Urban Villages and Urban Centers. A Collector Modified Street has one vehicle lane in each direction. Typical features include wide sidewalks, exclusive bicycle lanes, on-street parking, landscaping, and stormwater BMP’s.



**Local Modified Street**

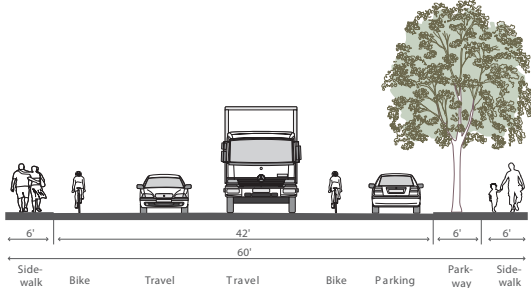
A Local Modified Street emphasizes access to individual properties and serve living or work spaces. A Local Modified Street allows for one lane in each direction and is not designed to accommodate regular bus or truck traffic. Typical features include relatively narrow cross sections, on-street parking on the south and west sides of the street, sidewalks, landscaping, and stormwater BMP’s. The north and east sides of the streets will be designed with approximately 13’ wide parkways to allow for a broader shade canopy.



\* Some roadways may have parking on only one side of the street.

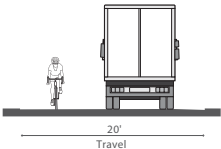
**Local Industrial Modified Street**

A Local Industrial Modified Street emphasizes truck access to industrial properties. A Local Industrial Modified Street allows for one lane in each direction and includes a bicycle lane. Typical features include limited on-street parking, generous sidewalks, landscaping, and stormwater BMPs.



**Modified Alleys**

A Modified Alley emphasizes access to individual properties, and accommodate parking access and service functions as an alternative to other streets and provide the opportunity to incorporate stormwater BMPs. It is anticipated that new alleys will be constructed as new projects are developed.
















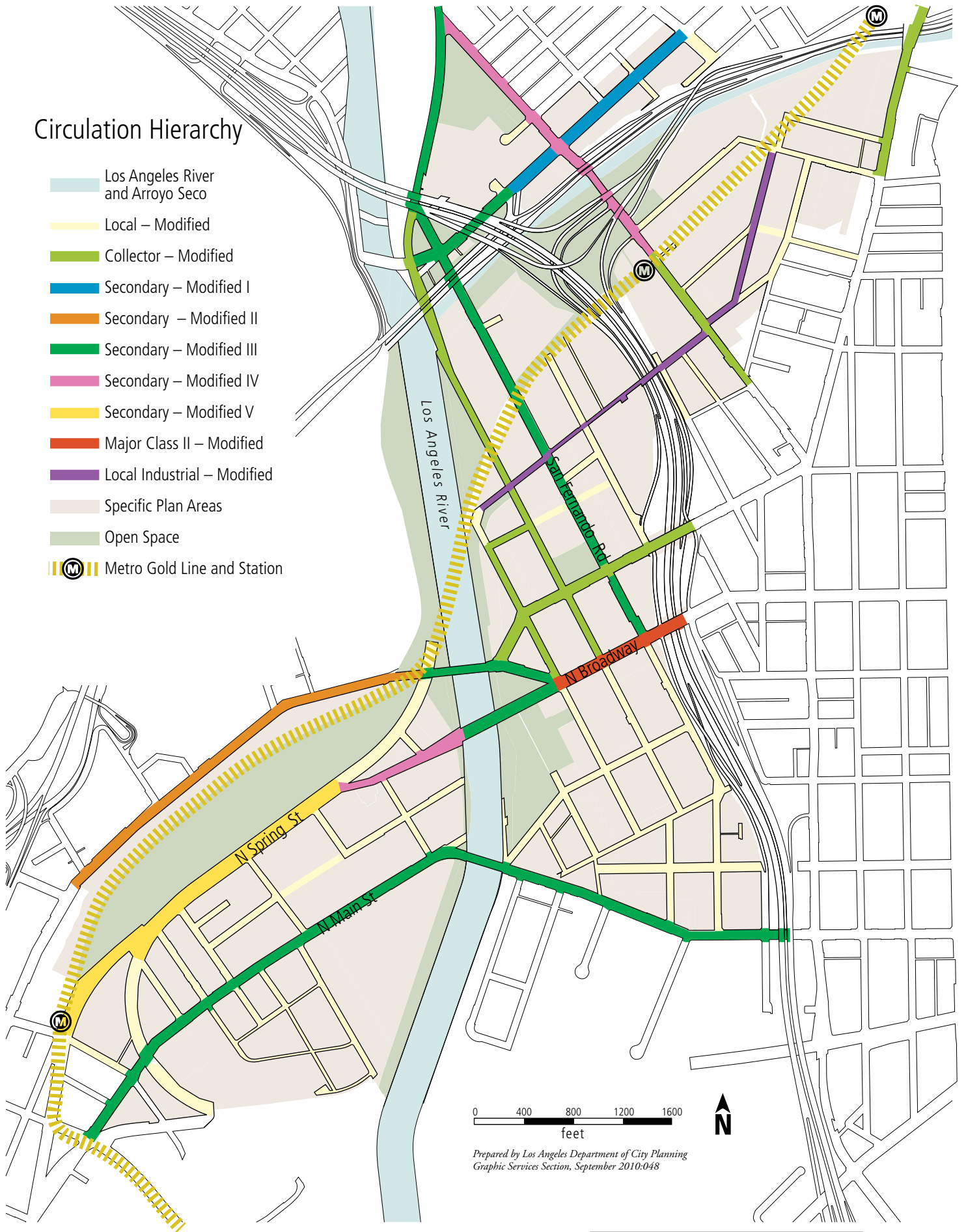
**REGULATIONS**

**Street Designations and improvements**

- 10.1.1. All major and secondary highways and all collector and local streets identified on the Circulation Hierarchy Map, on the following page, and in the Street Designation Table shall be constructed and improved in accordance with the standards adopted by the City Planning Commission insofar as such is practical and will not create an undue hardship.
- 10.1.2. Projects shall make improvements to that portion of the adjacent public right of way that parallels the property's street frontage as described in the Cornfield Arroyo Seco Streetscape Plan and consistent with the modified street standards identified herein. (A DRAFT Streetscape Plan is in progress and will be developed with community input over the next few months)
- 10.1.3. All improvements required to be made by the provisions of this subsection shall be done in accordance with the current applicable provisions of the Standards Specifications for Public Works Construction adopted by the City Council.
- 10.1.4. The City Engineer, in consultation with the General Manager of the Department of Transportation may approve and

# Circulation Hierarchy

-  Los Angeles River and Arroyo Seco
-  Local – Modified
-  Collector – Modified
-  Secondary – Modified I
-  Secondary – Modified II
-  Secondary – Modified III
-  Secondary – Modified IV
-  Secondary – Modified V
-  Major Class II – Modified
-  Local Industrial – Modified
-  Specific Plan Areas
-  Open Space
-  Metro Gold Line and Station



DRAFT

Prepared by Los Angeles Department of City Planning  
Graphic Services Section, September 2010:048

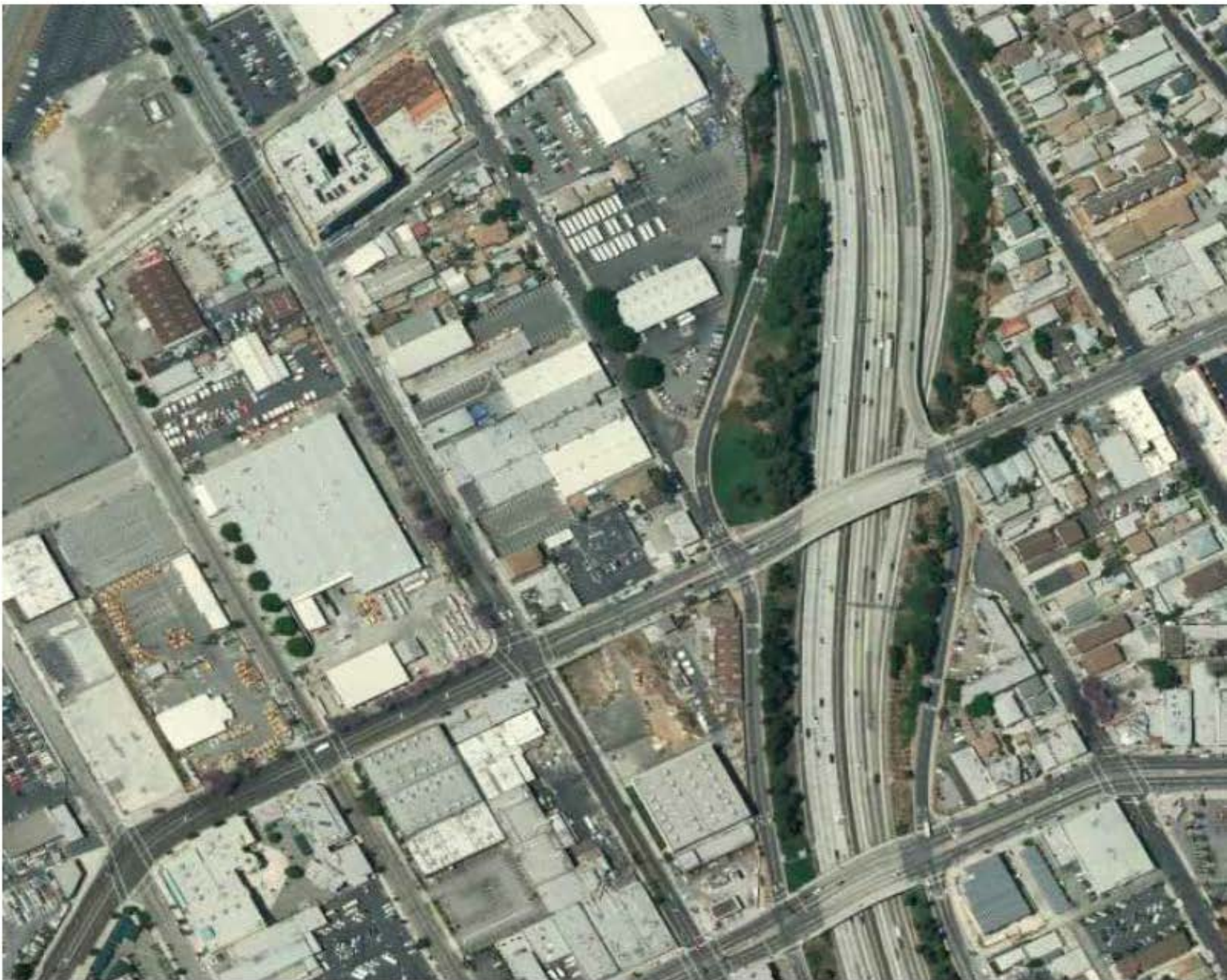
### 10.2.2. Street Designations Table

The following streets shall be designated according to the Table.

Street	Current Designation	New Designation
Albion	Local	Local - Modified
Alhambra	Local	Local – Modified
Ann (b/t Spring & Main)	Collector	Local – Modified
Ann (South of Main)	Local	Local – Modified
Artesian	Local	Local – Modified
Aurora	Local	Local – Modified
Ave 16	Local	Local – Modified
Ave 17	Local	Local – Modified
Ave 18	Local	Collector – Modified
Ave 19	Local	Collector- Modified
Ave 20 (South of Broadway)	Collector	Collector- Modified
Ave 20 (North of Broadway)	Secondary Hwy	Secondary - Modified 3
Ave 21	Local	Local- Modified
Ave 22	Local	Local- Modified
Ave 23	Local	Local - Modified
Ave 25	Local	Local - Modified
Ave 26 (North of Gold Line Bridge)	Secondary Hwy	Secondary- Modified 4
Ave 26 (South of Gold Line Bridge)	Secondary Hwy	Collector – Modified
Ave 33	Local	Local - Modified
Baker – to Aurora	Local	Local - Modified
Barranca	Local	Local - Modified
Bloom	Local	Local - Modified
Bolero	Local	Local - Modified
Broadway (West of Pasadena)	Major Hwy Class II	Secondary - Modified 2
Broadway (Pasadena to Ave. 18)	Major Hwy Class II	Secondary- Modified 2
Broadway (East of Ave. 18)	Major Hwy Class II	Major Hwy Class II
Cardinal	Local	Local - Modified
Clover	Local	Local – Modified
College	Local	Local – Modified
Darwin	Local	Local – Modified

<b>Street</b>	<b>Current Designation</b>	<b>New Designation</b>
Elmyra (North of Main)	Collector	Local – Modified
Elmyra (South of Main)	Local	Local – Modified
Figuroa (East of Ave. 22)	Major Hwy Class II	Secondary- Modified 1&3
Gibbons (West of Ave. 22)	Local	Local Industrial – Modified
Humboldt	Local	Local – Modified
Lacy	Local	Local – Modified
Lamar	Local	Local – Modified
Leon	Local	Local – Modified
Leroy	Local	Local – Modified
Livingstone	Local	Local – Modified
Llewellyn	Local	Local - Modified
Magdalena	Local	Local – Modified
Main	Secondary Hwy	Secondary – Modified 3
Messenger	Collector	Local – Modified
Moulton	Local	Local – Modified
Mozart	Local	Local – Modified
Naud	Collector Street	Local – Modified
Pasadena	Secondary Hwy	Collector- Modified
Rondout	Local	Local – Modified
San Fernando	Secondary Hwy	Secondary – Modified 3
Sotello	Collector	Local – Modified
Spring (West of Baker)	Major Hwy Class II	Secondary – Modified 5
Spring (East of Baker)	Major Hwy Class II	Secondary – Modified 3
Spring St. (Bridge)	Major Class Hwy II	Secondary – Modified 3
Weyse	Collector	Local – Modified
Wilhardt	Collector	Local – Modified





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# A-01

## Reference



### PURPOSE

- To provide general reference information and maps about adjacent community plans, community redevelopment project areas, neighborhood councils, employment assistance programs, and business improvement districts.
- To provide information and maps about existing historical, civic, cultural, and open space resources.





## COMMUNITY REDEVELOPMENT

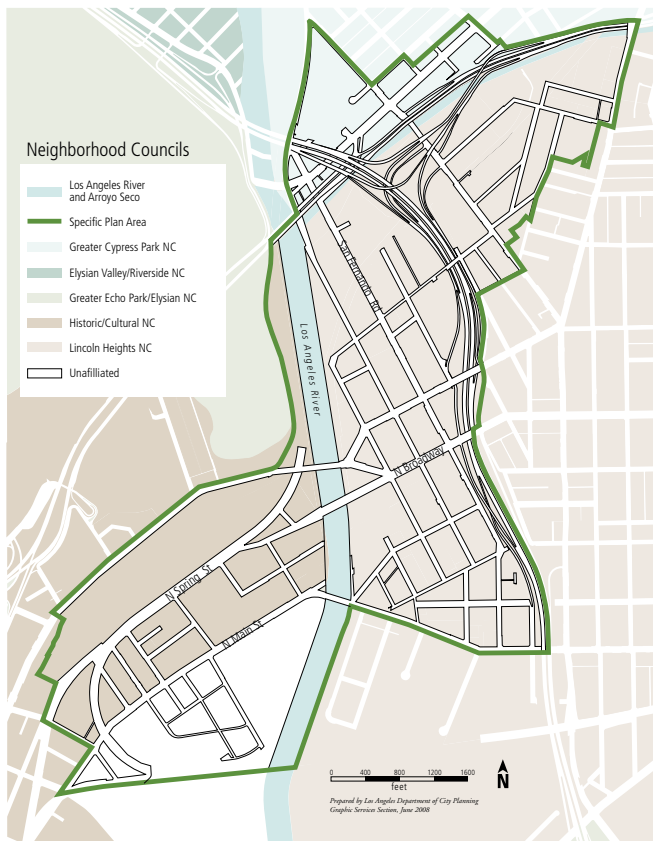
There are two Community Redevelopment Project Areas immediately adjacent to the Plan area. The Chinatown Redevelopment Project (Chinatown) borders the western edge of the Plan and the Adelante Eastside Redevelopment Project (Adelante) is adjacent to eastern portions of the Plan. The Adelante area was adopted on March 30, 1999 and includes approximately 2,200 acres. The Adelante project focuses on the preservation of industrial and commercial uses within the community. In addition, the project aims to improve local shopping areas. The Chinatown project area was adopted on January 23, 1980 and covers 303 acres. The project seeks to eliminate blight, create affordable housing and maintain the area's prominence as the focal point of commerce and culture for the Chinese population of Southern California.

Of special interest is the establishment of the Whiteside Redevelopment Project Area which lies immediately east of the map area illustrated here. The Whiteside project was initiated by the Los Angeles County Development Commission and is anticipated to be merged in the future with the Adelante project area to form a larger "Biomed Tech Focus Area." According to the County's website the goal of this City and County partnership would be to facilitate development and new job growth opportunities in the fields of biomedical research and related technology manufacturing.



## NEIGHBORHOOD COUNCILS

Since the revision of the City's Charter in 2000 over 88 Neighborhood Councils (NC) have been established throughout the City. Three distinct NC's are located within the Plan area. The Historical and Cultural NC predominates the area on the western bank. The southerly section of this area is currently not located within any of the three NC's although the William Mead Housing Project does have an active Resident Advisory Committee. The area on the east bank is largely contained with the Lincoln Heights NC with the exception of the northernmost edge that is contained within the Greater Cypress Park NC.





## EMPLOYMENT

The Los Angeles Business Assistance Program (LABAP) offers free training and technical assistance to business owners seeking to improve their organization's profitability. It also offers training to people wanting to learn how to open and operate their own business. Business owners can also obtain a variety of information at: [www.business.lacity.org/index.htm](http://www.business.lacity.org/index.htm). More information about LABAP is provided at: [http://www.lacity.org/cdd/bus\\_labap.html](http://www.lacity.org/cdd/bus_labap.html).

Portions of the Plan area include both a State Enterprise Zone as well as a Federal Empowerment Renewal Community Designation. Within these areas businesses can take advantage of State and Federal tax credits and deductions not available to businesses elsewhere. The goal of the incentives is business attraction, growth, and increased employment opportunity within economically challenged areas within the City.

Enterprise Zones assist businesses located in the zones to lower their operating costs by providing tax credits and deductions. The state offers incentives such as: hiring credits, sales & business use tax credits, and interest deductions. The City offers local incentives such as DWP rate discount waivers, sewer facility hookup payment plans, and Work Opportunity Tax Credit. Additional information is provided at: [http://www.lacity.org/cdd/bus\\_statecred.html](http://www.lacity.org/cdd/bus_statecred.html).

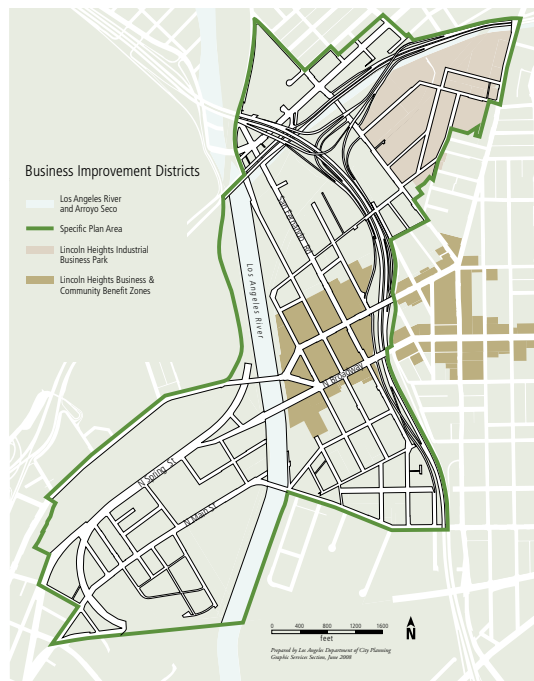
Federal Empowment Zones give residents improved access to goods and services. This Zone provides Federal Tax Credits and local incentives including: wage credits, section 179 deductions, partial exclusion of capital gains, DWP rate discount, city business tax waivers, Work Opportunity Tax Credit, and the Brownfield Tax Deduction. Additional information is provided at: [http://www.lacity.org/cdd/bus\\_fedcred.html](http://www.lacity.org/cdd/bus_fedcred.html).



## BUSINESS IMPROVEMENT DISTRICTS

Two Business Improvement Districts (BIDs) currently exist within the Plan area. The first, the Historic Lincoln Heights Industrial Zone Business District was established in April 1999 and loosely mirrors the boundaries of Area 4 with the exception that it does not include the properties west of Avenue 26. The BID imposes a fee of .02 cents per square foot of lot size on each of the property owners that provides a revenue source for the overall maintenance of sidewalks, landscaping and public rights of way within the the zone boundaries. A second BID, The Lincoln Heights Business and Community Benefit District was established on April 23, 2008 and includes Broadway Boulevard starting at the River and continuing east past the Plan area boundaries to Lincoln Park, and Pasadena Avenue from the River to Workman Street. Property owners will pay a designated fee based upon their linear frontage, lot square frontage, and building square footage. The revenues will fund services that fall into one of four categories including sidewalk operation and beautification, district identity, administration, and contingency/city fees. This BID recognizes the importance of maintaining a clean and safe environment to attract local retail customers to visit this historical “Main Street” of Lincoln Heights.

BID's can play a key role in invigorating a business community whether its focus is supporting industrial, commercial or retail employment areas. While there is currently no Business Improvement District located within subarea 1 the need for one may occur as the area redevelops in order to support the cooperative needs of the various business organizations as they respond to the changes that will occur with the redevelopment of the State Historic Park.



## HISTORICAL RESOURCES

There are a number of properties in and around the Plan area that have been designated as Historic-Cultural Monuments (HCM) by the City of Los Angeles. Two properties just west of the Plan have also been listed on the National Register of Historic Places and an application to include the Zanja Madre has been submitted to the National Register. The William Mead Housing site is listed on the California Register of Historic Places.

The Survey LA effort that is currently underway by the Department's Office of Historic Resources may potentially identify other historic resources within the Plan area.

**HCM No. 42 - San Antonio Winery.** 725-749 Lamar Street. Designated in 1966. In 1917, Santo Cambianica, an Italian immigrant, opened the San Antonio Winery near the Los Angeles River. The Winery remains the only producing winery in the City of Los Angeles.

**HCM No.82 - River Station Area.** 1231 N. Spring Street. Designated in 1971. Capitol Milling Company was one of Los Angeles' leading enterprises. They specialized in milling grains to produce flour, cereal and food. The nearby Southern Pacific Railroad allowed Capitol Milling to easily transport products nationwide.

**HCM No.156 - Fire Station No. 1.** 2230 Pasadena Avenue. Designated in 1976

**HCM No.211 - Granite Block Paving.** (Bruno Street between Alameda and N. Main St.). Designated in 1979.

**HCM No. 261 and National Register No. 2344 - Lincoln Heights Branch Library.** Opened its doors in 1916. This library has been called one of the most visually impressive buildings in Los Angeles. It is also one of the three remaining Carnegie Libraries in Los Angeles. Designers Lester H. Hibbard and H.B. Cody modeled the Lincoln Heights Branch after the Villa Papa Guilla in Rome.

**HCM No.281 - Cathedral High School.** 1253 Bishops Road. Designated in 1984.

**HCM No.384 - Department of Water and Power Building.** 2417 Daly Street. Designated in 1988.

**HHCM No.388 - Edison Electric Company (Los Angeles #3 Steam Power Plant).** 650 S. Avenue 21. Designated in 1988.

**HCM No. 396 - Federal Bank Building.** 2201 N. Broadway. Designated in 1988.

**HCM No. 442 - Albion Cottages and Milagro Market.** 1801-1813 Albion Street. Designated in 1989.

**HCM No. 587 - Lincoln Heights Jail (Los Angeles City Jail).** 401-449 N. Avenue 19. Designated in 1993.

**HCM No. 872- Raphael Junction Block/NY Suspenders Building.** 1635-37 N. Spring Street. Designated in 2007. Built in 1889 for early pioneering Los Angeles businessman Charles Raphael, the Victorian-era Flatiron style building represents one of the earliest industrial

developments in the downtown “Cornfields” area near the River and Spring Street Bridge. Significant for its unique late 19th Century triangular design and status as one of the oldest surviving buildings in this area of downtown, the Raphael Building once housed tenants of the New York Suspender Factory and California Ice Company.

**HCM No. 900 - North Spring Street Viaduct.** Constructed in 1928. Designated in 2008.

**HCM No. 901 - North Main Street Bridge.** Constructed in 1910. Designated in 2008. This bridge was the first in the series of monument LA bridges. It was the first open-spandrel arch bridge that was considered to be the longest and widest concrete bridge when opened in the state. It is also the first Beaux-Arts style bridge for LA river. It's decorative features like the columns, pylons, balustrades and balconies were restored as part of 1998 seismic rehabilitation.


**HCM No. 907- Buena Vista Viaduct (now called the North Broadway-Buena Vista Bridge)** Constructed in 1911. Designated in 2008.

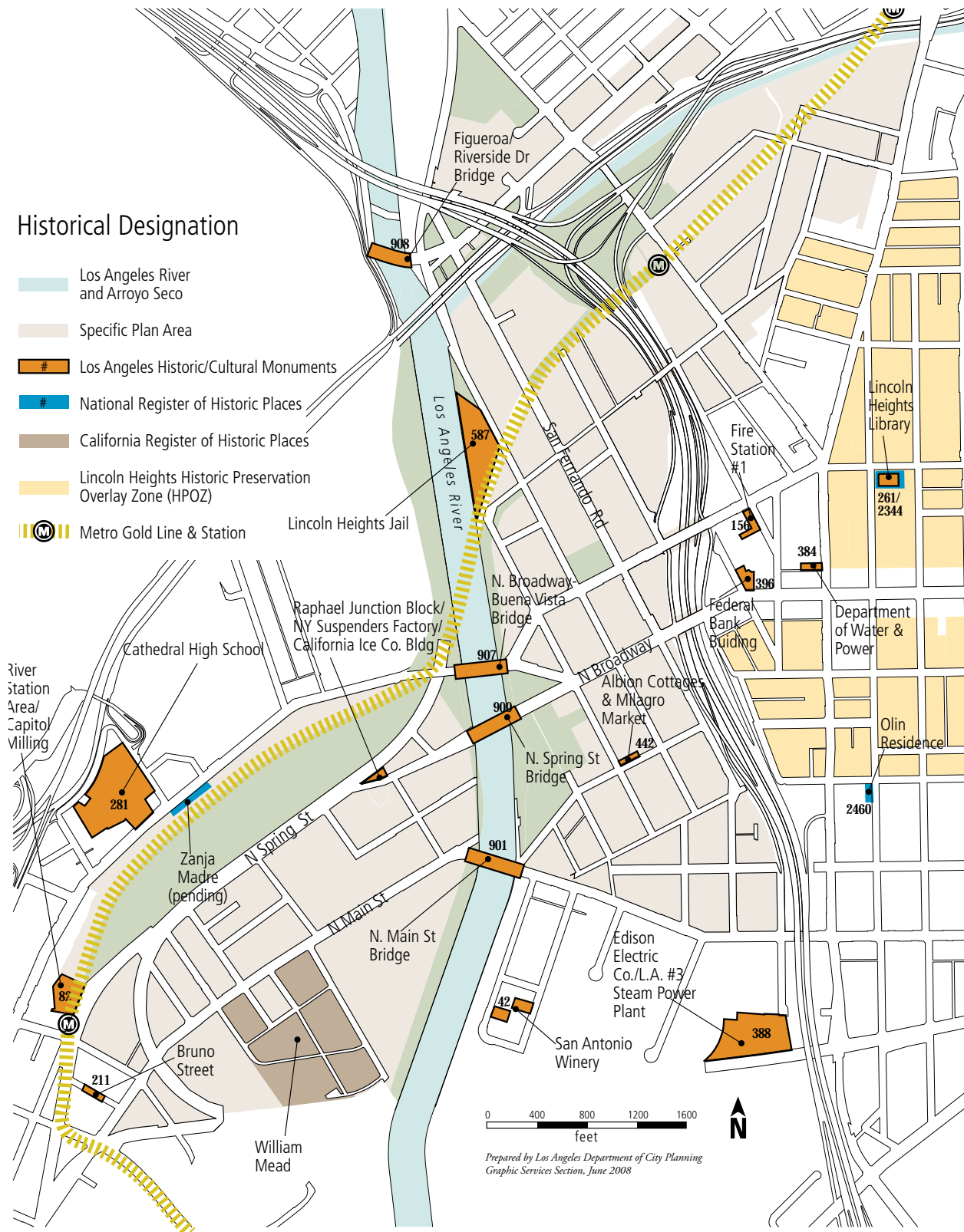
**HCM No. 908 – Figueroa/ Riverside Bridge.** Constructed between 1927 to 1939. Designated in 2008.

**California Register of Historic Places No. 2S2-William Mead Residences.** 1300 N. Cardinal Street. Built in 1942.

**National Register No. 2460 - Olin Residence.** 2622-2624 Mozart Street.

# Historical Designation

-  Los Angeles River and Arroyo Seco
-  Specific Plan Area
-  # Los Angeles Historic/Cultural Monuments
-  # National Register of Historic Places
-  # California Register of Historic Places
-  Lincoln Heights Historic Preservation Overlay Zone (HPOZ)
-  Metro Gold Line & Station



Prepared by Los Angeles Department of City Planning  
Graphic Services Section, June 2008

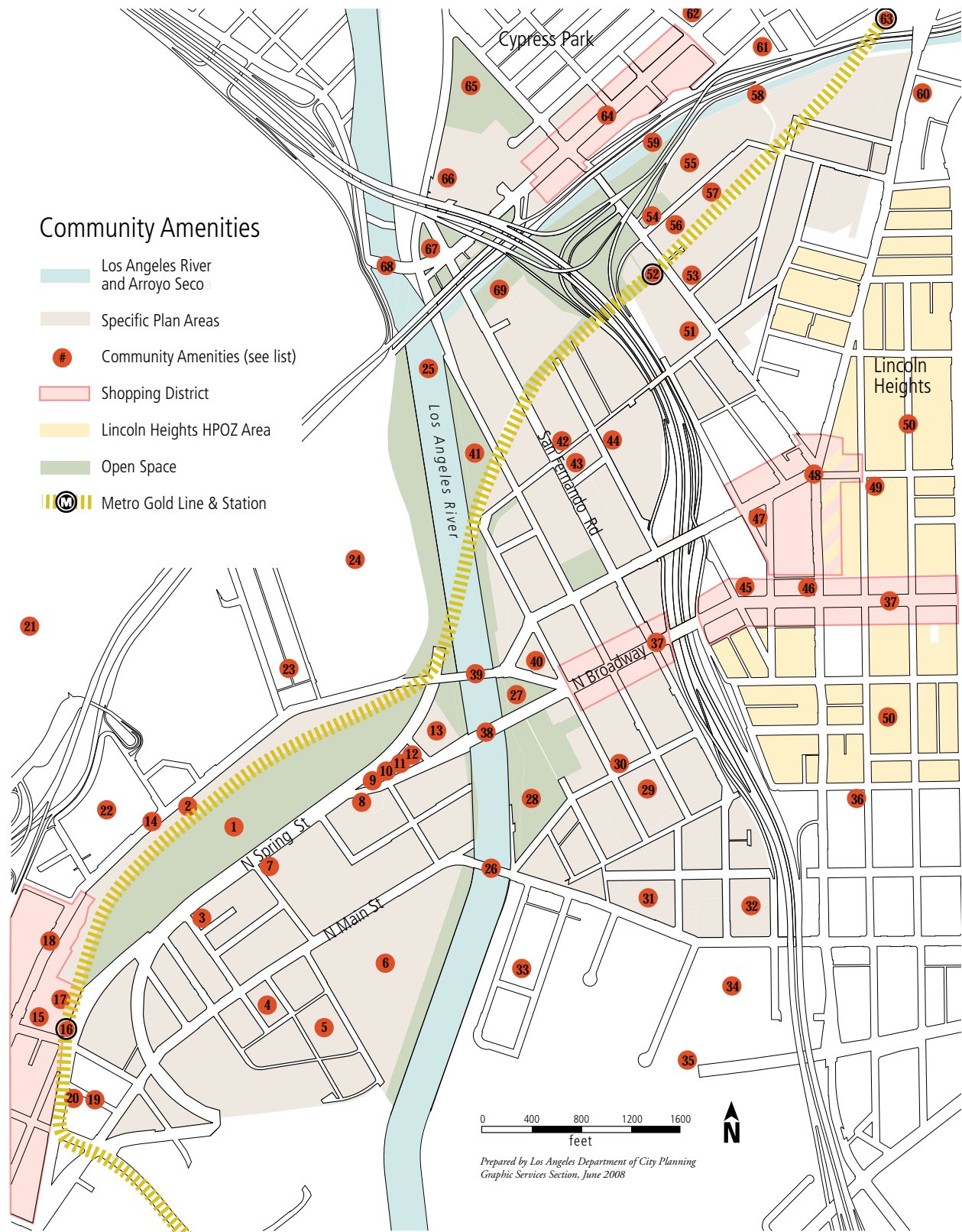


**COMMUNITY AMENITIES**

Everything in the CASP is intended to provide a framework for and support an increasingly active civic and cultural environment for residents, workers and visitors to the area. Figure 12-1 maps many of the current events, activities, cultural facilities and other aspects of life in the area’s public realm.

- 1. Los Angeles State Historic Park (event site)
- 2. Zanja Madre
- 3. Los Angeles Conservation Corps
- 4. Ann Street Elementary School
- 5. William Mead Public Housing
- 6. Los Angeles Department of Water and Power- Main Street Center
- 7. Nick’s Café
- 8. House at 1646 N. Spring Street- Bldg. Built 19\_\_
- 9. Raphael Junction Block/NY Suspenders Factory/ California Ice Co. Building- Historic Cultural Monument #872- Bldg. Built 19\_\_
- 10. Thomas Davis Co. Bldg. Built 19\_\_
- 11. Carnation Building- Built 19\_\_
- 12. Standard Oil Building- Built 19\_\_
- 13. Farmlab and Under Spring Events, openings, music
- 14. Future Bridge to State Historic Park
- 15. Blossom Plaza (future) Event site, outdoor dining, paseo
- 16. Gold Line Chinatown Station
- 17. Capitol Milling- Historic Cultural Monument #82
- 18. Chinatown
- 19. Bruno Street
- 20. Homegirl Cafe
- 21. Dodger Stadium
- 22. Cathedral High School- Historic Cultural Monument #281
- 23. Solano Canyon neighborhood
- 24. Elysian Park
- 25. Los Angeles River
- 26. North Main Street Bridge- Historic Cultural Monument #901
- 27. Downey Pool and Recreation Center
- 28. Albion Dairy Park
- 29. Albion Elementary School and Early Education Center
- 30. Albion Cottages & Milagro Market- Historical Cultural Monument #442

- 31. Kipp LA College Prep
- 32. school on Main Street
- 33. San Antonio Winery- Historic Cultural Monument #42
- 34. The Brewery
- 35. Edison Electric Co/LA #3 Steam Power Plant- Historic Cultural Monument #388
- 36. Olin Residence- National Register of Historic Places #2460
- 37. Broadway shopping/eating district
- 38. N. Spring Street Bridge- Historical Cultural Monument #900
- 39. N. Broadway Buena Vista Bridge- Historic Cultural Monument- #907
- 40. Young Nak Church
- 41. Lincoln Heights Jail- Historic Cultural Monument- #587
- 42. Goodwill Workforce Center and Outlet Store
- 43. Alta Lofts (formerly Fuller Lofts)
- 44. St. Vincent de Paul Thrift Store
- 45. Federal Bank Building- Historical Cultural Monument #396
- 46. Department of Water & Power Historical Cultural Monument #384
- 47. Fire Station- Historical Cultural Monument #156
- 48. Five Points Shopping District
- 49. Lincoln Heights Library- Historical Cultural Monument #261 (National Register of Historic Places #2344
- 50. Lincoln Heights HPOZ
- 51. AMCAL Housing
- 52. Lincoln Heights/Cypress Park Gold Line Station
- 53. Los Angeles Department of Water and Power- Artesian Site
- 54. Lacy Street Park
- 55. North Central Animal Shelter
- 56. Lacy Street Studios
- 57. Lacy Street Lofts
- 58. Arroyo Seco Pedestrian Bridge
- 59. Arroyo Seco
- 60. Elementary School (on Pasadena Avenue)
- 61. Elementary School (Cypress)
- 62. Nightingale Middle School
- 63. Heritage Square Gold Line Station
- 64. Figueroa Shopping District
- 65. Los Angeles River Center
- 66. Home Depot
- 67. Confluence Park
- 68. Figueroa Riverside Drive Bridge- Historical Cultural Monument #908



**OPEN SPACE**

The Plan area currently has limited park and open space amenities although recent acquisitions by California State Parks and the City of Los Angeles for the Los Angeles State Park (Cornfield) and Albion Dairy sites respectively have increased future park space by 38 acres. A temporary 11 acre park at the Cornfield site provides jogging and walking paths along with picnic areas. The Mountains and Recreation Conservation Association (MRCA) is developing the six acre Confluence Park in the north of the Plan area and efforts are underway to construct a multi-purpose path along the southern bank of the Arroyo Seco between San Fernando Boulevard and Avenue 26. Future long range plans envision a bikeway and multi-purpose path on the banks of the Los Angeles River.

In addition to the future open space projects, the area is nearby to the recreation facilities and nature preserves of Elysian Park and Debs park and the Rio de Los Angeles State Park are within a mile of the Plan. But, it is the small neighborhood parks, playgrounds, pocket parks, community gardens, public plazas, dog parks and open spaces of a more intimate scale that are lacking in the area. New open space requirements in the Plan will increase development of these needed park spaces and in so doing contribute to an urban trail network that provides physical and visual connections to the civic, historical and cultural elements of the Plan area.

The map and table on the following pages illustrate the location and amenities of the existing parks. Future park spaces are encouraged to reference this map and table in order to identify suggestions for potential amenities.






**Parks and Open Space in and around the Comfields-Aroyo Specific Plan Area**

- Los Angeles River and Arroyo Seco

Specific Plan Area
- Existing Open Space

Future Potential Open Space
- One Mile Radius of Specific Plan Area

Metro Gold Line and Stations

					Auditorium	Baseball	basketball
<b>Existing Parks Around CASP Area</b>		Type	Acreage	Dist. from Center CASP (2)			
1	Alpine Recreation Center	Neighborhood	1.9	1.06 mi	x		x
2	Cypress Recreation Center	Neighborhood	3.5	1.13 mi	x		
3	Debs Park	Community	34.0	1 mi			
4	Echo Park	Community	28.4	2 mi	x	x	x
5	Echo Park Deep Pool (3)	Neighborhood	2.1	2 mi			
6	Elysian Park	Regional	544.6	.5 mi	x	x	
7	Everett Park	Neighborhood	0.5	1.34 mi			
8	Hazard Park	Community	25.0	1.45 mi	x		x
9	Lincoln Heights Recreation Center	Neighborhood	2.9	0.6 mi	x		x
10	Lincoln Park	Community	42.5	1.12 mi		x	
11	Pecan Recreation Center	Neighborhood	4.3	1.71 mi	x		x
12	Prospect Park	Neighborhood	2.7	1.3 mi			
13	Ramona Gardens Park	Neighborhood	2.1	1.89 mi			x
14	Ramona Gardens Recreation Center	Neighborhood	6.4	1.89 mi	x	x	x
15	Rio de Los Angeles State Park	Community	40.0	1.7 mi			
16	State Street Recreation Center	Neighborhood	2.6	1.5 mi	x	x	x
17	Wabash Recreation Center	Neighborhood	2.1	2 mi	x		x
<b>Sub Total</b>			<b>745.6</b>				












#### Existing Parks IN CASP Area

18	Confluence Park	Neighborhood	0.4	.6 mi			
19	Downey Park	Neighborhood	4.5	0.13 mi		x	x
20	Lacy Street Park	Neighborhood	0.8	.74 mi			
21	Los Angeles State Historic Park	Neighborhood	11.0	.25 mi			
22	Recreation Center Inside Historic Jail						
<b>Sub Total</b>			<b>16.7</b>				

#### Potential Future Open Space + Existing Parks Surrounding CASP Area

23	Variety of Parks Resulting from 15% Open Space Requirement	Neighborhood	28.9				
24	Los Angeles State Historic Park	Community	33.0				
25	Historic Lincoln Heights Jail	Neighborhood	3.8				
26	Midway Yard	Community	11.0				
27	West Bank Greenway/Bike Path	Regional	5.4				
28	Arroyo Seco Gateway Park	Neighborhood	8.0				
29	Confluence Park	Neighborhood	1.2				
30	Albion Dairy + Downey Recreation and Park	Community	10.5				
<b>Sub Total</b>			<b>101.68</b>				



bbq	Children play area	football (1)	gymnasium	handball	picnic	swimming	restroom	soccer (1)	tennis	volleyball			
											SPORTS PROGRAMS		
	x		x							x	x	x	x
x	x		x		x						x	x	x
					x								
x	x	x	x		x			x	x		x	x	x
						x					x	x	
					x		x		x				x
													x
x	x		x	x	x				x		x	x	x
	x		x								x	x	x
			x		x		x		x				
	x			x	x					x	x	x	x
	x												
	x			x					x	x	x		x
			x								x	x	x
	x										x	x	x
	x		x								x	x	x

- (1) Rectangular Fields encompass activities such as Football, Soccer, Lacross.  
(2) Distance is measured from the eastern point of the Pasadena Ave bridge crossing the LA River.  
(3) Not shown in Map

						x							
						x							




# A-02

LEED ND Application



Cornfield Arroyo Seco  
Project #10094286  
Certification Level: CERTIFIED  
Stage 1  
22 April 2010



USGBC  
2101 L STREET, NW  
SUITE 500  
WASHINGTON DC 20037  
202 828-7422  
WWW.USGBC.ORG

PRESIDENT, CEO &  
FOUNDING CHAIRMAN  
S. Richard Fedrizzi

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Tom Cole  
Foster Planning Systems

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CALSTAR Manufacturing Corporation

IMMEDIATE PAST CHAIR  
Gail Kibben  
Center for Maximum Potential Bldg Systems

TREASURER  
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ACCION

SECRETARY  
Fiona Liffman  
Carnegie Mellon University

FOUNDERS

David Gortman  
Richard Fedrizzi  
S. Richard Fedrizzi

5 May 2010

Claire Bowin  
City Planner  
Los Angeles Department of City Planning  
201 North Figueroa Street  
Los Angeles, CA 90012-2655

Dear Claire,

The U.S. Green Building Council, in collaboration with the Congress for the New Urbanism and the Natural Resources Defense Council, is proud to recognize the efforts and accomplishments of your project team by awarding LEED® for Neighborhood Development Pre-review Approval to Cornfields Arroyo Seco Specific Plan in Los Angeles, CA. This means your project has completed Stage 1 of certification. Your project's LEED for Neighborhood Development rating reflects 44 documented and approved points, which corresponds to the certified certification level under the pilot program.

Enclosed you will find your project's scorecard. Additionally, a letter to present to interested third parties—such as local land use authorities, investors, or potential tenants—and a brief narrative which defines the three stages of certification, has been included. The LEED for Neighborhood Development project list will be updated to reflect your project's completion of Stage 1. You can now refer to your project as a LEED certified plan. In describing LEED for Neighborhood Development, you may state that LEED for Neighborhood Development is a certification program developed in collaboration with CNU and NRDC that integrates the principles of smart growth, new urbanism and green building into the first national system for neighborhood design. However, no projects are allowed to use the USGBC or LEED logos, nor the logos of CNU or NRDC. Also, please note that Natural Resources Defense Council, NRDC, and the NRDC logo are registered trademarks and may not be used in any advertising, press release, or other promotional or commercial materials without the express permission of the Natural Resources Defense Council, Inc., New York, New York 10011.

We hope your project decides to see its involvement with LEED for Neighborhood Development through to completion by submitting for Stage 2 and Stage 3. Your project can submit for Stage 2 after receiving its entitlements or necessary approvals by following the submittal requirements outlined in the rating system. This process should be relatively easy, if the plan does not change significantly, because minimal additional information will be required.

Please accept my congratulations on the Pre-review Approval of Cornfields Arroyo Seco Specific Plan. It is my sincere hope that this project serves as a model for future neighborhood development and more sustainable design. If you have any questions, please do not hesitate to contact Dara Zycherman, Manager, LEED for Neighborhood Development via email [nd@committees.usgbc.org](mailto:nd@committees.usgbc.org) or phone 202.828.1156.

We look forward to working with you in the future to further our common mission of transforming land development and creating a greener world for future generations.

Sincerely,

S. Richard Fedrizzi  
President, CEO & Founding Chairman  
U.S. Green Building Council



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David Guthef  
 Michael Bakawa  
 S. Richard Fedrizzi

5 May 2010

To Whom It May Concern,

The LEED® certification program encourages and accelerates adoption of sustainable green building and development practices through the use of universally understood and accepted performance criteria. Although primarily focused on buildings, we have recently developed a rating system for neighborhood development in collaboration with the Natural Resources Defense Council and the Congress for the New Urbanism that is being pilot tested with nearly 240 development projects. The LEED for Neighborhood Development Rating System integrates the principles of smart growth, urbanism, and green building into the first national certification tool for neighborhood design. LEED certification provides independent, third-party verification that a development's location and design meet high standards of environmentally responsible, sustainable development.

It is my pleasure to recognize Cornfields Arroyo Seco Specific Plan as having completed Pre-review Approval at the certified level. Cornfields Arroyo Seco Specific Plan has submitted substantial documentation to show compliance with the rating system's prerequisites and 44 points during the first (of three) stage of certification. The first stage involves the review of site plans and written commitments regarding the location of the project and the types of buildings and infrastructure to be constructed, prior to the project receiving its entitlements or necessary approvals.

LEED for Neighborhood Development Pre-Review Approval means that if the project is built according to the plan and commitments submitted, the project should be able to achieve LEED for Neighborhood Development certification. The next step for Cornfields Arroyo Seco Specific Plan is to gain any necessary approvals and entitlements from the relevant local authorities. Assuming the project is approved in its current state or with modifications that do not affect compliance with the rating system's criteria, the project can then go on to earn Certification of an Approved Plan, which is the second stage of certification. After the project is substantially constructed, and it submits documentation confirming that it was built according to the plans previously reviewed, it will have completed the third and final stage of LEED for Neighborhood Development certification: Certification of a Completed Neighborhood Development.

Please contact Dara Zycherman, Manager, LEED for Neighborhood Development, via email [dzycherman@usgbc.org](mailto:dzycherman@usgbc.org) or phone 202.828.1156 with any questions about the LEED for Neighborhood Development program.

Sincerely,

S. Richard Fedrizzi  
 President, CEO & Founding Chairman  
 U.S. Green Building Council





**LEED® for Neighborhood Development Pilot**

**Cornfield Arroyo Seco**  
**Project #10094286**  
**Certification Level: CERTIFIED**  
**Stage 1**  
**22 April 2010**

**44 Points Achieved** **Possible Points: 106**

Certified 40 to 49 points Silver 50 to 59 points Gold 60 to 69 points Platinum 80 or more points

**16 Smart Location & Linkage** **Possible Points: 30** **15 Green Construction & Technology** **Possible Points: 31**

Y	Prereq 1	Smart Location, Option 1	
Y	Prereq 2	Proximity to Water & Wastewater Infrastructure, Option 1	
Y	Prereq 3	Imperiled Species & Ecological Communities, No Species	
Y	Prereq 4	Wetland & Water Body Conservation, Option 2	
Y	Prereq 5	Agricultural Land Conservation, Option 2	
Y	Prereq 6	Floodplain Avoidance, Option 2	
2	Credit 1	Brownfields Redevelopment	2
1	Credit 2	High Priority Brownfields Redevelopment	1
8	Credit 3	Preferred Locations	10
	Credit 4	Reduced Automobile Dependence	8
1	Credit 5	Bicycle Network	1
3	Credit 6	Housing & Jobs Proximity, Option 1	3
1	Credit 7	School Proximity	1
	Credit 8	Steep Slope Protection	1
	Credit 9	Site Design for Habitat or Wetland Conservation	1
	Credit 10	Restoration of Habitat or Wetlands	1
	Credit 11	Conservation Management of Habitat or Wetlands	1

Y	Prereq 1	Construction Activity Pollution Prevention	
	Credit 1	LEED Certified Green Buildings	3
1	Credit 2	Energy Efficiency in Buildings	3
2	Credit 3	Reduced Water Use, Option 1	3
1	Credit 4	Building Reuse & Adaptive Reuse	2
1	Credit 5	Reuse of Historic Buildings	1
1	Credit 6	Minimize Site Disturbance through Site Design, Option 1	1
1	Credit 7	Minimize Site Disturbance during Construction, Option 1	1
1	Credit 8	Contaminant Reduction in Brownfields Remediation	1
5	Credit 9	Stormwater Management, February 2007 Version, Option 1	5
1	Credit 10	Heat Island Reduction, Option 1	1
	Credit 11	Solar Orientation	1
	Credit 12	On-Site Energy Generation	1
	Credit 13	On-Site Renewable Energy Sources	1
	Credit 14	District Heating & Cooling	1
	Credit 15	Infrastructure Energy Efficiency	1
	Credit 16	Wastewater Management	1
	Credit 17	Recycled Content in Infrastructure	1
1	Credit 18	Construction Waste Management	1
	Credit 19	Comprehensive Waste Management	1
	Credit 20	Light Pollution Reduction	1

**12 Neighborhood Pattern & Design** **Possible Points: 39**

Y	Prereq 1	Open Community	
Y	Prereq 2	Compact Development	
1	Credit 1	Compact Development	7
4	Credit 2	Diversity of Uses	4
3	Credit 3	Diversity of Housing Types	3
	Credit 4	Affordable Rental Housing	2
	Credit 5	Affordable For-Sale Housing	2
	Credit 6	Reduced Parking Footprint	2
	Credit 7	Walkable Streets	8
1	Credit 8	Street Network, Option 2	2
1	Credit 9	Transit Facilities	1
	Credit 10	Transportation Demand Management	2
1	Credit 11	Access to Surrounding Vicinity	1
	Credit 12	Access to Public Spaces	1
	Credit 13	Access to Active Spaces	1
	Credit 14	Universal Accessibility	1
1	Credit 15	Community Outreach & Involvement	1
	Credit 16	Local Food Production	1

**1 Innovation & Design Process** **Possible Points: 6**

	Credit 1.1	Innovation in Design	1
	Credit 1.2	Innovation in Design	1
	Credit 1.3	Innovation in Design	1
	Credit 1.4	Innovation in Design	1
	Credit 1.5	Innovation in Design	1
1	Credit 2	LEED Accredited Professional	1

## **LEED for Neighborhood Development Pilot Program Certification Process**

LEED for Neighborhood Development will certify projects that may have significantly longer construction periods than single buildings, and as a result the standard LEED certification process needed to be modified. The core committee wanted to be able to provide developers of certifiable projects with some form of approval even at the early, pre-entitlement stage. They also wanted to ensure that great plans became great real-life projects. With these goals in mind, the core committee created the following three-stage certification process:

### **Optional Pre-review (Stage 1)**

This stage is available but not required for projects at any point before the entitlement process begins. If pre-review approval of the plan is achieved, USGBC will issue a letter stating that if the project is built as proposed, it will be able to achieve LEED for Neighborhood Development certification. The purpose of this letter is to assist the developer in building a case for entitlement among land use planning authorities, as well as a case for financing and occupant commitments.

### **Certification of an Approved Plan (Stage 2)**

This stage is available after the project has been granted any necessary approvals and entitlements to be built to plan. Any changes to the pre-reviewed plan that could potentially affect prerequisite or credit achievement would be communicated to USGBC as part of this submission. If certification of the approved plan is achieved, USGBC will issue a certificate stating that the approved plan is a LEED for Neighborhood Development Certified Plan and will list it as such on the USGBC website.

### **Certification of a Completed Neighborhood Development (Stage 3)**

This step takes place when construction is complete or nearly complete. Any changes to the certified approved plan that could potentially affect prerequisite or credit achievement would be communicated to USGBC as part of this submission. If certification of the completed neighborhood development is achieved, USGBC will issue plaques or similar awards for public display at the project site and will list it as such on the USGBC website.



DRAFT

# A-03

## Stormwater Guidelines



### PURPOSE

- To assist Projects with meeting the parcel level Stormwater requirements.
- To provide technical information about infiltrates and runoff rates and required detention volumes, raingarden sizing, drywells, permeable paving, and vegetated filter strips.
- To provide information on a variety of Best Management Practices (BMPs).



To achieve the Stormwater requirements identified in Sub-Sections 7.7.1. - 7.7.4. of this Plan Projects will have a variety of Best Management Practices (BMPs) to choose from. Some BMPs, such as raingardens and swales are better suited for parcels with large open spaces already intended for landscaping. Others, such as dry wells, green roofs and flow-through planters are more suitable where the sites are highly constrained. While more costly than raingardens, these devices require less space. Rainwater harvesting can also be used as a means for meeting parcel level stormwater management requirements.

The tables and figures included here are intended to provide guidance for meeting the parcel level stormwater management requirements. The tables indicate a number of recommended BMPs for management of stormwater on individual parcels, provide critical dimensions per 1000 square feet of impermeable and permeable surface, and indicate whether the treatment and detention requirements can be met. The sizing of individual BMPs can vary greatly depending on soil characteristics and spatial constraints. While some BMPs will meet both of these requirements, others may only meet one of the requirements. With the exception of clay and some clay loam soils, and when bedrock prevents infiltration, open space does not need to be mitigated during the water quality event (WQE) because infiltration rates through the soil will be higher than rainfall rates.

The technical tables and figures are described here first followed by Table 7 which illustrates a variety of BMPs.



### Soil Infiltration Rates

Soil infiltration rates compared to the design storm rainfall rate are compared in Table 1 below.

**Table 1**  
Soil infiltration rates compared to rainfall intensity during the WQE

Soil conservation service group	Soil type	Saturated infiltration rate (in/hr)	WQE Rainfall rate (in/hr)
A	Sand	8.0	0.2
A	Loamy sand	2.0	0.2
B	Sandy loam	1.0	0.2
B	Loam	0.5	0.2
C	Silt Loam	0.25	0.2
C	Sandy clay loam	0.15	0.2
D	Clay loam and silty clay loam	<0.09	0.2
D	Clay	<0.05	0.2

Source: Urban Runoff Quality Management, WEF Manual of

### Water Quality Event (WQE) Runoff Rates and Detention Volumes

WQE runoff rates and required detention volumes for individual parcels are explained in Table 2.

**Table 2**  
WQE runoff rates and required detention volumes for parcels

Surface Type	Runoff Coefficient C	Area (sq ft)	Treatment Requirement		Treatment Volume
			Runoff Rate (cf/hour)	Volume (ft)	
Impermeable	0.9	1000	15.0	56	28
Open Space	0.35	1000	5.8	22	11

Note that treatment of runoff from open space is only required when soil infiltration is less than 0.2 inches per hour.

## Raingardens

Table 3 illustrates ideal dimensions for a rain garden in various soil conditions. The critical dimensions are bottom area and volume as these two factors affect how quickly water is infiltrated and how much water is spilled during a WQE. Spilled water should be diverted to an appropriate drainage system. Note that the footprints for sand, sandy loam and loam soils are somewhat similar, whereas, clay loam and clay soils tend to require larger footprints. When space is limited and clay loam or clay soils are present, an under drain system can be installed to allow for more rapid infiltration through an engineered soil such as clean sand. The under drain would discharge to an appropriate drainage system or to the street.

**Table 3**  
Raingarden example sizing per 1000 square feet of roof + 1000 square feet of open space

	Soil Type				
	Loamy Sand	Sandy Loam	Loam	Sandy Clay Loam	Clay
Bottom area (ft <sup>2</sup> )	29	33	36	44	217
Depth (ft)	1	1	1	1	1
Volume (ft <sup>3</sup> )	61.9	68	72.6	84.4	306.5
Footprint (ft <sup>2</sup> )	131	139	145	161	432

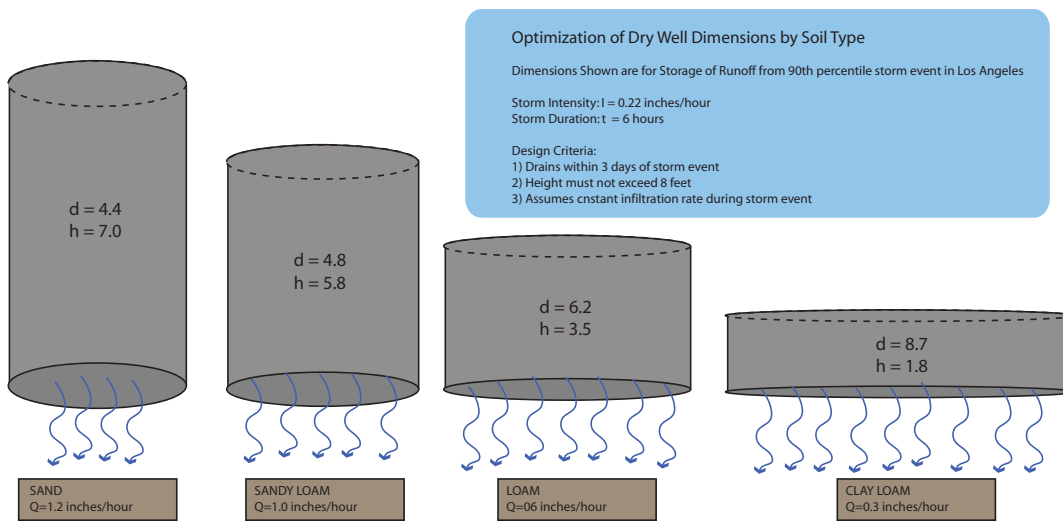
Note that raingarden sizes for sandy clay loam and clay soils are larger than the total runoff volume for the WQE due to the need to infiltrate all runoff through the bottom area over the course of no more than 72 hours following a storm.

## Dry Wells

Table 4 illustrates the variable configuration of dry wells depending on soil type. In order to be effective a dry well must drain relatively quickly so that it can be used to store water from consecutive storm events. Figure 1 below shows how with decreasing permeability of soil, a dry well's ideal dimensions become shorter and broader in order to allow for a greater surface area for infiltration. The required configuration of a dry well in clay loam and clay soils can make it difficult to construct and install under these conditions.

**Figure 1**

Dry-well dimensions by soil type to meet water treatment and retention requirements



**Table 4**

Dry Well sizing per 1000 square feet of impermeable drainage area

	Soil Type				
	Sand	Sandy Loam	Loam	Sandy Clay Loam	Clay
Pipe Diameter (ft)	4.1	4.1	5.3	7.5	not recommended
Depth (ft) (h)	5.5	5.8	6.0	6.0	not recommended
Volume (ft <sup>3</sup> )	71.5	74.8	1332.0	264.0	

Note: Constructability of a dry well in both clay loam and clay soils is limited by the shallow well height and large well diameter

### Permeable Paving

Permeable paving is an effective means of reducing and treating runoff. Table 5 indicates the percent or fraction of a paved area that could be treated with permeable paving in order to meet either the attenuation requirement or the treatment requirement. For instance, in a loamy sand condition, only 12 percent of a concrete patio would need to be paved with a permeable surface in order to infiltrate enough water during the WQE to meet the attenuation requirement for the patio runoff.

**Table 5**  
Permeable paving sizing as percentage of total paved area

Soil Type	Meets treatment and attenuation requirement
Loamy Sand	12%
Sandy Loam	24%
Loam	49%
Sandy	96%
Clay	use engineered soil and subdrain

### Vegetated Filter Strips




Vegetated filter strips can be used to either treat runoff or to retain runoff through infiltration. Table 6 indicates the sizing requirement for vegetated filter strips used for either treating or attenuating runoff from impermeable surfaces. Note that when only treatment is required, the filter strip size is minimal whereas, if the filter strip is used to meet the attenuation requirement, the size grows due to the slow rate of infiltration. Note that, in clay soils, additional storage is needed to meet the attenuation requirements.

**Table 6**  
Vegetated filter strip sizing per 1000 square feet of impermeable surface

Soil Type	Meets treatment requirement (ft <sup>2</sup> )	Meets flow attenuation requirement (ft <sup>2</sup> )
Loamy Sand	90	yes
Sandy Loam	182	yes
Loam	361	yes
Sandy Clay	729	Yes
Clay	784	requires an additional 16 cubic feet of storage



**Table 7**  
Best Management Practices

Drainage Area Type	BMP	Description	Image
Roof	Disconnected Roof Leader	Most BMP's used for managing roof runoff require the roof leader to be disconnected from the storm sewer network. Typically runoff collected from the roofs is piped directly to the storm sewers. By disconnecting a roof leader, runoff can be directed towards BMP's, infiltrated into the ground, or flow overland to the storm drainage network. This action will reduce the peak flows in the storm drainage network, and will make treatment and infiltration options available.	
	Flow Through Planter Box	Flow through planter boxes are formalized versions of raingardens, which are contained within a planting structure, generally formed of concrete. Planters are generally used when sites are constrained or when a specific aesthetic appearance is desired. Planter boxes occupy less space than raingardens due to the absence of side slopes and are usually attached or immediately adjacent to buildings in order to connect to individual roof leaders. Planters can be lined or can have open bottoms to promote infiltration. If infiltration is limited due to low permeability of native soil, a perforated pvc subdrain can be added to connect to an adjacent drainage or storm sewer. Planter boxes generally contain engineered soils consisting of several inches of planting soil mixture with a sub bed of sand and native soil.	
	Green Roof	A green roof is a roof of a building that is partially or completely covered with vegetation and soil over a waterproofing membrane. A green roof provides stormwater attenuation by reducing the amount of runoff that flows off of the roof. The effect is primarily achieved by infiltration of water into the soil medium during a storm, effectively lowering the runoff coefficient and increasing storage capacity of the roof. Depending on their design, green roofs can typically eliminate runoff from small storm events but will still produce runoff during larger events. The soil and subsurface sand layers of a green roof can provide both detention and retention of runoff. Runoff from unplanted areas of the roof may be directed to a green roof for treatment or detention. Typically however, a green roof only treats water from the area it occupies.	
	Cisterns	Cisterns provide above ground storage of rainwater harvested from roof spaces. Water from cisterns is typically used for either irrigation or internal toilet flushing. Because water is reused on-site, cisterns provide stormwater retention, completely removing the captured water from contributing to runoff. An additional benefit of cisterns is to reduce the water consumption of the building.	
	Dry Well	A dry well is an underground structure that dissipates it into the ground. Water flows through a dirty well. The influence of gravity and discharges occurs through a number of small exit openings distributed over the sides and bottom of the dry well. Once full, a dry well can only accept water as fast as it can dissipate water.  Simple dry wells consist of a pit filled with gravel, riprap, rubble, or other debris. Such pits resist collapse, but do not have much storage capacity because their interior volume is mostly filled by stone. A more advanced dry well defines a large interior storage volume by a reinforced concrete cylinder with perforated sides and bottom.	

Dimensions in well drained soils per 1000 sq ft roof area*	Design Criteria
n/a	
Area = 29ft <sup>2</sup> Volume = 51 ft <sup>3</sup> Depth to Soil = 1.75 ft Width = 2.5	Must drain within 24 hours - Spillway required to allow overflow - Overflows should be located at the opposite end of the planter box from the roof leader to promote settling. Box should be planted with dense vegetation to promote biofiltration.
1000 ft <sup>2</sup> Green roofs	Total water storage capacity of the soil must be equivalent to 0.75 inches. Soil and sand layers must be no less than 4 inches combined. Detention requirements in other areas of the site can be off-set by increasing the detention capacity of the green roof up to a maximum of 1.3 inches. If runoff from other unvegetated areas of roof is directed to the green roof for treatment, calculations must be shown to indicate the green roof has the retention/detention capacity to handle the additional flows.
57 ft <sup>3</sup>	The cistern size must be large enough to retain the entire WQE runoff. This may be prohibitive from an economic perspective due to the large size of the cistern.
Volume = 71.5 ft <sup>3</sup> Height = 5.5 ft Diameter = 4.1 ft	Dry wells should drain within 3 days of the WQE to allow space for subsequent storms. Typically, the depth should not exceed about 6 feet to avoid excessive excavation. Dry wells can be designed to retain and thus treat all of the WQE runoff. The size of the dry well is limited by the rate of infiltration through the bottom and side wall openings.
Treatment + Detention Volume = 90 ft <sup>3</sup> Height = 7.2 ft Diameter = 4.0 ft	Dry wells should drain within 3 days of the WQE to allow space for subsequent storms. Typically, the depth should not exceed about 8 feet to avoid excessive excavation. Dry wells can be designed to retain and thus treat all of the WQE runoff. In the former case, an overflow system should be designed which allows spilled water to be treated by an alternative means. It should be noted that the size is limited by the rate of drainage
Detention Only Volume = 61 ft <sup>3</sup> Height = 7.2 Diameter = 3.3 ft	In this case, an overflow system should be designed which allows spilled water to be treated by an alternative means.

**Table 7**  
Best Management Practices continued

Drainage Area Type	BMP	Description	Image
	Minimize Site Imperviousness	Decreasing the amount of impervious surface used for driveways, patios and other open spaces is an effective measure to decrease runoff and therefore the treatment and detention requirements, by reducing the runoff coefficient of land. Pavement should only be used in areas where drivable surfaces are required. One example of reducing pavement is to create planted areas down the center of driveways. Aside from reducing the runoff coefficient this will also aid in treating runoff from the driveway.	
	Vegetated Filter Strips	Separation of hardscape areas with planted open space reduces sheet flow runoff from hardscape, reduces overall site imperviousness and provides treatment of runoff from hardscape areas. Vegetated filter strips provide treatment through bio filtration- the process of water flowing horizontally through vegetation. They also provide retention through infiltration though the required sizing to meet the retention requirement tends to be much larger.	
	Permeable Pavement	Permeable Pavement is an effective BMP to manage stormwater on paved areas. Stormwater infiltrates through the porous material and can naturally infiltrate into the ground. Directing runoff from impermeable surfaces to permeable paving areas can help reduce the treatment requirements significantly. In areas where infiltration is limited by the soils permeability, a subdrain can be connected to an adjacent storm sewer. Alternative natural surfaces such as crushed gravel and coarse sand can also be used to the same effect. Permeable paving also reduces the overall imperviousness of a site.	
Multi Purpose (Receive Runoff from Roofs or Open Space)	Raingarden	Raingardens are cost effective BMP's that can be used to detain and treat stormwater runoff from both roof surfaces and landscaped areas. A raingarden is a slightly depressed area with an overflow outlet draining to a nearby storm sewer or other drainage system. Treatment process includes infiltration and biofiltration. The overflow prevents flooding on the adjacent land, before naturally infiltrating into the ground. They also can have soil media that aids in filtering sediments and pollutants in stormwater. In cases where subsurface rock will not allow for infiltration, the bottom of the raingarden can be lined with a liner/ fabric, and a subdrain can be connected to the street network to drain flows. If infiltration is limited due to low soil permeability or not desirable adjacent to a buildings foundation, a perforated pvc subdrain can be added to connect to an adjacent drainage or storm sewer. The raingarden would then be sized according to the subsurface engineered soil infiltration rates.	
	Vegetated Swale	Swales are linear BMP's that aid in treating stormwater while conveying flow to a desired location. Overland flow could direct stormwater to landscaped swales that are on the edges of a property that blend into the rest of the surrounding yard. For residential applications swales are typically 3' wide at the bottom, and have side slopes at 3:1. The swales should be lined with grass and or flood tolerant vegetation to promote biofiltration and to reduce flows. Soil media can also be used to filter sediments and remove pollutants prior to infiltration into the ground. In cases where infiltration is limited due to low soil permeability a subdrain can be connected to a nearby storm sewer or drainage.	

\* For all other soil types, see the BMP Sizing Charts

\*\* 1/4 acre lot assumed to be 80% impermeable, 20% open space

Critical Dimensions in well drained soils per 1000 sq ft impermeable surface*	Design Criteria
N/A	
90 ft <sup>2</sup>	Runoff must either infiltrate completely into the vegetated filter strip, or spend a minimum of 10 minutes flowing through the vegetated filter strip in order to meet treatment requirements. The amount of water retained is equivalent to the water infiltrated during the storm event and depends on soil type. In most soils, the rate of infiltration will be the limiting sizing factor. However, in most clay soils, the residence time is the limiting factor. In clay soils, additional storage is required in conjunction with the filter strip.
120 ft <sup>2</sup>	For treatment to occur, runoff must infiltrate completely through the permeable paving surface. The sizing at left is for a permeable paving system with an infiltration efficiency of 75% compared to open ground. The size represents the portion of a 1000 square foot surface that would be required to have permeable paving. As the permeability of the native soil decreases, the fraction of the surface requiring permeable paving increases. Permeable paving is not recommended in clay soils. All drainage from the impermeable surface must sheet flow across the installed permeable paving.
Bottom Area = 29 ft3 Volume = 62 ft3 Total footprint = 131 ft2	Raingardens should be sized to drain over a period of no more than 3 days. For aesthetic purposes, depths should not exceed about 3 feet. Maximum unsupported side slopes of 3:1. Must drain completely within three days. A spillway is required to allow overflow. Overflows should be located at the opposite end from the inlet to promote settling and biofiltration. Should be planted with dense vegetation to promote biofiltration.
Bottom Area = 127 ft2	Runoff must either infiltrate completely into the vegetated swale, or spend a minimum of 10 minutes flowing through the swale in order to meet treatment requirements. In clay and sandy clay soils residence time tends to be the size limiting criteria. A swale is also used for conveyance and has a depth that is accommodated by gently sloping sides (no greater than 3:1). The depth should be sufficient such that the swale can convey runoff from the 25 year storm event.







# A-04

## Street Standards



### PURPOSE:

- Promote a multi-modal street network
- Establish recommended standards for modified cross sections
- Illustrate modified street standards
- Establish street assumptions and criteria

## RECOMMENDED STANDARDS AS ILLUSTRATED BY CROSS SECTIONS

The Plan's Street Standards are modifications of the existing street designations and apply to the Plan's street segments illustrated in the cross-sections on the following pages. The primary distinction between the various street designations that occur in the Plan is their number of traffic lanes. The designations are then further distinguished by the width of sidewalk and the presence or absence of on-street parking and/or bicycle lanes.

**Major Class II.** Four full-time traffic lanes (two in each direction) and two additional peak-period traffic lanes that displace off-peak parking.

**Secondary.** Four full-time traffic lanes (two in each direction). Depending upon the road width and its particular role this designation also may include full-time parking lanes and/or bicycle lanes.

**Collector.** Two full-time traffic lanes (one in each direction) and full-time parking and bicycling lanes.

**Local.** Two full-time traffic lanes (one in each direction) and full-time parking on at least one side of the street.

The Plan's Street Standards are illustrated by a series of cross-sections. The cross-sections show the typical midblock conditions. Intersections are not shown. For each street, the existing street designation and existing cross sections by segment are shown in the left column. The proposed cross-sections for those same segments are shown in the right column. The legend on the following page identifies each element in the cross section diagrams.

The proposed Plan Street Standard for each street segment includes:

**Right-of-way width (ROW).**

**Roadway width** (curb to curb).

**Sidewalk width within the ROW.** The sidewalk width cannot be reduced. In other words, the roadway cannot be widened at the expense of the sidewalk.

These standards will be accompanied by the Cornfield Arroyo Seco Streetscape Plan which will include improvement standards for pedestrian-scale street lights, tree plantings, landscaped parkways, bio-swales, bicycle parking, trash cans, benches, and transit accommodations. Property owners are required to maintain all improvements on the adjacent sidewalk.

Upon final approval of these standards the Bureau of Engineering will add a layer to its Navigate LA website to inform all developers of the future block-by-block requirements for streets and sidewalk widths.

The following assumptions and criteria further guide the development of future roadway improvements.

### ASSUMPTIONS

**Lane Capacity.** Lane capacity assumptions for planning purposes are as follows.

850/lane one-way.

750/lane two-way with continuous center turn lane or left turns/median and parking.

700/lane two-way with left turns at intersections (from parking) and parking.

**Buses.** Metro operates several bus lines through the Plan area, and Foothill Transit and DASH both run a single line through the Plan. The transit corridors served by Metro are

Avenue 26, Broadway, Figueroa, Main, Pasadena and San Fernando. In addition to these corridors Foothill Transit runs down Avenue 19 and the DASH uses a portion of Spring, Ann, and College. Due to the high volume of buses and number of riders all the streets served by transit need to have adequate sidewalk width for pedestrians, typically 15 feet minimum and more where there are higher concentrations of pedestrians.

**Bicycles.** The 2010 Bicycle Plan designates all or portions of Avenues 18, 19 and 26, Broadway, Figueroa, Humboldt, Main, Spring, San Fernando, and Pasadena, Sotello, and Mesnager as Bikeways. In particular Broadway, Figueroa, Main and Pasadena have been identified on the Citywide Bicycle Network as priority bikeway corridors. Humboldt, Spring and San Fernando have also been identified as priority bikeway corridors on the Neighborhood Bicycle Network. The Plan recommends that the majority of these priority bikeways on either the Citywide or Neighborhood Networks be striped with Class II Bicycle Lanes. The widths on Main and San Fernando cannot accommodate both Bicycle Lanes and Parking Lanes and therefore the Plan recommends that Class II Bicycle Lanes be installed in -lieu of on-street parking. On Avenue 26, north of the Gold Line Bridge, severe width constraints due to the freeway on and off-ramps prohibit the addition of Bicycle Lanes through a portion of Avenue 26. Traffic volumes on Broadway east of the LA River require the six peak-hour lanes and therefore a Class II bikeway cannot be accommodated through this portion of Broadway.

**Pedestrians.** The Plan's Street Standards recommend the broadening of sidewalk widths throughout the Plan area to better accommodate pedestrians commuting to transit, to school, nearby stores, restaurants, and parks.

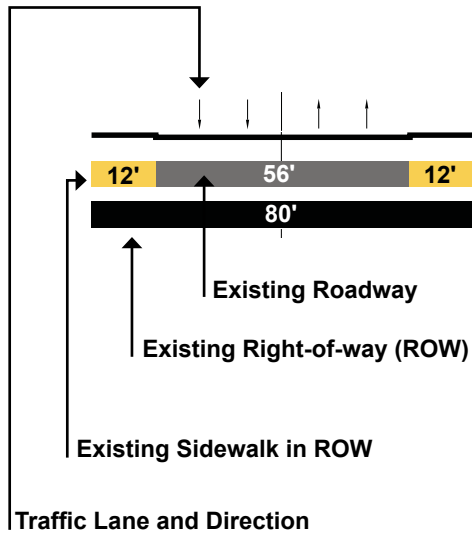
## CRITERIA

Due to the wide variety of street conditions found throughout the Plan area these criteria are established as general rules and there are always exceptions to general rules.

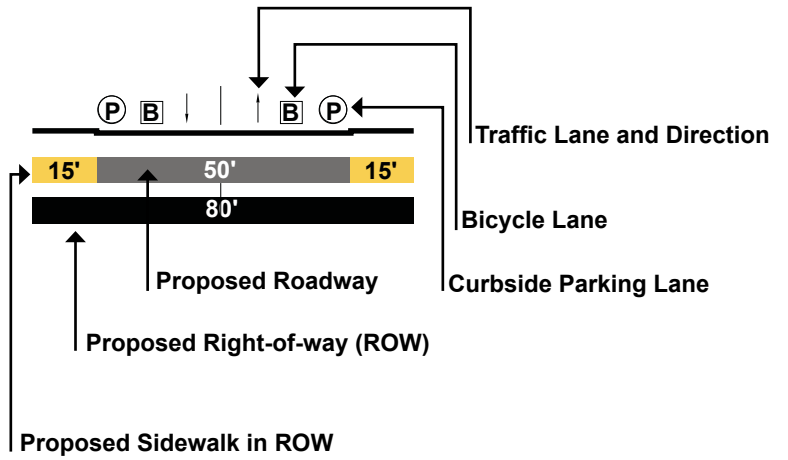
1. Roadways should maintain a consistent roadway width unless there is an overriding need.
2. Intersections should flare only at Major to Major intersections.
3. Striping should preserve on-street parking with left turns permitted only at the intersections except where a continuous turn lane is needed due to significant mid-block turn movements.
4. Accept slower speed (35 mph or less) lane widths as appropriate for the Plan's streets.
  - 35mph or less
  - Curb Lanes 10-12 feet
  - Traffic Lanes 9-11 feet
5. Travel lanes adjacent to bicycle lanes should be a minimum of 11 feet.
6. Bicycle lanes should be a minimum of 6 feet but no more than 7 feet.
7. Sidewalk widths vary based on street width and traffic adjacency as well as land use.
8. Standards work both ways. For example, if a new street standard is currently wider than the proposed street width than a roadway narrowing should be triggered by the same actions that trigger roadway widening.
9. Vegetated Stormwater Curb extensions should be installed at all Local to Local intersections.
10. Maximize curb-side parking- convert red curb to parking where appropriate.
11. Allow peak-period curbside parking where curb lane is at least 18 feet wide.

# STREET CROSS SECTION LEGEND

**EXISTING**  
**Secondary Hwy**  
**(Current Street Designation)**



**PROPOSED**  
**Collector Modified**  
**(Proposed Street Designation)**

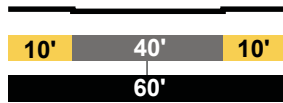


# North - South Streets (Looking North)

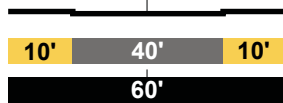
## EXISTING

### ANN STREET Collector Street

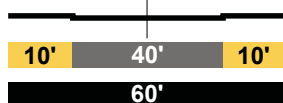
Spring - Weyse



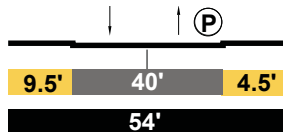
Weyse - Naud



Naud - Main

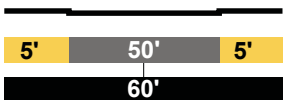


### Main - Magdalena Local Street



### ARTESIAN STREET Local

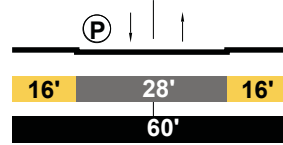
Ave. 34 - Ave. 33



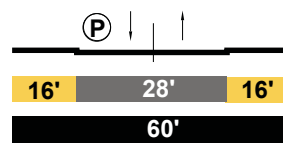
## PROPOSED

### ANN STREET Local Modified

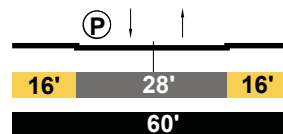
Spring - Weyse



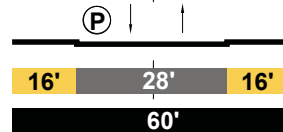
Weyse - Naud



Naud - Main

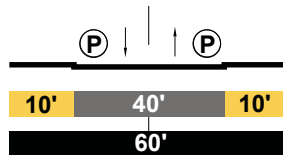


Main - Magdalena



### ARTESIAN STREET Local Modified

Ave. 34 - Ave. 33



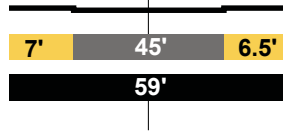


# North - South Streets (Looking North)

## EXISTING

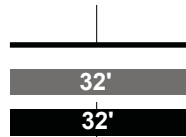
### AURORA STREET Local

Baker - Spring



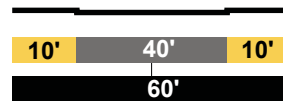
### AVE. 16 Local

Albion - Mozart

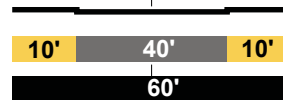


### AVE. 17 Local

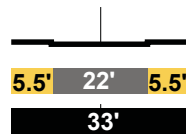
Albion - Mozart



Mozart - Darwin



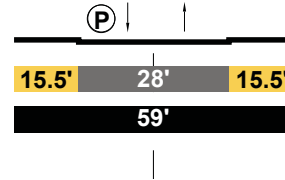
Darwin - Main



## PROPOSED

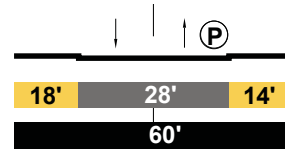
### AURORA STREET Local Modified

Baker - Spring



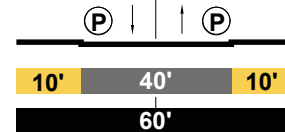
### AVE. 16 Local Modified

Albion - Mozart

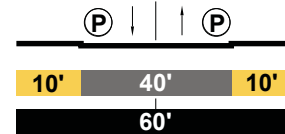


### AVE. 17 Local Modified

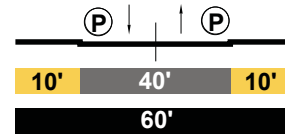
Albion - Mozart



Mozart - Darwin



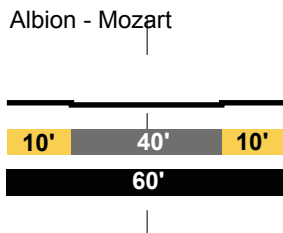
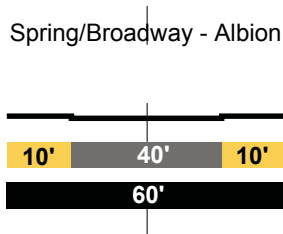
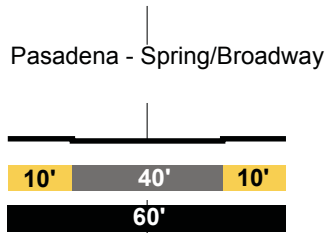
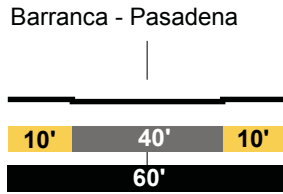
Darwin - Main



# North - South Streets (Looking North)

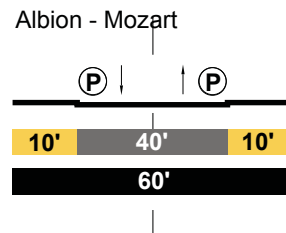
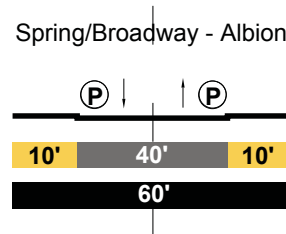
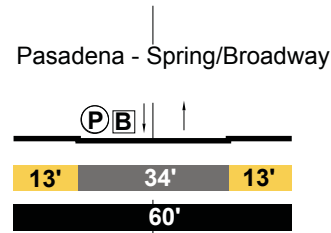
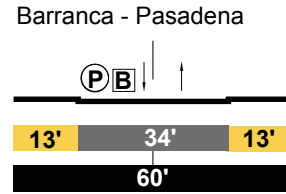
## EXISTING

### AVE. 18 Local



## PROPOSED

### AVE. 18 Local Modified

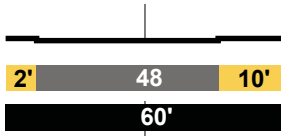


# North - South Streets (Looking North)

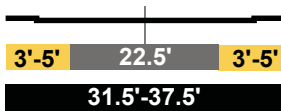
## EXISTING

### AVE. 19 Local

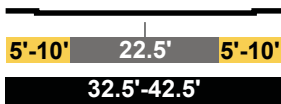
San Fernando Rd - Riverside



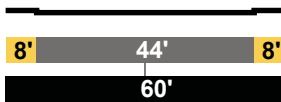
Riverside - Southbound Pasadena Fwy.



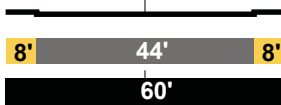
Riverside - Northbound Pasadena Fwy.



Pasadena Fwy. - Humboldt



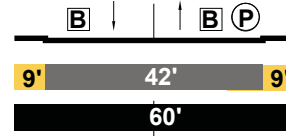
Humboldt - Barranca



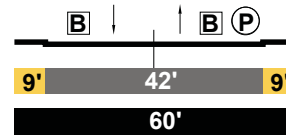
## PROPOSED

### AVE. 19 Collector Modified 2

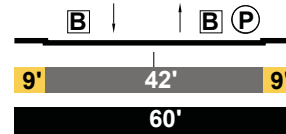
San Fernando Rd - Riverside



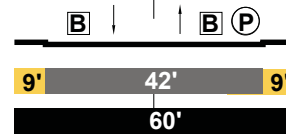
Riverside - Southbound Pasadena Fwy.



Riverside - Northbound Pasadena Fwy.



Pasadena Fwy. - Humboldt



Humboldt - Barranca

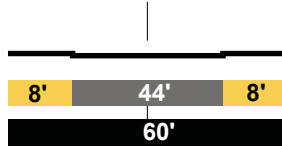


# North - South Streets (Looking North)

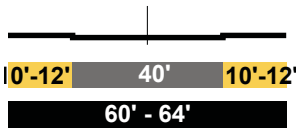
## EXISTING

### AVE. 19 Local

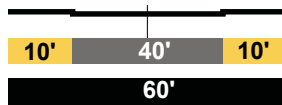
Barranca - Pasadena



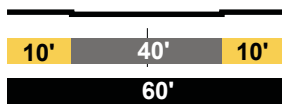
Pasadena - Broadway



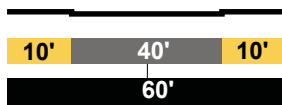
Broadway - Albion



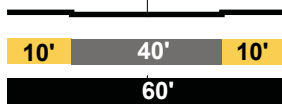
Albion - Mozart



Mozart - Darwin



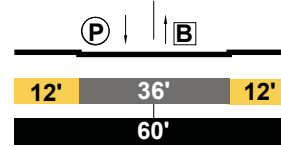
Darwin - Main



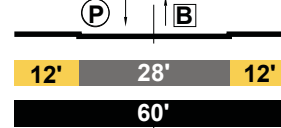
## PROPOSED

### AVE. 19 Local Modified

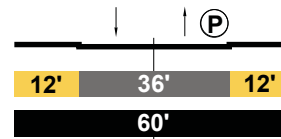
Barranca - Pasadena



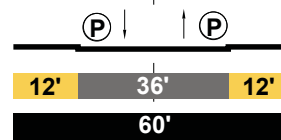
Pasadena - Broadway



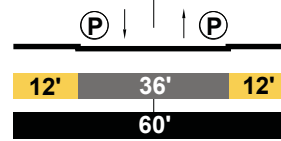
Broadway - Albion



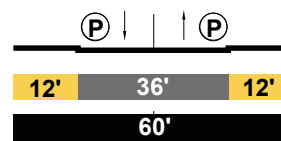
Albion - Mozart



Mozart - Darwin



Darwin - Main

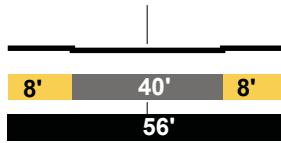


# North - South Streets (Looking North)

## EXISTING

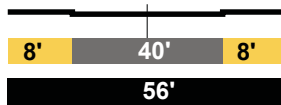
### AVE. 20 Secondary Hwy

Pasadena - Broadway

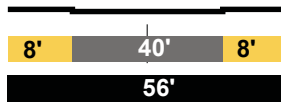


### AVE. 20 Secondary Hwy

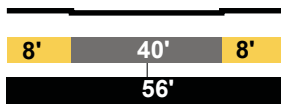
Broadway - Albion



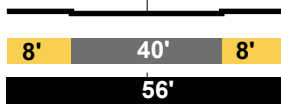
### Collector Street Albion - Mozart



Mozart - Darwin



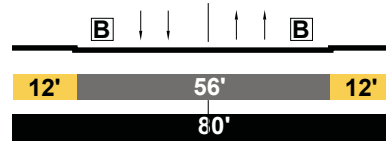
Darwin - Main



## PROPOSED

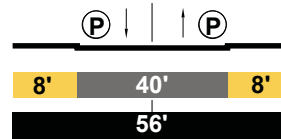
### AVE. 20 Secondary Modified

Pasadena - Broadway

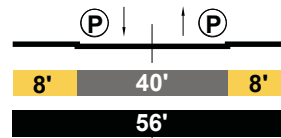


### AVE. 20 Local Modified

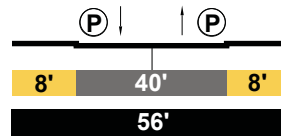
Broadway - Albion



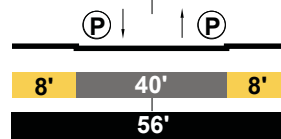
Albion - Mozart



Mozart - Darwin



Darwin - Main

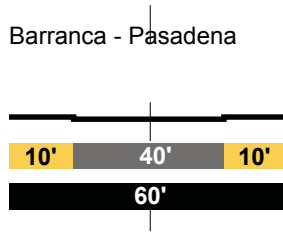
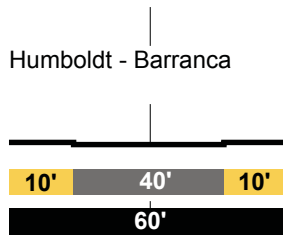
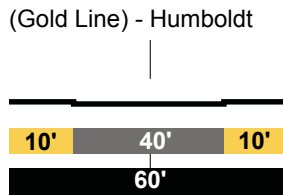




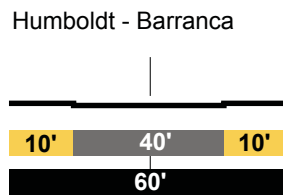
# North - South Streets (Looking North)

## EXISTING

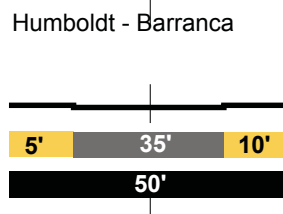
### AVE. 21 Local



### AVE. 23 Local

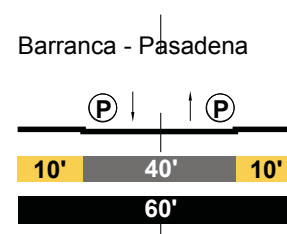
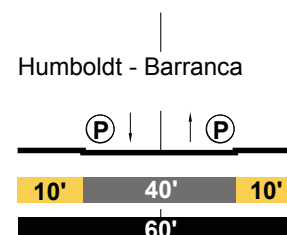
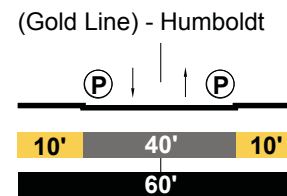


### AVE. 25 Local

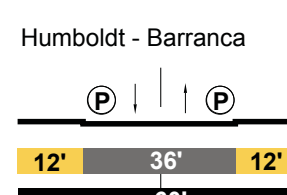


## PROPOSED

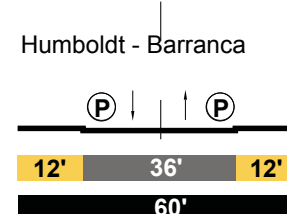
### AVE. 21 Local Modified



### AVE. 23 Local Modified



### AVE. 25 Local Modified

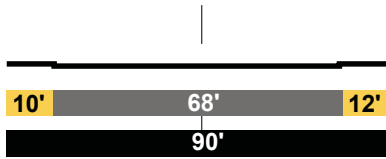


# North - South Streets (Looking North)

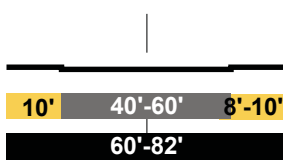
## EXISTING

### AVE. 26 Secondary Hwy

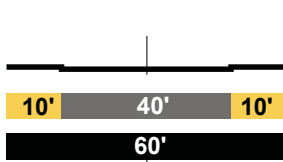
Figueroa - Pasadena Fwy.



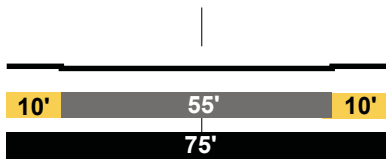
Pasadena Fwy. - Lacy



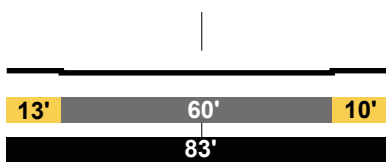
Lacy - Gold Line Bridge



Gold Line Bridge - Artesian



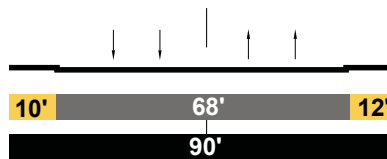
Artesian - Humboldt



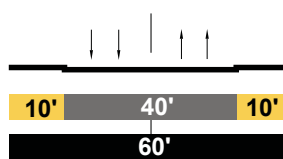
## PROPOSED

### AVE. 26 Secondary Modified 4

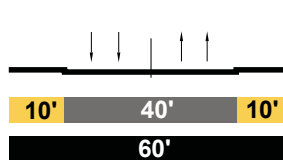
Figueroa - Pasadena Fwy.



Pasadena Fwy. - Lacy

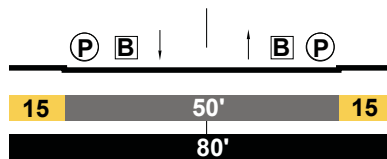


Lacy - Gold Line Bridge

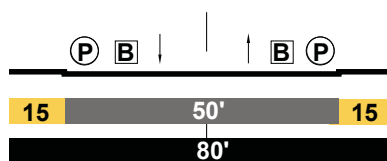


### AVE. 26 Collector Modified

Gold Line Bridge - Artesian



Artesian - Humboldt

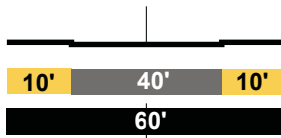


# North - South Streets (Looking North)

## EXISTING

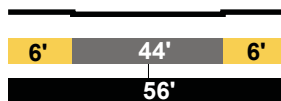
### AVE. 26 Local

Humboldt - Barranca



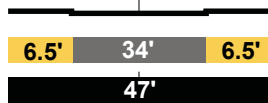
### AVE. 28 Local

Huron - Figueroa

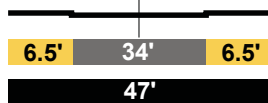


### BLOOM Local

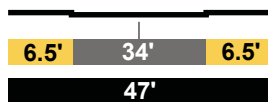
Main - Magdalena



Magdalena - Cardinal



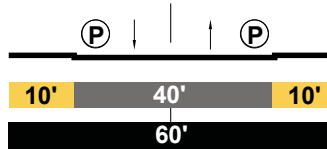
Cardinal - Bolero



## PROPOSED

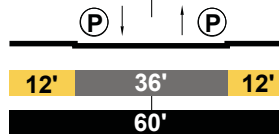
### AVE. 26 Collector Modified

Humboldt - Barranca



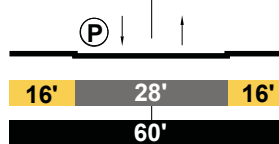
### AVE. 28 Local Modified

Huron - Figueroa

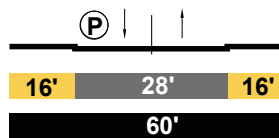


### ELMYRA BLOOM Local Modified

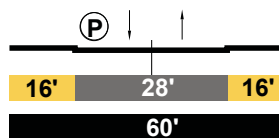
Main - Magdalena



Magdalena - Cardinal



Cardinal - Bolero

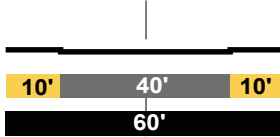


# North - South Streets (Looking North)

## EXISTING

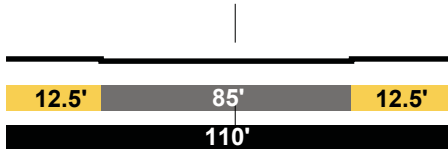
### COLLEGE Secondary Hwy

Spring - Main



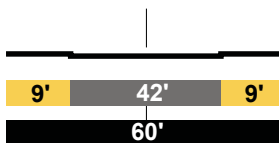
### CYPRESS Local

Figuroa - Arroyo Seco Ave

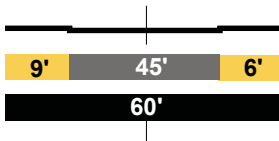


### ELMYRA Collector

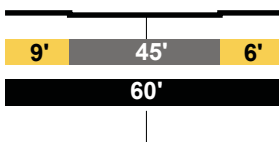
Spring - Main



Main - Magdalena



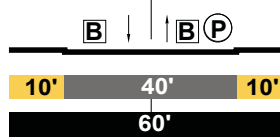
Magdalena - Cardinal  
Local



## PROPOSED

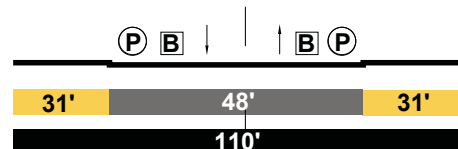
### COLLEGE Collector Modified

Spring - Main



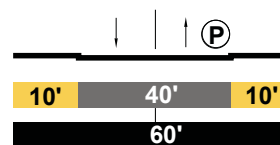
### CYPRESS Local Modified

Figuroa - Arroyo Seco Ave

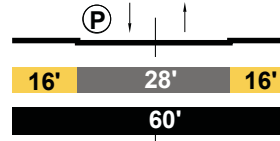


### ELMYRA Local Modified

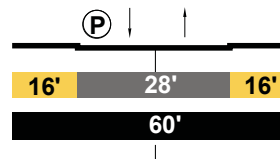
Spring - Main



Main - Magdalena



Magdalena - Cardinal

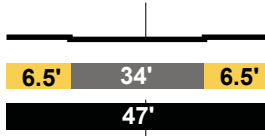


# North - South Streets (Looking North)

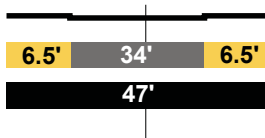
## EXISTING

### LEROY Local

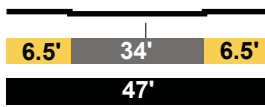
Main - Magdalena



Magdalena - Cardinal

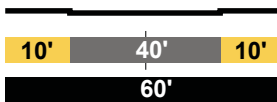


Cardinal - Bolero



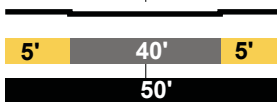
### LLEWELLYN Local

Roundout - Main



### MESNAGERS Collector

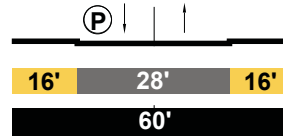
Spring - Naud



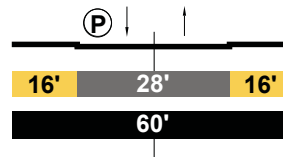
## PROPOSED

### LEROY Local Modified

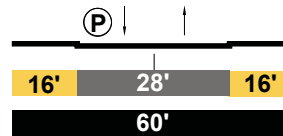
Main - Magdalena



Magdalena - Cardinal

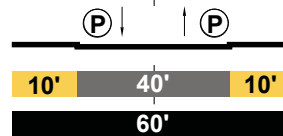


Cardinal - Bolero



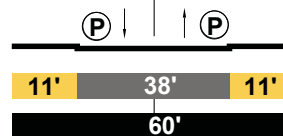
### LLEWELLYN Local Modified

Roundout - Main



### MESNAGERS Local Modified

Spring - Naud



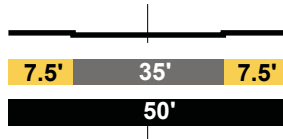


# North - South Streets (Looking North)

## EXISTING

RIVER  
Local

Huron - Figueroa



ROUNDOUT  
Local

Spring - Main

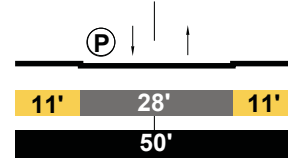


SAN FERNANDO RD

## PROPOSED

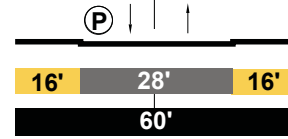
RIVER  
Local Modified

Huron - Figueroa



ROUNDOUT  
Local Modified

Spring - Main



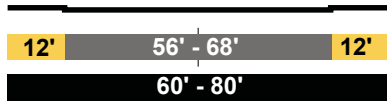
SAN FERNANDO RD

# North - South Streets (Looking North)

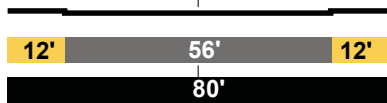
## EXISTING

### SAN FERNANDO RD Secondary Hwy

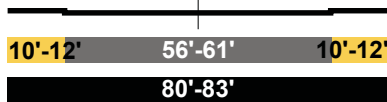
Ave 26 - Ave 19



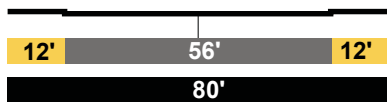
Ave. 19 - Figueroa



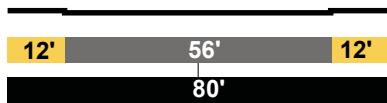
Figueroa- Humboldt



Humboldt - Barranca



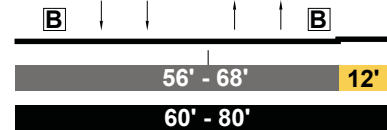
Barranca - Pasadena



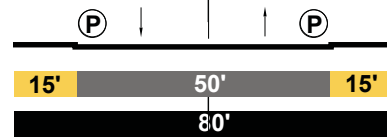
## PROPOSED

### SAN FERNANDO RD Secondary Modified 3

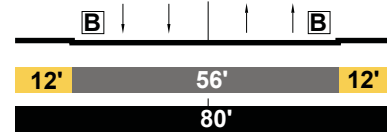
Ave 26 - Ave 19



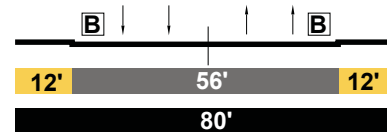
Ave. 19 - Figueroa



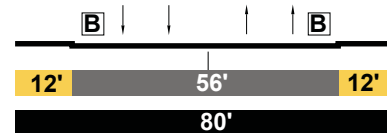
Figueroa- Humboldt



Humboldt - Barranca



Barranca - Pasadena

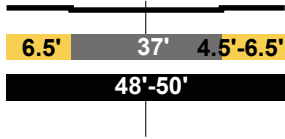


# North - South Streets (Looking North)

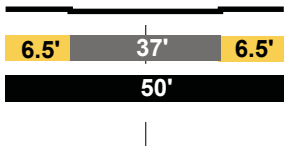
## EXISTING

**SOTELLO**  
Collector

Spring - Naud

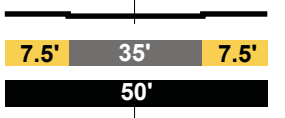


Naud - Main

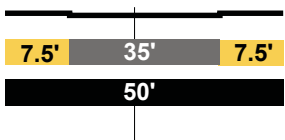


**WILHARDT**  
Collector

Spring - Naud



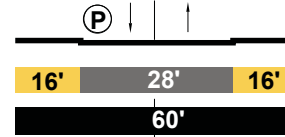
Naud - Main



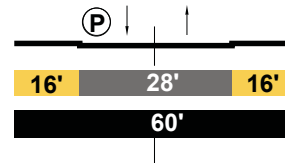
## PROPOSED

**SOTELLO**  
Local Modified

Spring - Naud

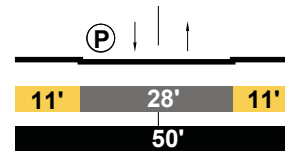


Naud - Main

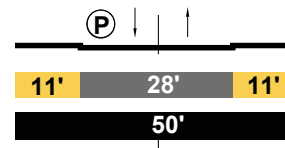


**WILHARDT**  
Local Modified

Spring - Naud



Naud - Main

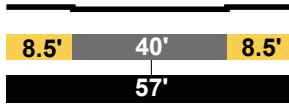


# East - West Streets (Looking West)

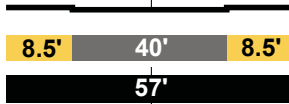
## EXISTING

### ALBION STREET Local

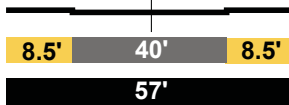
Main - Ave. 16



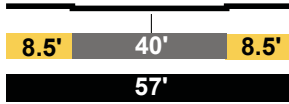
Ave. 16 - Ave. 17



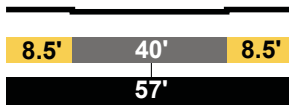
Ave. 17 - Ave. 18



Ave. 18 - Ave. 19



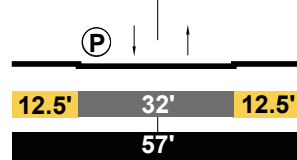
Ave. 19 - Ave. 20



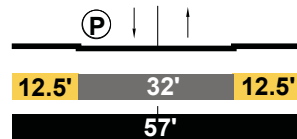
## PROPOSED

### ALBION STREET Local Modified

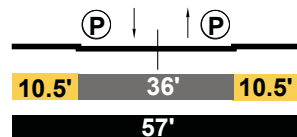
Main - Ave. 16



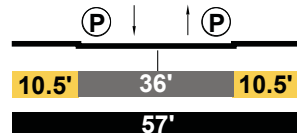
Ave. 16 - Ave. 17



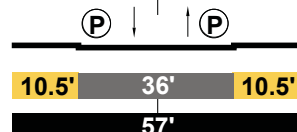
Ave. 17 - Ave. 18



Ave. 18 - Ave. 19



Ave. 19 - Ave. 20



# East - West Streets (Looking West)

EXISTING

PROPOSED

ALHAMBRA

ALHAMBRA

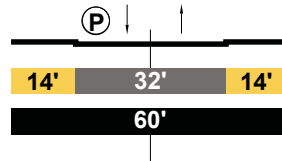
## ARTESIAN PL STREET Local

Artesian - Ave. 26



## ARTESIAN PL STREET Local Modified

Artesian - Ave. 26



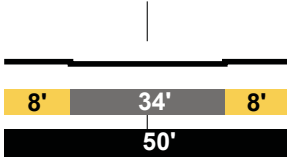


# East - West Streets (Looking West)

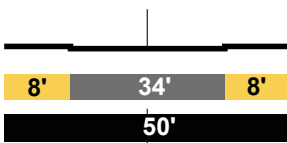
## EXISTING

### AVE. 33 Local

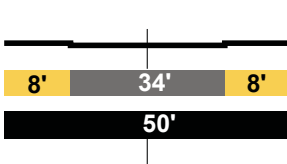
Pasadena - Humboldt



Humboldt - Artesian

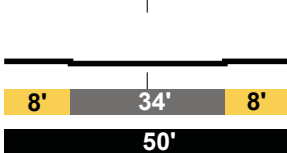


Artesian - Lacy



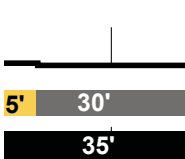
### AVE. 34 Local

Pasadena - Artesian



### BAKER STREET Local

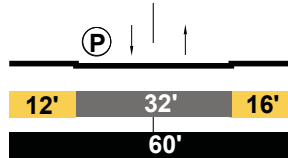
Spring- Aurora



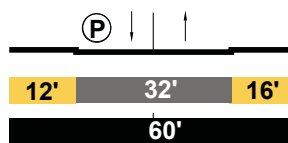
## PROPOSED

### AVE. 33 Local Modified

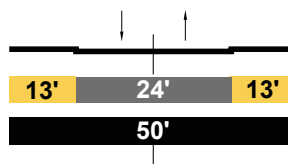
Pasadena - Humboldt



Humboldt - Artesian

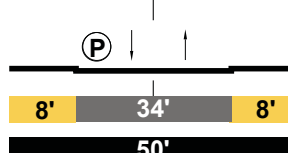


Artesian - Lacy



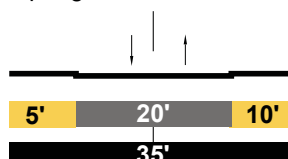
### AVE. 34 Local Modified

Pasadena - Artesian



### BAKER STREET Local Modified

Spring- Aurora

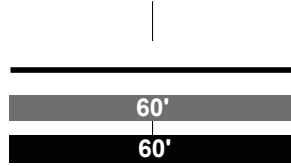


# East - West Streets (Looking West)

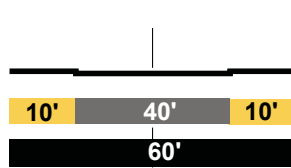
## EXISTING

### BARRANCA Local

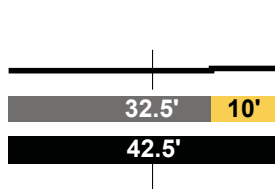
Ave. 18 - Ave. 19



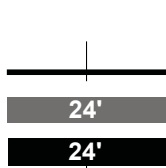
San Fernando Rd - Ave. 21



Ave. 23 - Ave. 25

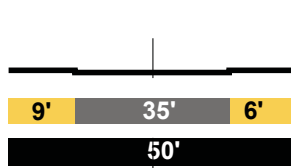


Ave. 25 - Ave. 26



### BOLERO STREET Local

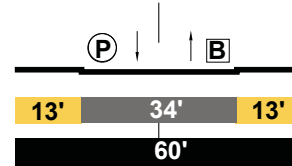
Bloom - Leroy



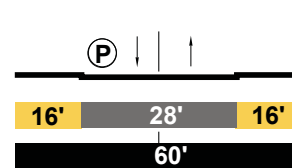
## PROPOSED

### BARRANCA Local Modified

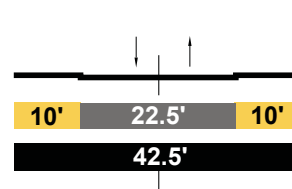
Ave. 18 - Ave. 19



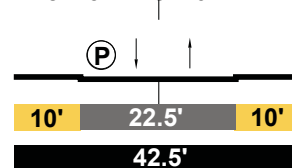
San Fernando Rd - Ave. 21



Ave. 23 - Ave. 25

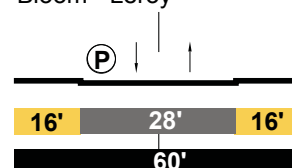


Ave. 25 - Ave. 26



### BOLERO STREET Local Modified

Bloom - Leroy

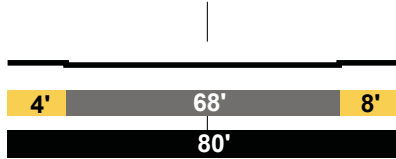


# East - West Streets (Looking West)

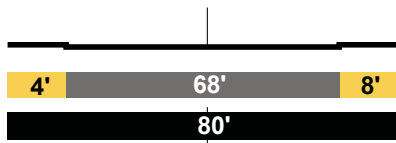
## EXISTING

### BROADWAY Major Hwy Class II

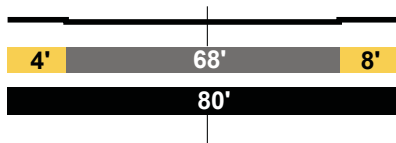
Cottage Home - Bishops



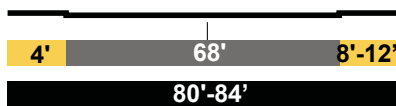
Bishops - Savoy



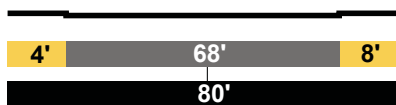
Savoy - Solano



Solano - Casanova



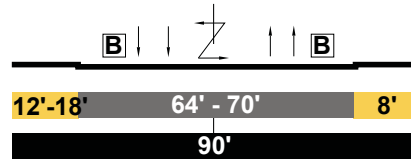
Casanova - Park Row Drive



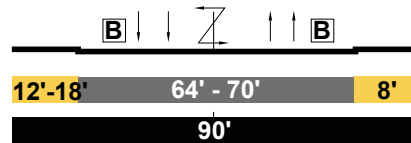
## PROPOSED

### BROADWAY Secondary Modified 2

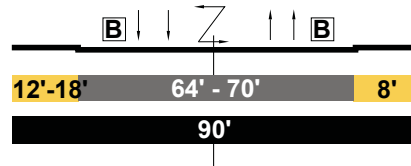
Cottage Home - Bishops



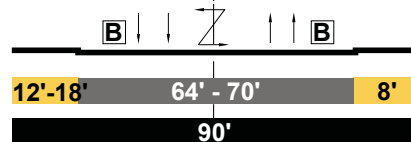
Bishops - Savoy



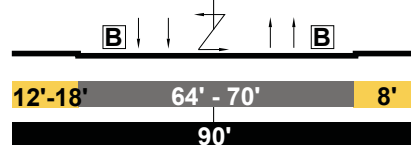
Savoy - Solano



Solano - Casanova



Casanova - Park Row Drive

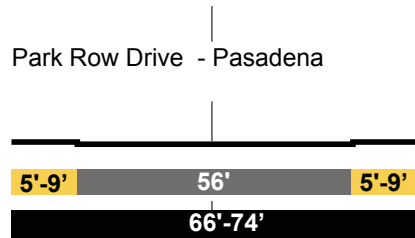


# East - West Streets (Looking West)

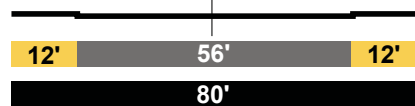
## EXISTING

### BROADWAY/BRIDGE

#### Major Hwy Class II



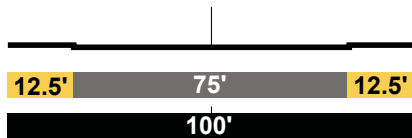
#### Pasadena - Ave. 18



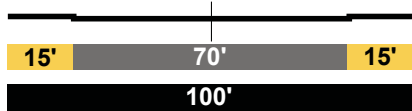
### BROADWAY

#### Major Hwy Class II

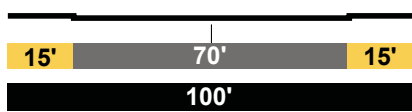
#### Ave.18 - Ave.19



#### Ave.19 - Ave.20



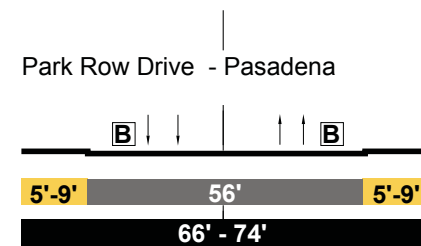
#### Ave.20 - Ave.21



## PROPOSED

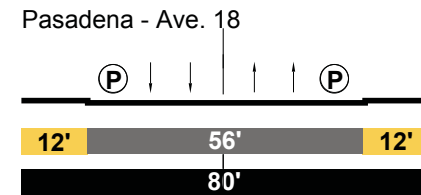
### BROADWAY/BRIDGE

#### Secondary Modified 2



### BROADWAY/BRIDGE

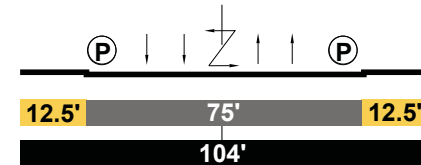
#### Secondary Modified 2



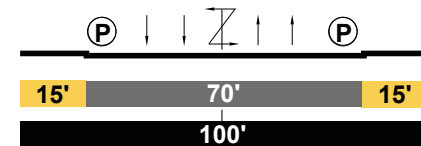
### BROADWAY

#### Major Hwy Class II Modified

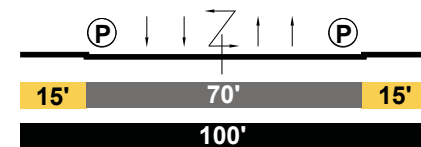
#### Ave.18 - Ave.19



#### Ave.19 - Ave.20



#### Ave.20 - Ave.21



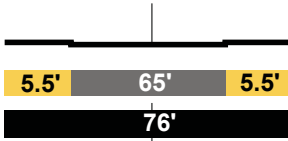
# East - West Streets (Looking West)

## EXISTING

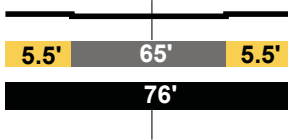
### CARDINAL STREET

Local

Elmyra - Bloom



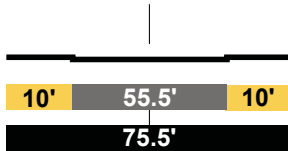
Bloom - Leroy



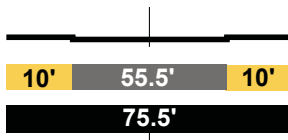
### DARWIN

Local

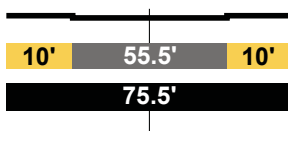
Ave.17 - Ave.19



Ave.19 - Ave.20



Ave.20 - Ave.21

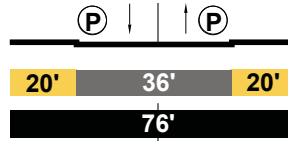


## PROPOSED

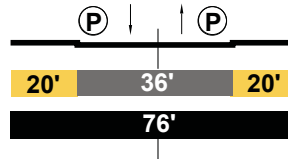
### CARDINAL STREET

Local Modified

Elmyra - Bloom



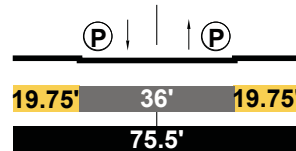
Bloom - Leroy



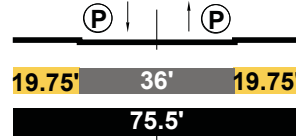
### DARWIN

Local Modified 3

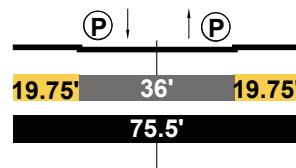
Ave.17 - Ave.19



Ave.19 - Ave.20



Ave.20 - Ave.21



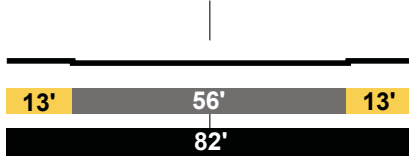


# East - West Streets (Looking West)

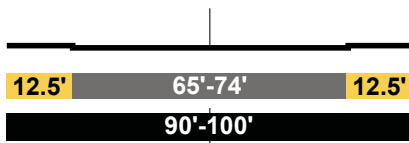
## EXISTING

### FIGUEROA Major Hwy Class II

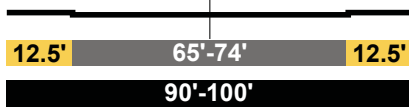
Ave. 20 into (San Fernando Rd) - Ave. 22



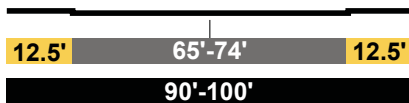
Ave.22 - Ave.26



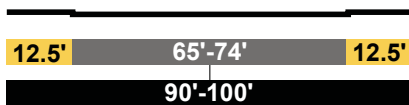
Ave.26 - Ave.28



Ave.28 - River



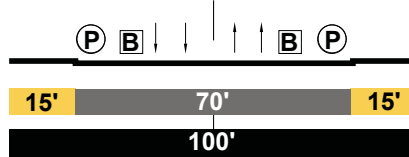
River - Cypress



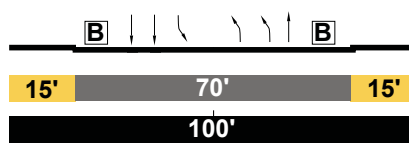
## PROPOSED

### FIGUEROA Secondary Modified 3

San Fernando Rd - Ave. 22

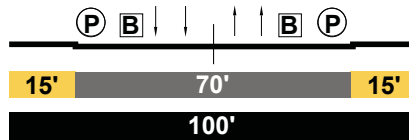


Ave.22 - Ave.26

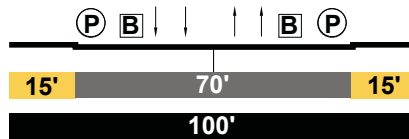


### FIGUEROA Secondary Modified 1

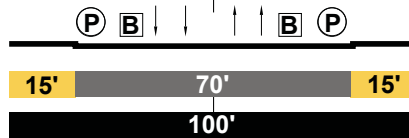
Ave.26 - Ave.28



Ave.28 - River



River - Cypress

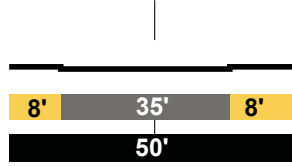


# East - West Streets (Looking West)

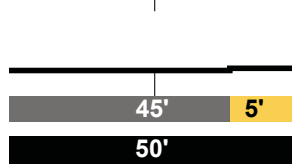
## EXISTING

### HUMBOLDT Local

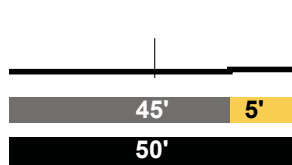
Ave. 19 - San Fernando Rd



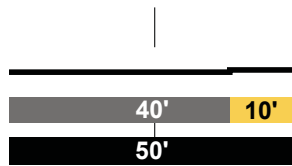
San Fernando Rd - Ave. 21



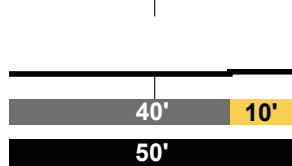
Ave. 21 - Ave. 23



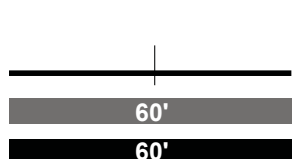
Ave. 23 - Ave. 25



Ave. 25 - Ave. 26



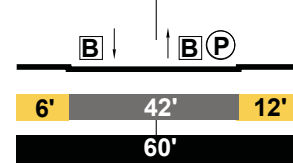
Ave. 26 - Ave. 30



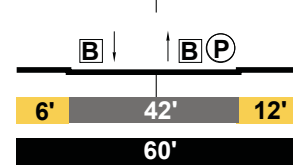
## PROPOSED

### HUMBOLDT Local Modified

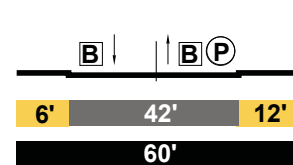
Ave. 19 - Ave. 20



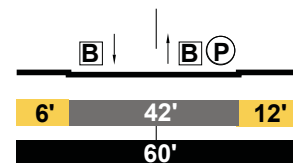
Ave. 20 - Ave. 21



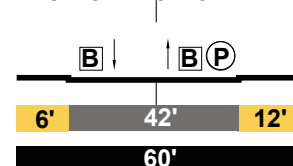
Ave. 21 - Ave. 23



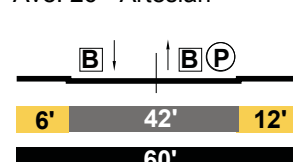
Ave. 23 - Ave. 25



Ave. 25 - Ave. 26



Ave. 26 - Artesian

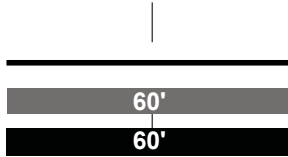


# East - West Streets (Looking West)

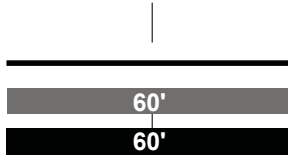
## EXISTING

### HUMBOLDT Local

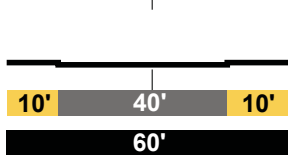
Artesian - Ave. 30



Ave. 30 - Ave. 31

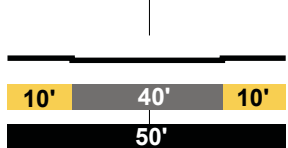


Ave. 31 - Ave. 33



### LACY STREET Local

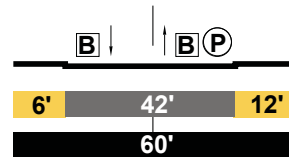
Ave. 26 - Ave. 33



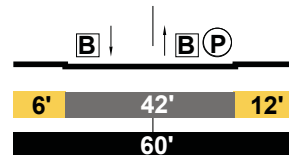
## PROPOSED

### HUMBOLDT Local Industrial

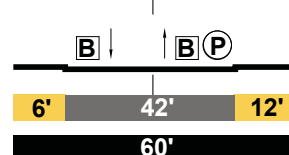
Artesian - Ave. 30



Ave. 30 - Ave. 31

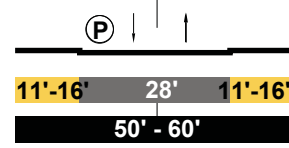


Ave. 31 - Ave. 33



### LACY STREET Local Modified

Ave. 26 - Ave. 33

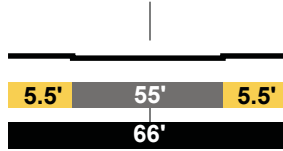


# East - West Streets (Looking West)

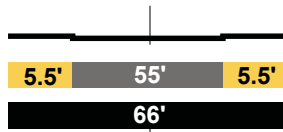
## EXISTING

### MAGDALENA Local

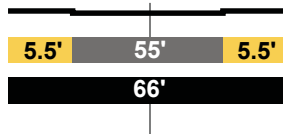
Elmyra - Ann



Ann - Bloom



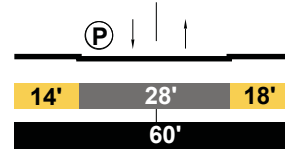
Bloom - Leroy



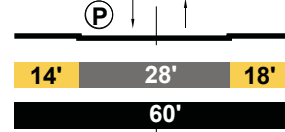
## PROPOSED

### MAGDALENA Local Modified

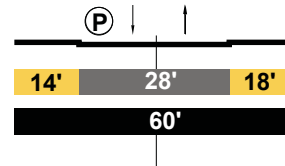
Elmyra - Ann



Ann - Bloom



Bloom - Leroy

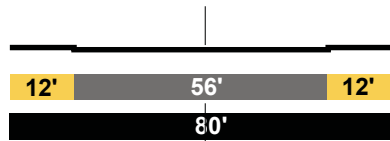


# East - West Streets (Looking West)

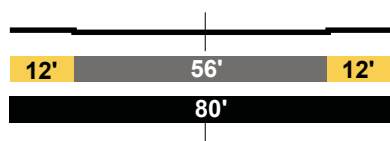
## EXISTING

### MAIN Secondary Hwy

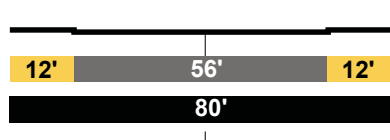
Vignes - College



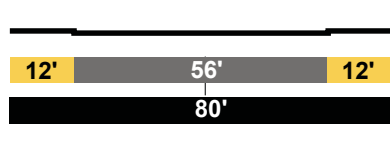
College - Roundout



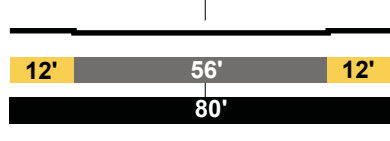
Roundout - Llewellyn



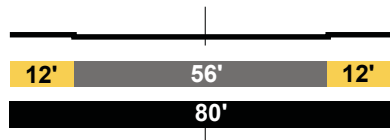
Llewellyn - Elmyra



Elmyra- Ann



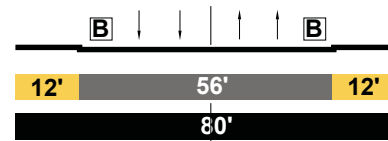
Ann- Bloom



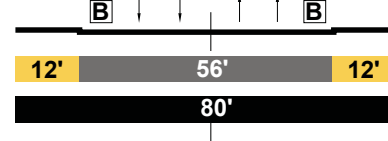
## PROPOSED

### MAIN Secondary Modified 3

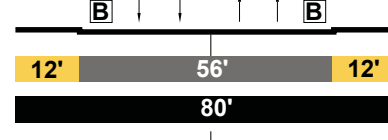
Vignes - College



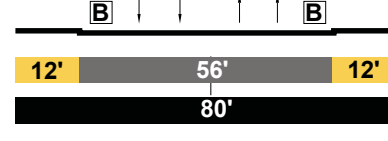
College - Roundout



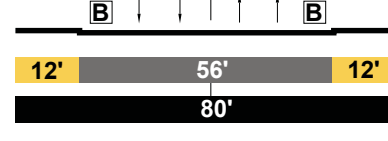
Roundout - Llewellyn



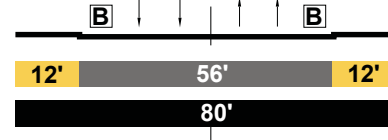
Llewellyn - Elmyra



Elmyra- Ann



Ann- Bloom



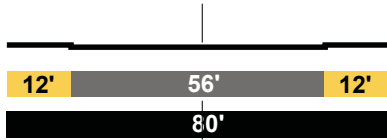


# East - West Streets (Looking West)

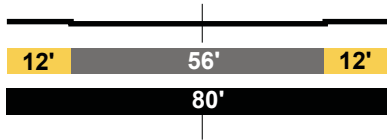
## EXISTING

### MAIN Secondary Hwy

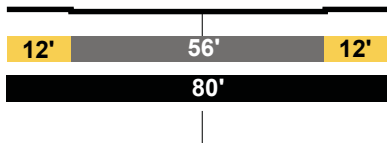
Bloom - 150' east of Bloom



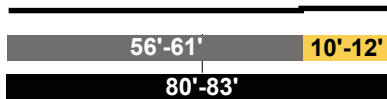
150' east of Bloom - 400' east of Leroy



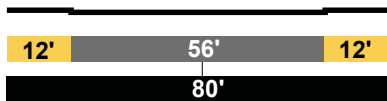
400' east of Leroy - Wilhardt



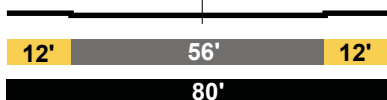
Wilhardt - Albion



Albion - Gibbons



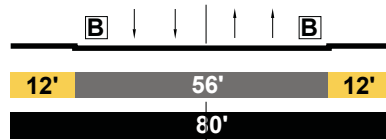
Gibbons- Lamar



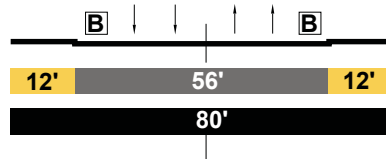
## PROPOSED

### MAIN Secondary Modified 3

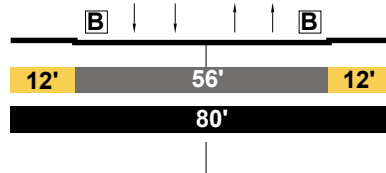
Bloom - 150' east of Bloom



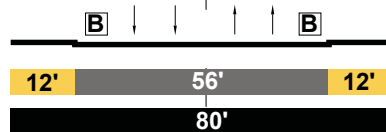
150' east of Bloom - 400' east of Leroy



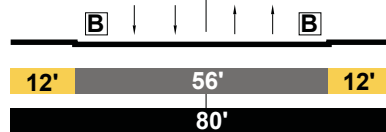
400' east of Leroy - Wilhardt



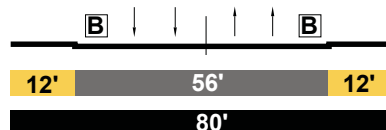
Wilhardt - Albion



Albion - Gibbons



Gibbons- Lamar

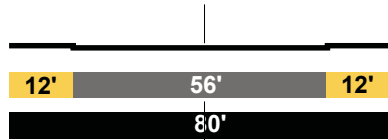


# East - West Streets (Looking West)

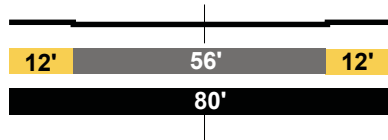
## EXISTING

### MAIN Secondary Hwy

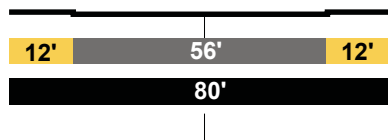
Lamar - Ave 17



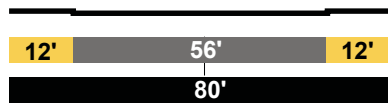
Ave 17 - Clover



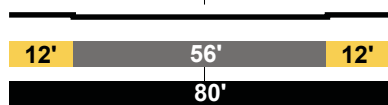
Clover - Ave 19



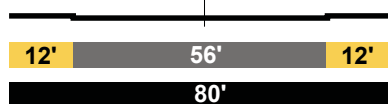
Ave 19 - Moulton



Moulton - Ave 20



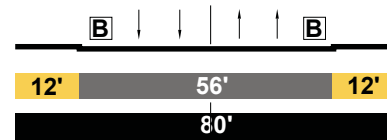
Ave 20 - Ave 21



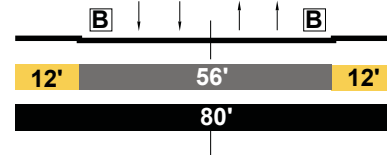
## PROPOSED

### MAIN Secondary Modified 3

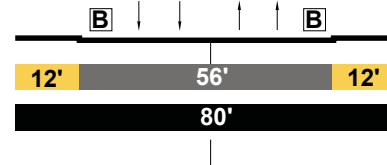
Lamar - Ave 17



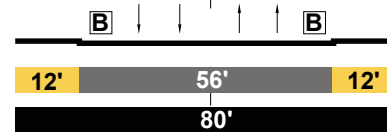
Ave 17 - Clover



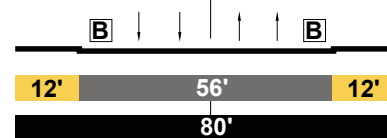
Clover - Ave 19



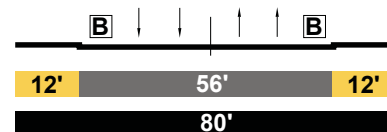
Ave 19 - Moulton



Moulton - Ave 20



Ave 20 - Ave 21

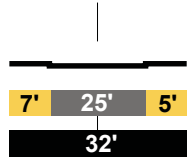


# East - West Streets (Looking West)

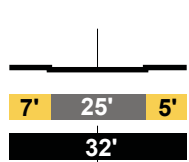
## EXISTING

### MOZART Local

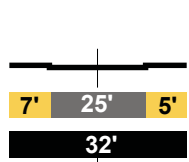
Ave.16 - Ave.17



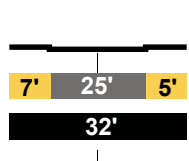
Ave.17 - Ave.18



Ave.18 - Ave.19



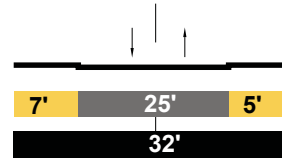
Ave.19 - Ave.20



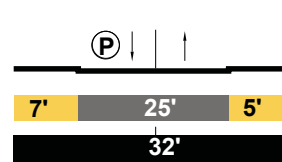
## PROPOSED

### MOZART Local Modified

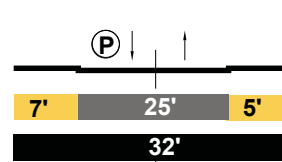
Ave.16 - Ave.17



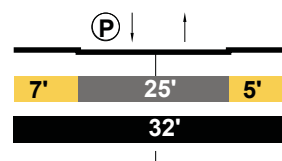
Ave.17 - Ave.18



Ave.18 - Ave.19



Ave.19 - Ave.20

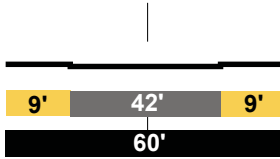


# East - West Streets (Looking West)

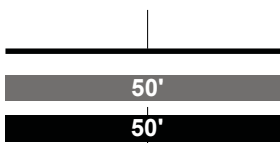
## EXISTING

### NAUD Collector

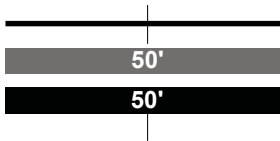
Ann - Sotello



Mesnagers - Wilhardt

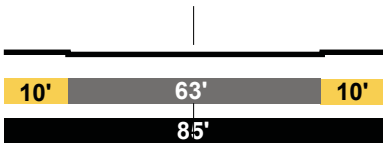


Wilhardt - (LA River)

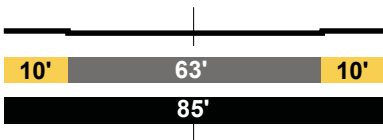


### PASADENA Secondary Hwy

Broadway - Ave. 18



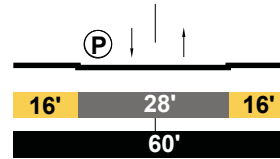
Ave. 18 - Ave. 19



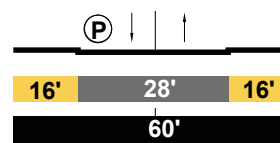
## PROPOSED

### NAUD Local Modified

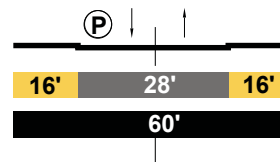
Ann - Sotello



Mesnagers - Wilhardt

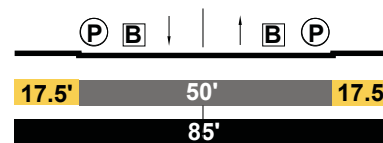


Wilhardt - (LA River)

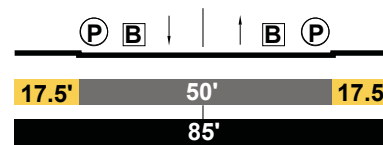


### PASADENA Collector Modified

Broadway - Ave. 18



Ave. 18 - Ave. 19

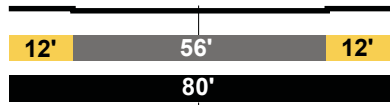


# East - West Streets (Looking West)

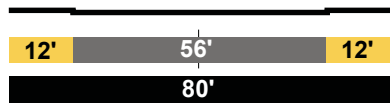
## EXISTING

### PASADENA Secondary Hwy

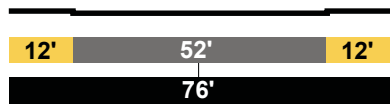
Ave. 19 - Ave. 20



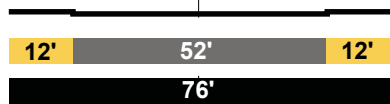
Ave. 20 - Ave. 21



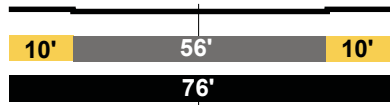
Ave. 33 - Ave. 34



Ave. 34 - Ave. 35



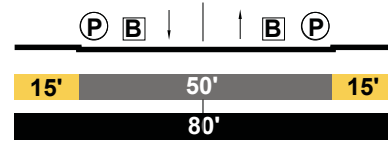
Ave. 35 - (Arroyo Seco/Pasadena Fwy.)



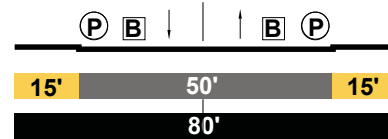
## PROPOSED

### PASADENA Collector Modified

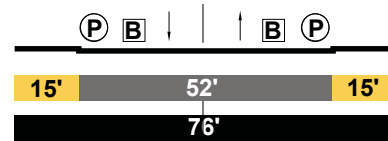
Ave. 19 - Ave. 20



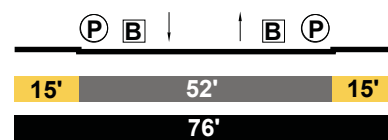
Ave. 20 - Ave. 21



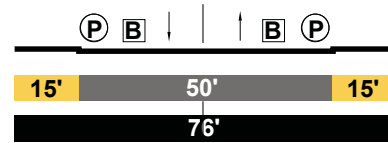
Ave. 33 - Ave. 34



Ave. 34 - Ave. 35



Ave. 35 - (Arroyo Seco/Pasadena Fwy.)



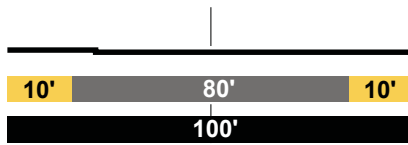
# East - West Streets (Looking West)

## EXISTING

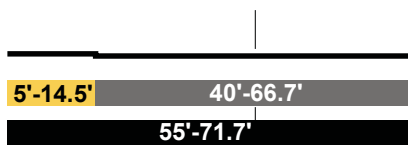
### SPRING

#### Major Class Hwy II

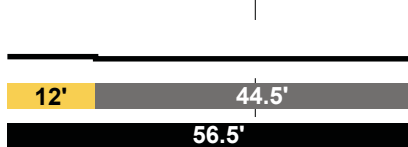
College - Roundout/Elmyra



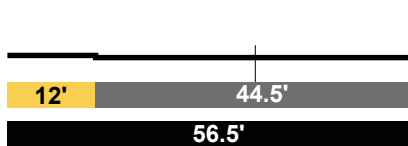
Roundout/Elmyra- Ann



Ann - Sotello



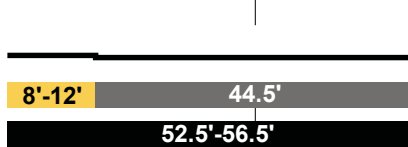
Sotello - Mesnager



Mesnager - Baker



Baker - Wilhardt

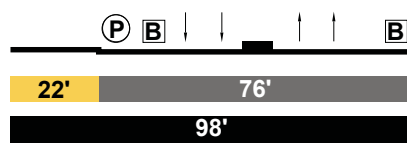


## PROPOSED

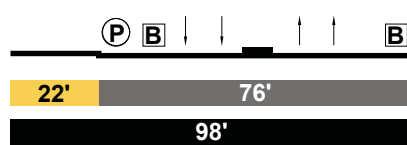
### SPRING

#### Secondary Modified 5

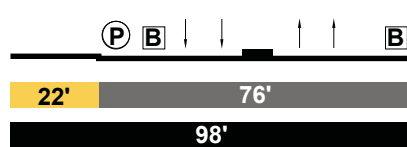
College - Roundout/Elmyra



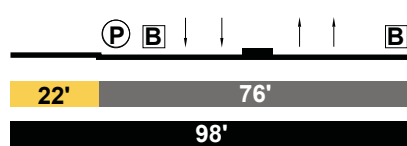
Roundout/Elmyra- Ann



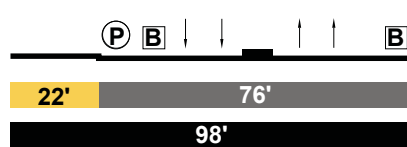
Ann - Sotello



Sotello - Mesnager



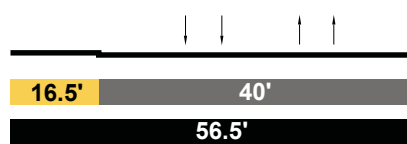
Mesnager - Baker



### SPRING

#### Secondary Modified 4

Baker - Wilhardt



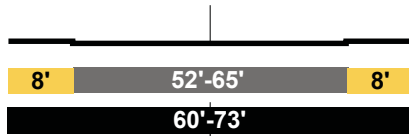


# East - West Streets (Looking West)

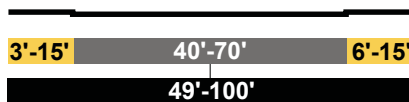
## EXISTING

### SPRING Major Class Hwy II

Wilhardt - Aurora

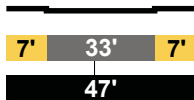


Aurora - Ave. 18



### WEYSE STREET Collector

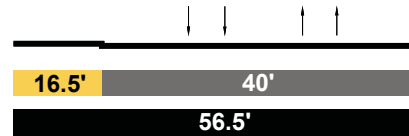
Ann - (ends before Sotello)



## PROPOSED

### SPRING Secondary Modified 4

Wilhardt - Aurora



Aurora - Ave. 18

### BRIDGE SECTION Secondary Modified 3

### WEYSE STREET Local Modified

Ann - (ends before Sotello)

