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PROJECT REPORT ON FAIRFAX-SANTA MONICA STATION AREA PLANNING

Prepared by the Los Angeles County  
Department of Regional Planning  
for the Southern California Rapid Transit District

METRO RAIL PROJECT  
Task Number 18CAA23

May 15, 1984

## INTRODUCTION

This Project Report on the Fairfax-Santa Monica Station Area Planning Program represents the final element in Los Angeles County-Southern California Rapid Transit District contract number 2611, as amended, for station area planning in West Hollywood. It incorporates four reports previously submitted under separate covers as distinct contract task components for the Preliminary Engineering Phase of the Metro Rail Project. The four reports, incorporated as four chapters of this report, include:

Station Area Boundaries Report 18BAH2745;

Land Use Plan Alternatives Report 18BAH2947;

Preliminary Station Area Plan 18BAH2947; and,

Station Area Plan Implementation Report 18BAH2946.

The Station-Area Boundaries Report, Chapter I, delineates the area included for analysis and preliminary land use planning studies. Study area boundaries are located at this initial stage of the planning process to include all property and existing development that may be affected by the introduction of the station at Fairfax Avenue and Santa Monica Boulevard. The boundaries of the study area do not necessarily define the limits or specific boundaries of the Station Area Plan developed in a subsequent phase of the planning project.

Chapter II discusses alternative concepts for the Station Area Plan, presenting three different land use and development policy schemes for the Station Plan area. The alternatives are developed from a policy foundation in the adopted West Hollywood Community Plan. Alternative policies are presented as supplementary to the Community Plan's basic policy with regard to land use, intensity of development, building heights, parking requirements and design and use standards.

The Station Area Plan alternatives were reviewed and extensively discussed by the West Hollywood Citizens Advisory Committee in a series of meetings and Regional Planning Department staff presentations between November, 1982 and April, 1983. Citizens Advisory Committee recommendations on a preferred alternative formed the basis for development of the Station Area Plan.

Chapter III presents the Preliminary Fairfax-Santa Monica Station Area Plan. The Plan is structured as a set of Metro Rail station-related policies and guidelines which complement the adopted policies and regulations of the West Hollywood Community Plan. It articulates a strategy for development of the area surrounding the Metro Rail station and proposes specific design and development guidelines.

DK

HAYVENHURST  
HTS  
BLVD



SANTA

CRESCENT

NORTON  
AVE



MONICA

LAUREL AVE

AVE



EDINBURGH AVE

AVE



HAYWORTH

ORANGE GROVE  
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GROVE AVE

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HAMPTON  
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LEXINGTON

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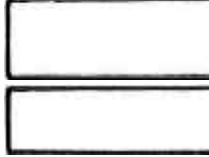
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CURSON AVE

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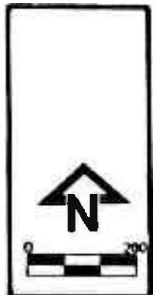
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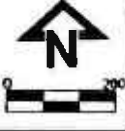
GENESEE

SPAULDING

STANLEY



WILLOUGHBY



# FAIRFAX-SANTA MONICA STATION AREA

On June 10, 1983 the West Hollywood Community Standards District was adopted by the Los Angeles County Board of Supervisors, implementing the West Hollywood Community Plan. The Community Standards District incorporated the station area boundary delineated in the West Hollywood Community Plan, which encompasses a smaller area than that defined for the preceding study and alternatives analysis. This boundary has therefore been established as the Station Area Plan boundary.

The West Hollywood Citizens' Advisory Committee reviewed and commented on drafts of the Station Area Plan between May and July of 1983, and a final draft of the Preliminary Fairfax-Santa Monica Station Area Plan was completed on August 31, 1983.

The final chapter of the Project Report is a proposed implementation component consisting of a new Transit Corridor Mixed Use Zone and Transit Corridor Community Standards District amendments to the Zoning section of the Los Angeles County Code.

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## FAIRFAX-SANTA MONICA STATION STUDY AREA BOUNDARIES

The Fairfax-Santa Monica Station Study area boundaries extend from Fountain Avenue on the north to Willoughby Avenue and the City/County boundary on the south, and generally between Crescent Heights Boulevard on the west and Spaulding Avenue on the east. Plan area boundaries extend further east and west to include the commercial properties lining Santa Monica Boulevard for approximately two blocks (500 to 600') beyond Crescent Heights and Spaulding respectively. The western Plan boundary south of Santa Monica Boulevard runs at mid-block between Crescent Heights and Havenhurst Drive, while the northeast corner of the Plan area is cut in from Spaulding and Fountain along Genesse Avenue and mid-block between Lexington and Hampton Avenues.

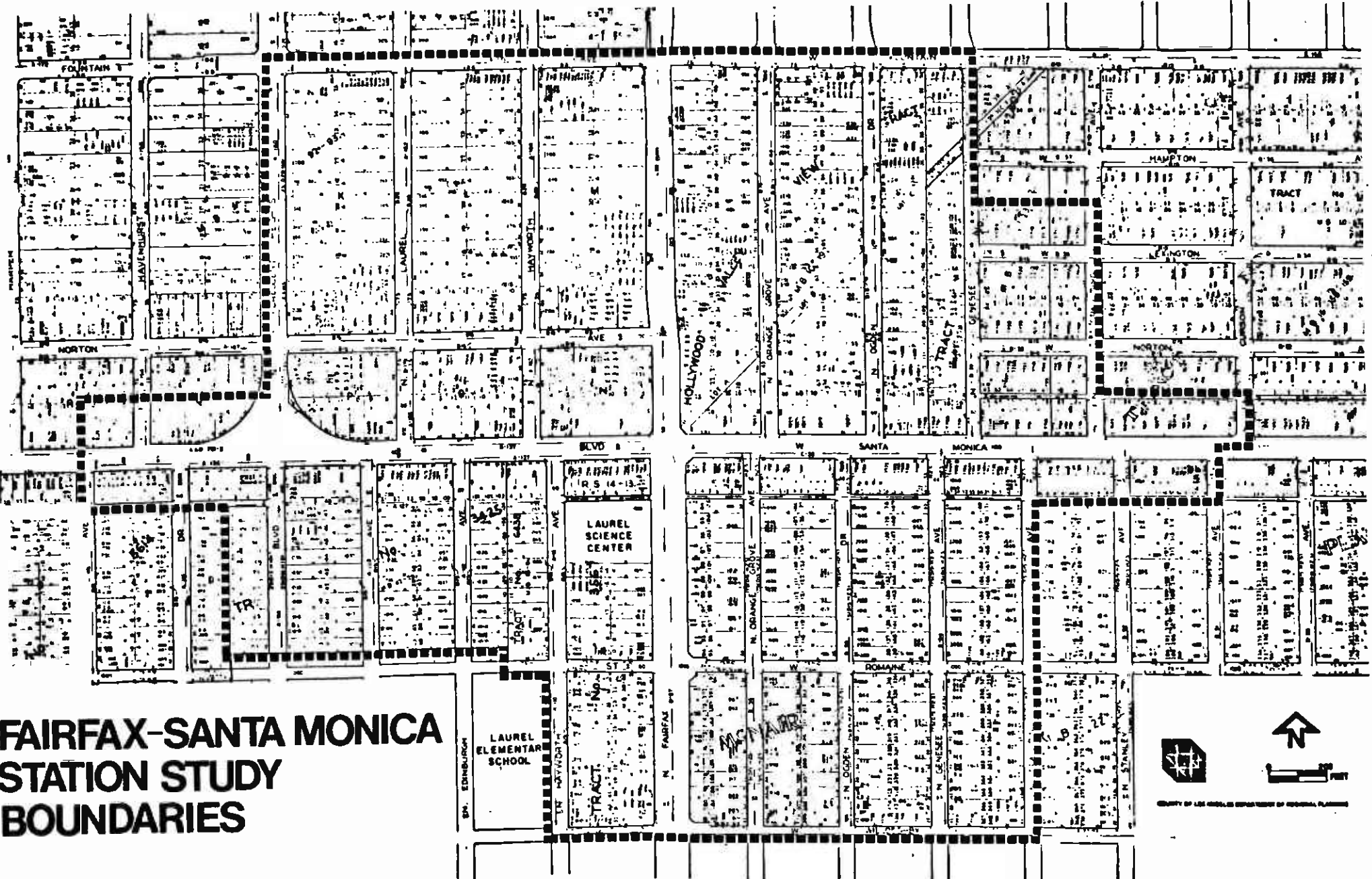
To facilitate its description, the Station Plan area is divided into four sectors by lines drawn north-south on Fairfax and east-west on Santa Monica. The northwest sector is therefore the Plan area north of Santa Monica and west of Fairfax, while the northeast sector is north of Santa Monica and east of Fairfax. The southwest and southeast sectors are south of Santa Monica, and west and east of Fairfax respectively.

### Northwest Sector

This area includes a C-3-CRS (Commercial-Residential) zoned strip of properties lining the north side of Santa Monica Boulevard and extending some 500 feet northward from Santa Monica along the west side of Fairfax. These commercial strip areas are one lot in depth from Santa Monica and Fairfax frontages, respectively. A parking overlay zone (intended to serve adjacent commercial uses) covers the entire strip of properties to the rear of the commercially zoned strip of properties which front on Santa Monica Boulevard, though the overlaid properties are developed in residential uses; most of them at multi-family densities. The remainder of the northwest sector is developed almost completely in medium density apartment and condominium structures. This residential area is zoned R-4 (Unlimited Residence), and is afforded density bonus opportunities by the West Hollywood Community Plan for development of rental units.

The commercial development lining the north side of Santa Monica Boulevard varies in size, type and condition from building to building. The boundary of the Station Plan area is extended westward to a point, at La Jolla Avenue, where the nature and condition of commercial development begin a noticeable change toward greater continuity of improved facades and buildings, with a higher percentage of new structures and fewer buildings in poor condition. A vacant combination of parcels at the northwest corner of Santa Monica and Havenhurst, currently used as a surface parking lot, may have potential related to station area development.





# FAIRFAX-SANTA MONICA STATION STUDY BOUNDARIES



CITY OF LOS ANGELES DEPARTMENT OF PUBLIC PLANNING

## Northeast Sector

Approximately 2,000 feet of the commercially zoned and developed strip of properties lining the north side of Santa Monica Boulevard is included in this sector, extending it east to Curson Avenue. The commercial strip is zoned C-3-CRS to one lot depth from Santa Monica as is the area on the northeast corner of Santa Monica and Fairfax extending approximately 500 feet north and east of the intersection. Development on these commercially zoned properties is varied in type and size, and is generally in fair condition. The only vacant property in this series of commercial developments is a group of lots comprising approximately 150 feet of frontage on Santa Monica Boulevard at Curson Avenue. There appear to be some opportunities for improvement or development of properties at various locations between Fairfax and Curson. A heavily used parking lot on the northeast corner of Fairfax and Santa Monica may present development opportunities relative to the Metro Rail station area.

The remainder of the northeast quadrant is zoned R-4 (Unlimited Residence) and residentially developed at a mixture of densities from single family homes to high density complexes. The eastern Plan area boundary is located slightly beyond 1/4 mile from the proposed station location and includes areas which are generally underdeveloped relative to zoning potential.

## Southwest Sector

The southern extent of this area is defined by the Los Angeles City/County boundary at Willoughby between Fairfax and Hayworth and follows this boundary line north on Hayworth and along Romaine Street. The western edge of the area then runs northward at mid-block between Crescent Heights and North Havenhurst Drive, then jogs westward to include a 500 foot portion of the one-lot deep commercial strip on the south side of Santa Monica Boulevard.

The Santa Monica Boulevard commercial strip in this sector is a fairly continuous series of structures of various size lining the sidewalks along Santa Monica Boulevard. Commercial development along the west side of Fairfax is a collection of unrelated commercial and service establishments interspersed with a school and a few residential structures. While Santa Monica's street front functions to some degree as a pedestrian-oriented commercial area, the Fairfax frontage presents no continuous pedestrian-oriented series of businesses or storefronts.

The condition of structures and facades along the south side of Santa Monica varies from poor to excellent; the majority in fair condition with some potential for improvement. As is the case in the northwest sector, the western limit of the Plan area along this commercial strip is located where an apparent transition in

building quality and number of new structures begins and continues to the west. All commercial zoning is C-3-CRS, and parking overlay zones cover the row of residentially developed parcels south of the Santa Monica Boulevard commercial properties.

The remainder of the southwest quadrant is R-4 zoned property developed predominantly at high densities. A few parcels developed at lower densities could be redeveloped to their higher density potentials. A roughly 40,000 square foot property to the south of Santa Monica Boulevard, fronting on Fairfax and currently occupied by a series of small buildings housing a specialized school use, appears to have potential for development interrelated with the proposed Metro Rail station. This property is zoned C-3-CRS on the half fronting on Fairfax while its western half is zoned R-4.

### Southeast Sector

The relatively continuous strip of commercial development along the south side of Santa Monica Boulevard in this sector is composed of a series of very narrow (25 to 30 foot) parcels and development frontages. Buildings in this strip are generally in fair to poor condition, with some potential for improvement. Some of the structures and facades have been remodeled and improved substantially but virtually no new structures have been built in recent years. A series of small lot commercial structures in fair to poor condition on the first two blocks east of Fairfax may present station-related redevelopment opportunities where lots can be consolidated.

Commercial and light industrial uses line the Fairfax frontages in this sector on a series of 50 foot by 60 foot lots which, as on the other side of Fairfax, do not form a continuous commercial strip. As on Santa Monica, the commercial properties along the first block south of Santa Monica, to Romaine Street, hold potential for redevelopment to station-related facilities and uses.

The development which completes the sector is a combination of high and medium density residential which has some potential for redevelopment at increased densities under its R-4 zoning. East of Spaulding Avenue the development pattern is substantially more built-out to its R-4 high density zoning potential.

CHAPTER II

LAND USE PLAN ALTERNATIVES

TASK 18BAH2947

## INTRODUCTION

This chapter discusses alternative concepts for a Fairfax-Santa Monica Station Area Plan. The Station Area Plan will complete the detailed planning for the metro rail station anticipated in the West Hollywood Community Plan with station area-specific policy and implementation. The Plan is intended to lay out a strategy for the most advantageous development of the area surrounding the metro rail station facility with respect to the needs and objectives of the West Hollywood community as well as those of the overall metro rail system. From the community perspective, the Plan must protect the existing positive aspects of the commercial and residential areas likely to be affected by the station's operation, and form the basis for promoting and guiding new development that both serves local needs and injects vitality into the community. As an element of the regional metro rail system, the Fairfax-Santa Monica Station Plan area will become the location of significant mass transit station activities. The station area could, however, become a transit destination itself, attracting still greater number of people to the station and surrounding area. Should the station area develop into a high intensity location it would support regional metro rail system objectives of promoting concentration of development in urban centers and generally intensifying a transit-centered urban corridor. Merging these diverse sets of objectives into a planning program which can work to the mutual benefit of West Hollywood and the metro rail system is therefore the general goal of the station area planning process.

The Station Area Plan will set forth the potentials and define the limits of future development in the station area. It is also intended to be the key instrument in governing the transition of the Plan area from its present state to a future role as a mass transit station area.

Development planning mechanisms including land use controls, intensity limits and incentives, design and use guidelines, circulation plans, parking regulations and public facilities improvement programs will implement the Station Area Plan's policies on areawide and development project levels.

The planning framework is intended to act as the foundation and basic guidelines for any future development plans created by the Southern California Rapid Transit District or other agencies for property within the Plan area. In this hierarchy of planning policy the Station Area Plan will set the tone, direction and limits for RTD station master planning, which will in turn govern RTD development policy and actions in the future.

### Plan Alternatives

The following discussion of alternative land use concepts presents three different land use and development policy schemes for the Fairfax-Santa Monica Station Plan area. All three alternatives are developed from a policy foundation in the existing West

Hollywood Community Plan. Alternative policies are presented as supplementary to the Community Plan's basic policy in the areas of land use, intensity of development, building heights, parking requirements, and design and use standards. The primary means of promoting and implementing the alternative Plan proposals would be through the offering of significant incentives, in the form of increased development intensity potentials and revised building height, design and parking requirements, in exchange for developer adherence to the specific design and use guidelines of the Plan alternatives.

The three alternative land use plan concepts are based on a metro rail station location in the right-of-way of Fairfax Avenue extending approximately 200 feet north and 400 feet south of Santa Monica Boulevard, with the track and station below street level.

An array of development and design options are presented in the context of the three alternative land use plan concepts ranging from the relatively limited accommodation of the station facility proposed in Alternative I to the more extensive concepts presented in Alternative II and III.

Alternative I would increase the development potential of the properties immediately adjacent to the proposed station to a moderate degree in order to enhance the joint development potential of the station area. This concept presumes that the general activities associated with the metro rail station would be limited to the basic transportation function of the station facility and that the adjacent new development would be focused on the station. The role of the Fairfax-Santa Monica Station in this alternative is seen as primarily a point in the overall system through which riders either pass or gain access to other destinations in the system. The station area would not constitute a destination for riders other than those who enter the system at this point as commuters or transfer to or from other transit modes.

The density bonus proposals of Alternative I would increase the mixed use development potential near the station to some 200,000 square feet above that currently permitted by West Hollywood Community Plan policy. This level of station-related development potential would not anticipate or attempt to accommodate the activity levels that an auxiliary rail line and transfer station near Fairfax and Santa Monica would introduce.

Alternative II presents a concept for major redevelopment of an area encompassing the first two to three blocks surrounding the proposed station. With the objective of promoting development of an intense and diverse concentration of facilities and attractions capable of drawing substantial numbers of people to the Fairfax-Santa Monica Station area, this alternative would increase development potential near the station to twice that currently prescribed by the West Hollywood Community Plan. The increases in development potentials for these designated core

properties would be granted in exchange for developers' adherence to design and use guidelines, which are structured to promote high levels of commercial and entertainment diversity in a vital pedestrian-oriented urban setting. Residential development potentials would be similarly increased in the designated Mixed Commercial zones and would be increased up to 50% above Community Plan limits in the residential zones within 750 feet of the station. The land use and development intensity scheme of Alternative II focuses higher development potential and its anticipated high activity levels near the station, while attempting to preserve the Community Plan level of development intensity for the surrounding neighborhoods and business areas.

This alternative development concept could accommodate the introduction of an auxiliary or other connecting rail transit line or system as long as the point of interface with the Fairfax metro rail line would be at the Fairfax-Santa Monica Station location.

Alternative III proposes to promote the intensification of development adjacent to the metro rail station to create a focal point of new economic and pedestrian activity which can become a cornerstone for the revitalization and eventual redevelopment of the Santa Monica Boulevard commercial strip within the Station Plan area. The focus of design and use guidelines for the Plan area is an improvement of the pedestrian-oriented commercial environment of Santa Monica Boulevard with special consideration given to the immediate station area. Development incentives include increased intensity and broader design potentials as well as the introduction of street and parking facilities improvements along Santa Monica Boulevard.

This alternative would increase the development intensity potential in mixed commercial zones to twice that prescribed by the Community Plan in the Station Plan area. These increases would be spread along Santa Monica Boulevard properties, with a focal point at the intersection of Fairfax and Santa Monica. This concept could be adapted to the introduction of an auxiliary or other connecting rail transit line depending on the location of the facilities or any transfer stations. Such adaptation could, however, require change in land use pattern or development intensity limits in the vicinity of the additional facilities or station.

## EXISTING DEVELOPMENT IN THE STATION PLAN AREA

### Commercial Areas

The commercial uses within the Station Plan area are located in strips along Santa Monica Boulevard and Fairfax Avenue. Narrow frontages and shallow depths of parcels along Santa Monica Boulevard create small parcels and place substantial constraints on redevelopment. Structures on these parcels are often nonconforming with regard to lot coverage and building codes. More than 75% of the existing commercial structures were built prior to 1958, when parking standards were first adopted. This has resulted in inadequate off street parking facilities, with patrons of businesses often being forced to park in adjacent residential areas.

Many buildings appear to be in poor condition with cluttered signs and a general lack of storefront design continuity. Some businesses such as gas stations and repair shops, as well as parking lots and other such open areas, are unrelated to the commercial and entertainment uses which contribute elements of interest and continuity to street level settings.

The commercially developed areas contain approximately 500,000 square feet of commercial floor space; far below the potential permitted. No significant commercial development has taken place during the past two decades, due to a combination of economic factors and certain development restrictions and requirements. The majority of structures consist of one or two stories, again, below the permitted maximum building height.

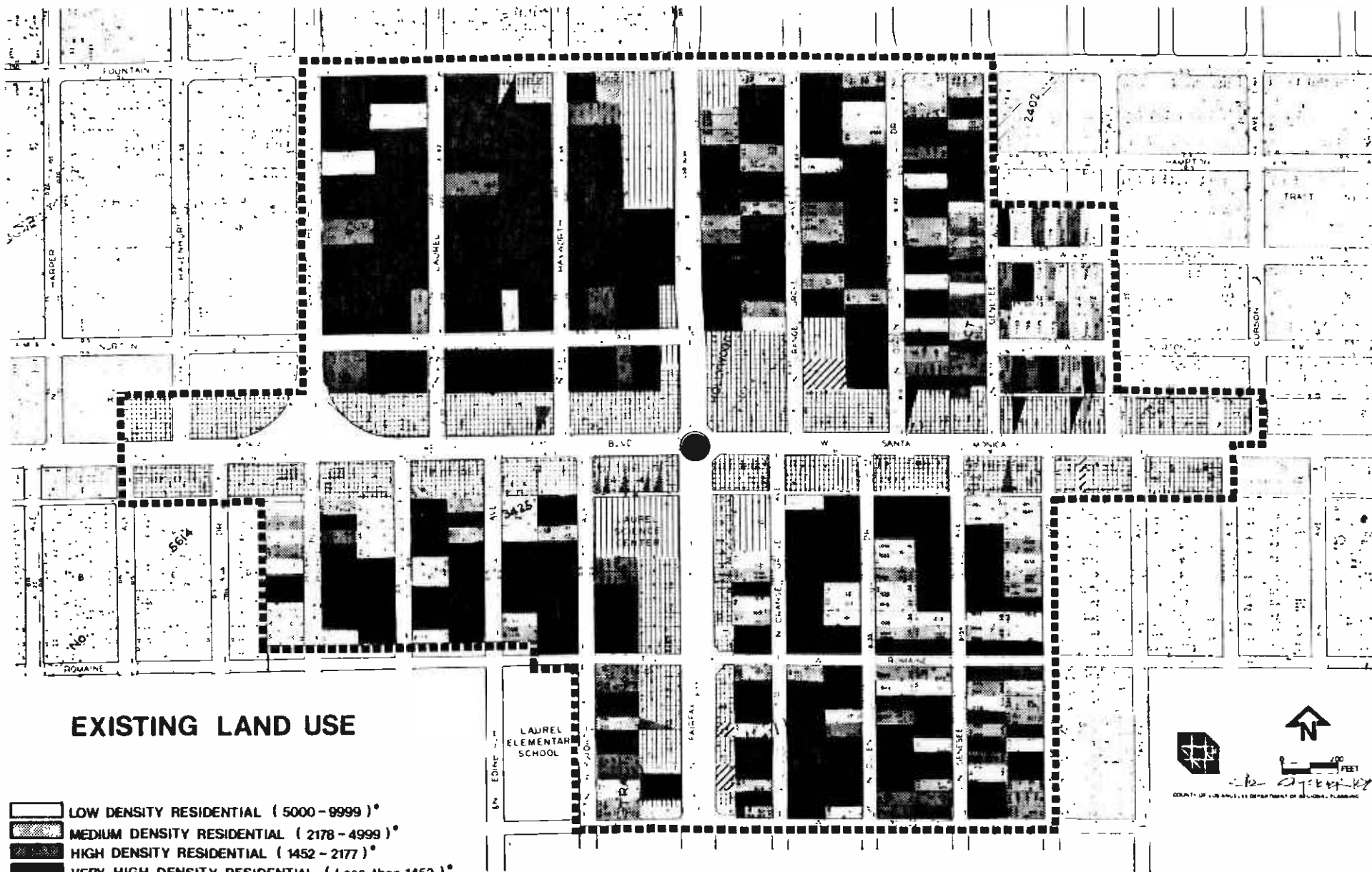
### Residential Areas

The residential neighborhoods within the Station Plan area generally consist of multi-family residential uses with a few older single and two family residences. During the past 35 years zoning in this area has permitted the construction of 2 and 3 story apartments at high densities; sometimes approaching 70 units to the acre. Lot sizes, with the exception of those in the northwest quadrant are generally small (5,000 to 6,000 square feet) and front on narrow streets (30 foot roadways). Many residential developments have inadequate off-street parking facilities because they were built prior to the adoption of residential parking requirements in 1974. Building heights within the Station Plan area range between 15 and 40 feet, and only in a few instances exceed 40 feet.








Presently there are no vacant lots in the Station Plan area, though approximately 25% of parcels are underdeveloped relative to current zoning. A number of factors, including but not limited to reconstruction and land costs, zoning restrictions and rent levels, have discouraged or diminished the economic feasibility of removing or extensively redeveloping older structures in order to increase density or modernize buildings. In some areas existing development densities exceed those



permitted by current zoning, and offer no redevelopment advantage. Other development remains unchanged due to the significantly higher on-site parking requirements to which new or substantially remodeled structures would have to conform. Still other existing developed properties would be subject to increased building setbacks, which would reduce development potential on already small lots. In addition, when related to the increasingly high costs of land acquisition and development, existing apartment unit and commercial floor space rent levels often appear too low for new project feasibility.



**EXISTING LAND USE**

-  LOW DENSITY RESIDENTIAL ( 5000 - 9999 )\*
-  MEDIUM DENSITY RESIDENTIAL ( 2178 - 4999 )\*
-  HIGH DENSITY RESIDENTIAL ( 1452 - 2177 )\*
-  VERY HIGH DENSITY RESIDENTIAL ( Less than 1452 )\*
-  COMMERCIAL
-  INDUSTRIAL
-  PUBLIC, INSTITUTIONAL

**FAIRFAX-SANTA MONICA  
STATION AREA PLAN**



\* (Square feet of lot area per dwelling unit )

## WEST HOLLYWOOD COMMUNITY PLAN POLICY WITHIN THE STATION PLAN AREA

The adopted Community Plan designates Santa Monica Boulevard as well as Fairfax Avenue properties in a Mixed Commercial land use category. For commercial developments, office or retail, height is restricted to 45 feet and floor area ratio (FAR) to two times the buildable lot area. The Community Plan however also encourages combined commercial/residential developments by offering height and floor area ratio bonuses. If at least 1/3 of the total floor area of the project is utilized for apartments or condominiums, maximum height may be increased to 60 feet and overall floor area ratio to 3. Lot coverage is permitted up to 90% with the balance as landscaped open space.

The Community Plan designates the residential areas within the Station Plan area in a high density land use category. Permitted density ranges from 30 to 50 dwelling units per net acre and maximum structure height is 45 feet. In the northwest station area quadrant, densities up to 75 units to the net acre are offered as an incentive for the construction of rental housing.

To permit more flexible development standards and to promote the development of unique, landmark structures, the Community Plan encourages "specific plan" projects. On commercially or industrially zoned parcels in excess of 40,000 square feet, development standards, including height limits and floor area ratio, are relaxed. Public hearings and review by the Regional Planning Commission and Board of Supervisors are required as part of the "specific plan" approval process.

Land use policies of the Community Plan applicable to Santa Monica Boulevard and Fairfax Avenue properties are:

- To encourage lot assemblage;

- To develop height control standards to provide for a variety of building heights;

- To revitalize strip commercial areas;

- To encourage mixed use developments; and

- To promote development of buffer areas.

## LAND USE PLAN ALTERNATIVES

### ALTERNATIVE I

#### Concepts and Objectives

(Promote Station Area Joint Development Consistent with West Hollywood Community Plan Policy.)

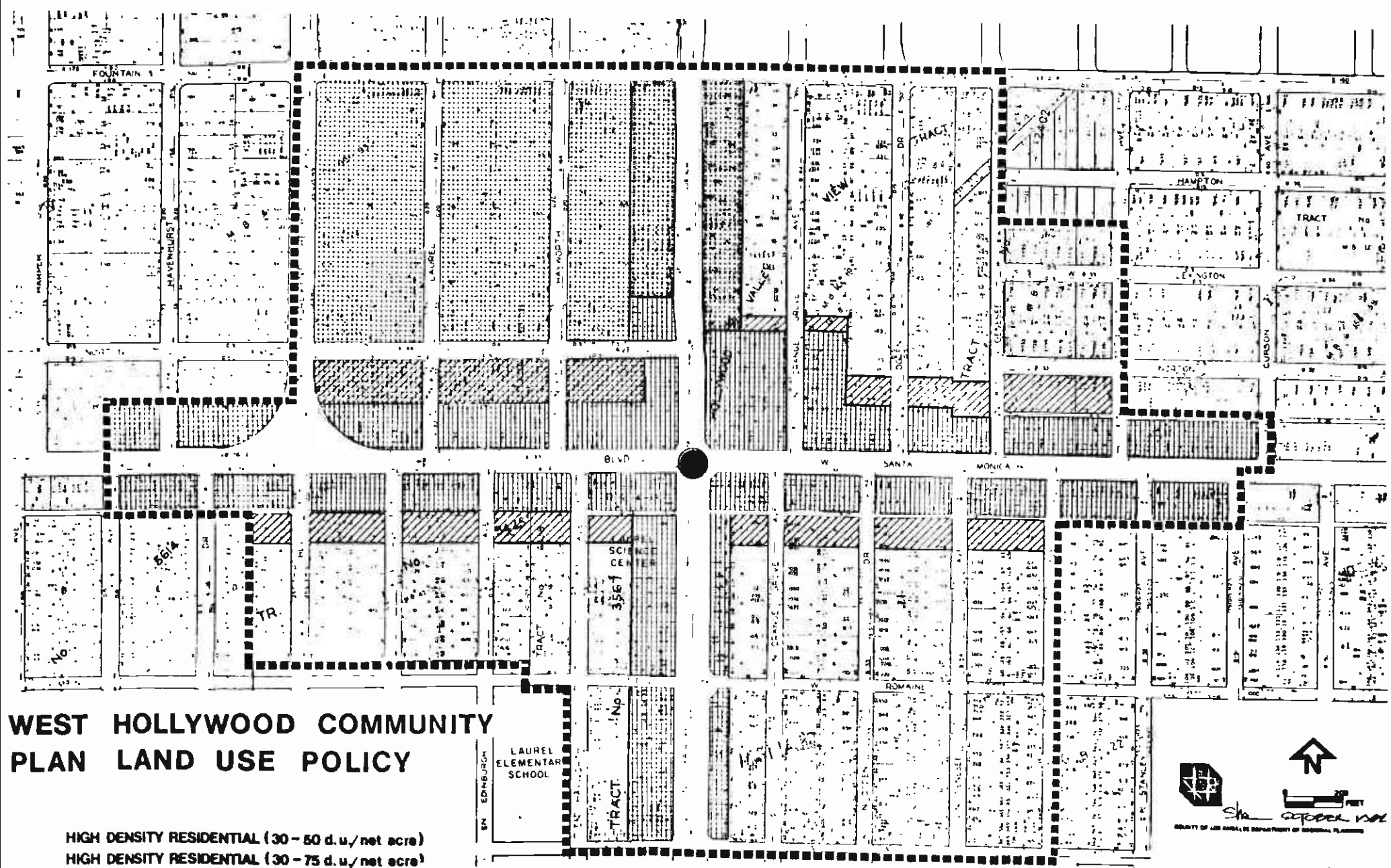
This concept would hold development potential within the bounds set by the land use, intensity and height limitations of the Community Plan while granting specific, limited exceptions in the form of intensity bonuses and parking reductions as incentives for private development of station-enhancing facilities. All potential for development bonuses is confined to the immediate station-surrounding properties with joint development possibilities. Projects proposing to include retail business or other public-oriented space at station or ground level, park and ride space near station entrances or direct pedestrian accessways from new developments or existing neighborhoods to station entrances would be granted intensity bonuses, while new development would be granted parking reductions on the basis of direct or easy access to the station. Permitted building heights would be increased for development receiving bonuses.

The primary objective of this alternative is to encourage and facilitate joint development of station facilities and surrounding new development to create a mutually supportive complex at the intersection of Fairfax and Santa Monica. A further objective of this concept is the introduction of facilities which will create a new focal point of development in West Hollywood while remaining consistent with the character of the station area neighborhood.

#### Station Plan Policy Areas

##### Land Use

The pattern of land use delineated by the West Hollywood Community Plan is unchanged in the proposals of Alternative 1. This concept is limited to enhancement of the joint development potentials of properties adjacent to the proposed metro rail station by presenting possibilities for increased development intensities or reduced parking requirements on properties in Community Plan-designated Mixed Commercial zones. Therefore, the Community Plan pattern of mixed (commercial/residential) land uses lining Santa Monica Boulevard and Fairfax Avenue frontages, with high density residential throughout the remainder of the Station Plan area, is complemented by increased development potential at Fairfax and Santa Monica without departing from the Community Plan's land use pattern.



# WEST HOLLYWOOD COMMUNITY PLAN LAND USE POLICY

- HIGH DENSITY RESIDENTIAL (30 - 50 d.u./net acre)
- HIGH DENSITY RESIDENTIAL (30 - 75 d.u./net acre)
- MIXED COMMERCIAL
- PARKING OVERLAY
- PROFESSIONAL OFFICE OVERLAY

# FAIRFAX-SANTA MONICA STATION AREA PLAN



. Intensity of Development

Alternative 1 offers floor area ratio (FAR) bonus potential in exchange for incorporation of public-serving and station facility-enhancing elements in development proposals (See Alternative I Concept Map). Such beneficial elements may include development of sidewalk or station-level retail commercial, entertainment, recreational or open space facilities which are available and attractive to the general public using the metro rail facilities or adjacent buildings. Intensity bonuses may also be granted to developments which merge small lots into more advantageous size or configuration for redevelopment. The total bonus development potential offered by Alternative I is approximately 200,000 square feet of commercial or mixed commercial/residential floor area.

Other bonus-producing elements of new developments may include pedestrian access enhancement such as mid-block pathways and various direct pedestrian linkages between new buildings and the station or street level station entrances. The bonus potentials for these properties can increase overall FAR from the 2.0 commercial or 3.0 mixed use limits of the West Hollywood Community Plan to maximums of 3.5 for either commercial or mixed use developments as indicated on the Alternative I Concept Map. In conjunction with intensity increases, parking requirements for specific uses would be reduced to improve project feasibility.

. Mixed Use Development

Development incentives and bonuses offered in Alternative I are intended to encourage the creation of a variety of uses and facilities adjacent to the station that will contribute to a lively and highly functional concentration which enhances and expands public use of the station area and metro rail system.

. Use Guidelines

Uses to be permitted in the mixed use and commercial areas designated in Alternative 1 include:

- |                    |   |
|--------------------|---|
| Office;            | generally located above street level, leaving ground floor space for pedestrian oriented commercial uses. |
| Retail sales;      | located at street level with sidewalk exposure, at station level, or near station entrances.              |
| Personal services; | located at street or station level if the nature of the service warrants high exposure.                   |
| Entertainment;     | located at various levels but preferably as part of public street or station-oriented settings.           |

- Restaurants; located at various levels but encouraged at street level. Sidewalk cafes and food shops are also encouraged eating places.
- Recreational; preferably public recreational opportunities easily accessible from public circulation paths such as sidewalks, plazas, station facilities and street bridges.
- Cultural; located according to the nature of the cultural facility and its potential for increasing street environment vitality. Art galleries, performing arts theaters, and exhibition spaces are suited to street level locations.
- Residential; located off Santa Monica or Fairfax frontages and street levels. Residential components of development should be appropriately insulated from business and high activity portions of developments, but should have easy access to commercial and recreational facilities as well as to station entrances.
- Parking; located to best serve, respectively, on-site businesses and office space, residential use, and park-and-ride access to the station. Parking facilities should not interfere with the continuity or function of street level commercial or recreational facilities, and should be segregated according to the type of use they serve.

#### Design Guidelines

- Height Limits; On those properties designated for intensity and parking bonus potentials, the height of new development may reach 100 feet. Height limits specified in the West Hollywood Community Plan would remain unchanged throughout the remainder of the Station Plan area.
- Building Setbacks; All new development on properties adjacent to the station should be required to locate street level facades on the front setback line on Fairfax and Santa Monica frontages. Development on these properties should also extend to side property lines. Rear yard building profiles should step back progressively on higher floors to minimize impacts on adjacent residential development.

Facade  
Design;

Street level facades of new development adjacent to the station should line the inner edge of the sidewalk in a continuous series of retail sales or other commercial uses. Street level facades themselves should be highly transparent, affording easy visual access from the sidewalk to interior activities or displays. Driveway openings in facades should be of minimum width. Other openings or setbacks, such as plazas or arcades, should involve less than 50% of a respective building's sidewalk frontage and should have a depth inward from the sidewalk which is less than 75% of the opening's street front width. All such arcade or plaza space should be lined by retail sales, restaurant, cultural display or entertainment spaces with transparent facades.

Sidewalk &  
Public Space  
Design;

When new developments include public plaza or arcade space in their designs, these spaces should be located adjacent to public sidewalks and should be built at street level. Open plaza or arcade spaces should involve less than 50% of a respective development's frontage.

. Circulation

The development concept of Alternative I involves a relatively moderate level of additional growth in a very limited area around the station. The traffic impacts of this growth should likewise be concentrated around the station, and the overall volume of traffic in the area should not increase to an extent that necessitates expansion of street capacities. Some of the additional work-and business-related trips in the station area are expected to be confined to metro rail, bus and pedestrian modes, while automobile trips to and from the new development surrounding the station and to and from the station itself are expected to increase traffic in the vicinity of the station. Direct pedestrian linkages from new developments to station facilities, as well as bridge or underground linkages between new buildings, may be considered in joint development or intensity bonus agreements.

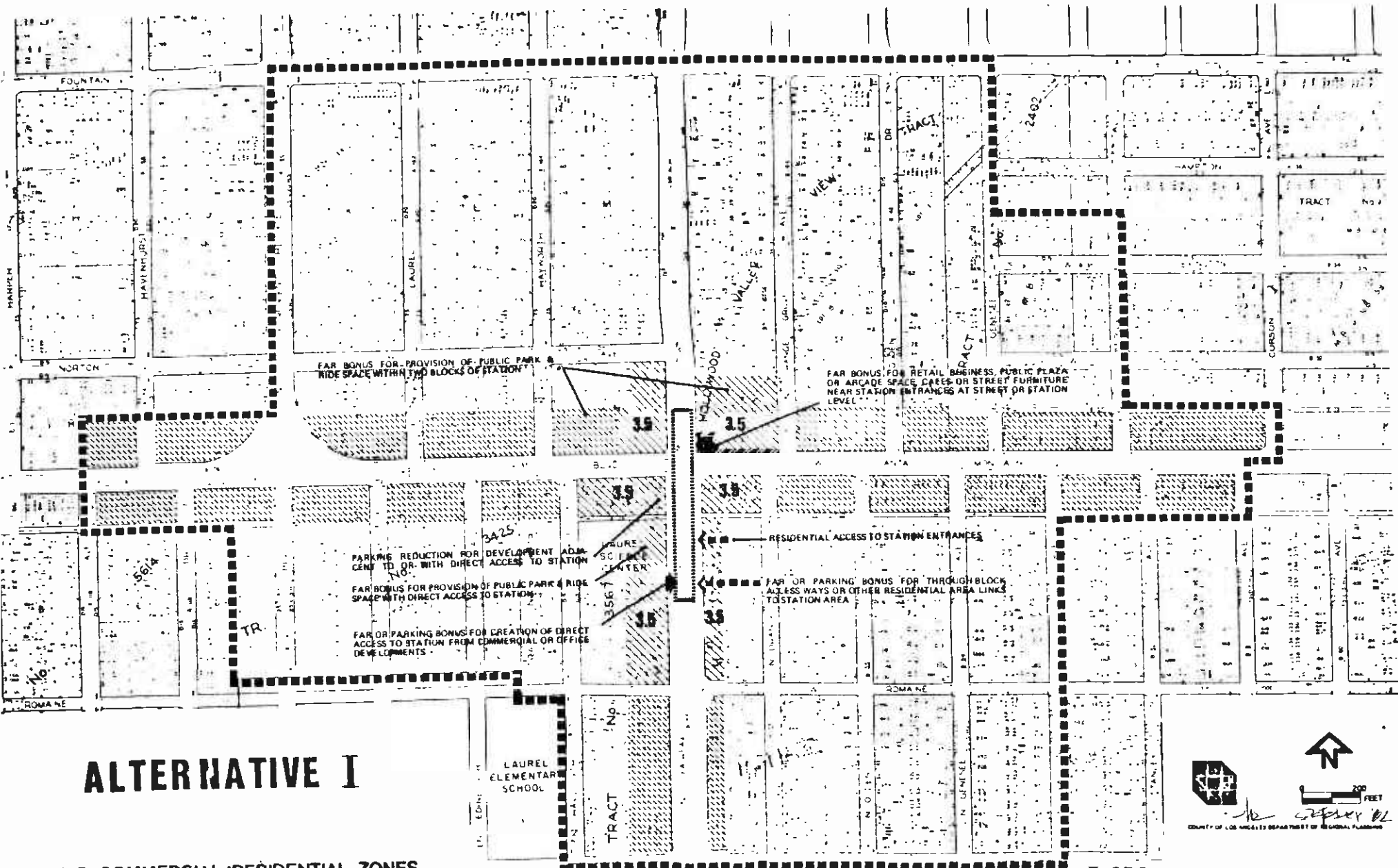
. Parking

Parking requirements for new commercial development adjacent to the station may be reduced by as much as 30% while residential parking requirements in mixed use developments may be reduced as much as 50% on the basis of their proximity to the station.



. Public Facilities Improvements

Development anticipated by Alternative I does not appear to require the support of any significant amount of publicly funded facilities improvement, other than the metro rail facilities, though a range of privately developed improvements connected with new development could enhance the overall station area environment for business and transit system activities. Development of public-serving facilities by private builders should be encouraged through intensity bonus mechanisms and joint development agreements.



# ALTERNATIVE I

- MIXED COMMERCIAL/RESIDENTIAL ZONES
- 2-3 F.A.R. AS PERMITTED BY COMMUNITY PLAN
  - 3.5 F.A.R.

# FAIRFAX-SANTA MONICA STATION AREA PLAN



## ALTERNATIVE II

### Concepts and Objectives

(Promote Major Mixed Use High Intensity Development Focused on the Fairfax-Santa Monica Station Facility.)

This concept involves a substantial increase in the development potential of all properties in the first one to two blocks adjacent to the metro rail station facility. Floor area ratio potentials are raised to more than double the levels permitted by the West Hollywood Community Plan while parking requirements for all types of uses may be reduced by virtue of proximity and/or direct connections between the new developments and the metro rail station. Specific development requirements and guidelines are designed to assure that the station and adjacent new development are highly integrated and mutually supportive. Beyond the immediately adjacent core, development potential of properties within a 750 foot walking distance of the station is enhanced in terms of intensity and reduced parking requirements.

The general objective of this scheme is to focus development, commerce and pedestrian activity on the metro rail station location in a manner which promotes a high level of urban vitality while deriving great advantage from the mass transportation services of the metro rail system. A more specific objective is to promote and facilitate the complete transformation of the adjacent station-surrounding blocks into an urban complex which is integrated with the mass transit function as the focal point of activity in this part of West Hollywood and a significant destination in the metro rail system. The redeveloped core would be primarily visitor- and new business-oriented. This new intensity and type of development would be expected to add a high level of economic vitality to the Station Plan area though its high intensity development and activity would be primarily confined to the core area. While neighborhood-serving business and facilities may be developed in the core area, neighborhood-related business is expected to become focused outside the core area in the lower intensity Mixed Commercial zones along Santa Monica and Fairfax frontages.

Another objective is to capitalize on the efficiency and accessibility of the metro rail system to promote a more vital pedestrian-oriented neighborhood surrounding the core area. This should improve the efficiency of movement in the area (with reduced localized dependency on cars) as well as expand the pedestrian clientele and exposure for businesses within the Station Plan area.

### Station Plan Policy Areas

#### • Land Use

The land use pattern proposed by Alternative II generally conforms to the West Hollywood Community Plan land use designations within the Station Plan area. Where Alternative II

varies from the Community Plan is in the blocks immediately adjacent to the station location; where the Alternative proposes to convert the residential portions of the blocks directly northwest, southwest and southeast of the Santa Monica and Fairfax intersection to Mixed Commercial designations. This is intended to permit greater flexibility in land assembly and major project design on properties with high interface and joint development potential vis-a-vis the station facility. Beyond this major mixed use core, the land use pattern consists of Mixed Commercial zones lining Santa Monica Boulevard and part of Fairfax with the remainder of the Station Plan area in high density residential use.

• Intensity of Development

Alternative II presents substantial intensity potentials for development near the station (See Alternative II Concept Map). The greatest of these bonus potentials apply to the blocks immediately surrounding the station; where mixed use floor area ratios may reach 6.0, which is twice that possible under the current West Hollywood Community Plan. This intensity of development would be permitted in an overall scheme for the high intensity core which requires all development to be interrelated with station facilities, major pedestrian circulation paths, and with the retail commercial environment of Santa Monica Boulevard. Other specific requirements for intensity bonuses may include specified mixtures of uses and facilities within developments, conformity to architectural or schematic designs for the entire core area, provision of specific public facilities or space, or station joint development or value capture assessment participation. Bonuses may also be utilized to encourage merger of small lots into more advantageous scale or configuration for new development.

Development potential beyond the high intensity core is increased to a lesser degree for properties located within 750' of the station. Beyond 750' the commercial, mixed use and residential development limits of the Community Plan remain unchanged. Within 750' of the station Mixed Use and Commercial FAR's may reach 5.0, while Community Plan residential density limits may be exceeded by up to 20% and parking requirements reduced as much as 50%.

• Mixed Use Development

The high intensity core proposed in Alternative II is intended to offer a broad range of opportunities and incentives for development of a complex urban concentration keyed to the location and function of the metro rail station. A mixture of uses including a substantial portion of residential and a wide variety of commercial, office and support facilities is encouraged through use of intensity bonuses, reduced parking requirements and special considerations in joint development. Mixed use development guidelines will encourage, if not require, retail and entertainment uses oriented to public spaces

at street level. Mixed use development in the high intensity core as well as along Fairfax and Santa Monica street front-ages should provide a full range of services for local residents and metro rail users in order to reduce auto usage and related parking facilities needs in the Station Plan area. Such a complex mixture of new development is expected to draw users from other parts of the metro rail system as well as from within the Station Plan area. Beyond the high intensity core, mixed use development should be geared to serving the local population.

• Use Guidelines

Uses to be permitted in the mixed use and commercial area designated in Alternative II include:

- Office; generally located above street level, leaving ground floor space for pedestrian oriented commercial uses.
- Retail sales; located at street level with sidewalk exposure, at station level, or near station entrances at street or station level.
- Personal Services; located at street or station level if the nature of the service warrants high exposure.
- Entertainment; located at various levels but preferably as part of public street or station-oriented settings.
- Recreational; preferably public recreational opportunities easily accessible from public circulation paths such as sidewalks, plazas, station facilities and street bridges.
- Cultural; located according to the nature of the cultural facility and its potential for increasing street environment vitality or attracting inter-station patrons.
- Residential; located off Santa Monica or Fairfax front-ages and street levels. Residential components of development should be appropriately insulated from business and high activity portions of developments but should have easy and direct access to commercial and recreational facilities as well as to station entrances. Wherever existing residential uses are removed and are replaced by redevelopment in mixed use, an equal or greater number of new residential units should be incorporated in the mixed use development on that site.

Parking; located to best serve, respectively, on-site businesses and office space, residential uses, and park-and-ride access to the station. Parking facilities should not interfere with the continuity or function of street level commercial or recreational facilities, and should be segregated according to the type of use they serve.

• Design Guidelines

Height limits; Within the mixed use core there should be no height limits. In the remainder of the 750' radius intensity bonus zone commercial and mixed use developments should be limited to 120' in height, and residential developments limited to a height of 80'.

Building Setbacks; All mixed use or commercial developments fronting on Fairfax or Santa Monica should be built to the front setback line across their entire frontages with the exception of driveway openings, public plaza, arcade, or sidewalk cafe space. Where new multi-story developments are constructed adjacent to existing or planned residential buildings, the new structures should be set back progressively from the property line at higher elevations or limited in height.

Facade Design; Creation of a continuous facade of interest to pedestrians is the general objective of new development design guidelines. Therefore, retail business space should have a primarily transparent street facade offering visual access by pedestrians to interior space or displays. Openings which disrupt sidewalk facades, such as driveways, should be avoided or minimized in width. Openings for plazas or arcades should not involve more than 50% of a respective building's sidewalk facades and their depths in from the sidewalk should not exceed 75% of their respective widths.

Sidewalk & Public Space; New public open space should be accessible and visible from public sidewalks or station facilities. Such open spaces should not, however, create blank spots or disruptive openings in the continuum of retail facades lining sidewalks. Indoor public spaces associated with the station may be appropriate.

## . Circulation

Development to the potentials proposed in Alternative II can be expected to increase auto traffic in the Station Plan area, and particularly in the high intensity core. Though a significant percentage of circulation related to new development and the metro rail station should be by pedestrian, bus and perhaps other auxiliary modes, automobile trips related to the central core development will increase due to the sheer volume of new station area development. To accommodate the anticipated increase, this alternative plan would include the widening of Santa Monica Boulevard and Fairfax Avenue to their respective full planned improvement widths within the Station Plan area. Additional traffic facilities improvements necessary for reducing congestion in the Station Plan area may involve secondary streets and parking facilities in general circulation planning for the high intensity core and remaining Plan areas.

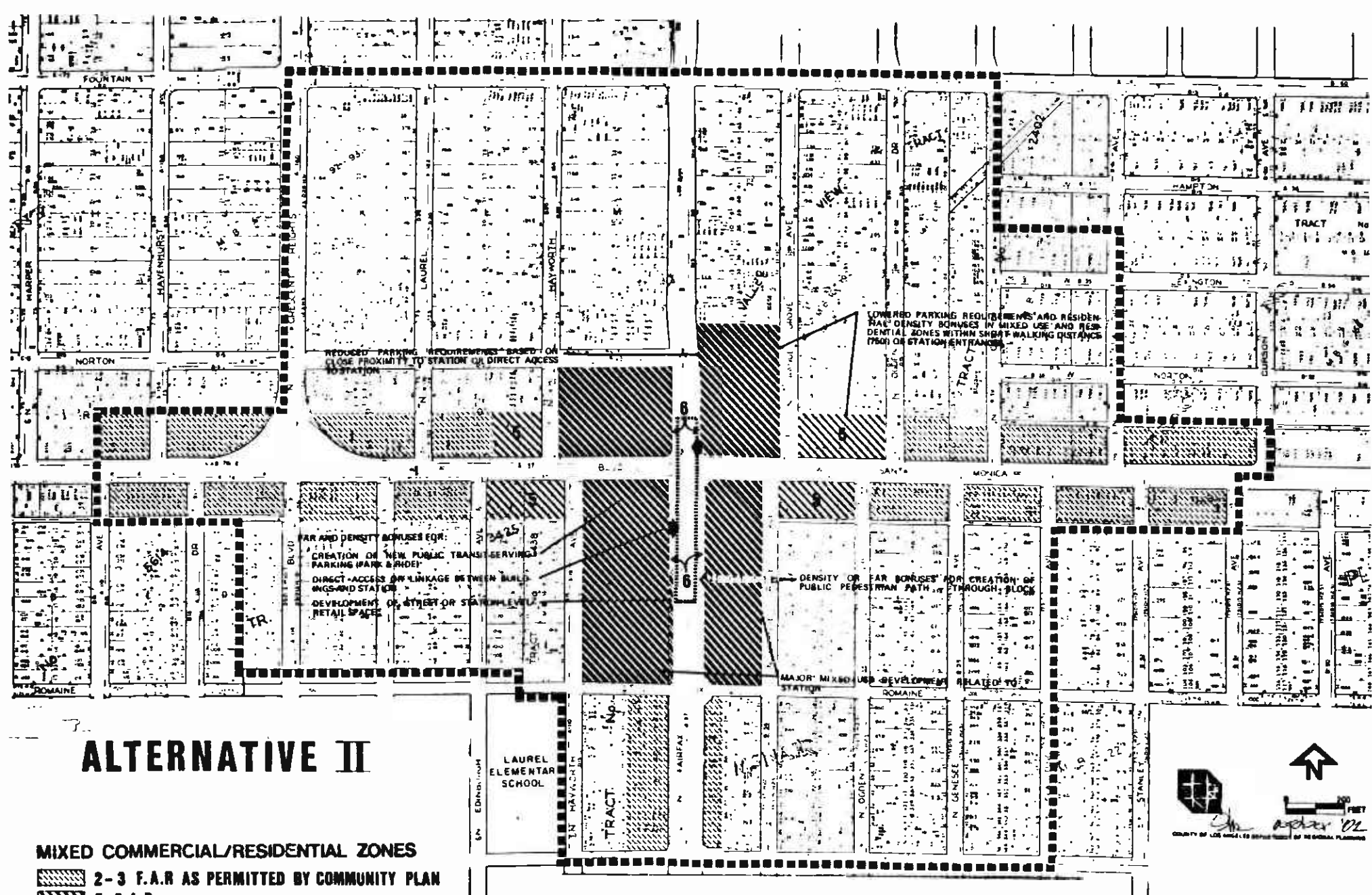
## . Parking

The proximity of major new development potential and the surrounding high density residential neighborhood to the proposed Fairfax-Santa Monica metro rail station supports a change of emphasis from auto to pedestrian access in Station Area Plan Alternative II. Increased efficiency of pedestrian access and movement between major development and the station, and throughout the intensified portions of the Station Plan area, is therefore a basis for reduction of existing parking requirements for a range of uses. Commercial and residential parking requirements may be reduced by as much as 30% and 50%, respectively, in the central core and near station areas.

While curb parking may remain a component of automobile access in Alternative II, it will represent a much smaller percentage of the Station Plan area's parking potential. Emphasis should be placed on creation of efficient off street parking and related circulation in the intensified areas. The general pattern and characteristics of parking in the central core and 750 foot zone around the station is expected to change markedly from present conditions, with characteristically more efficient on-site parking serving new development in the core area and reducing the deficiencies and circulation problems caused by the present situation.

## . Public Facilities Improvements

Beyond the proposed widening of specific streets and upgrading of additional circulation elements, Alternative II relies primarily on private and station joint development action to improve station and street level facilities. Such improvement action is encouraged by offering incentives to developers in exchange for their provision of specified facilities as part of their projects. More extensive facilities improvements may be funded and maintained through special assessments or value capture schemes related to the overall station area development.



# ALTERNATIVE II

- MIXED COMMERCIAL/RESIDENTIAL ZONES**
- 2-3 F.A.R. AS PERMITTED BY COMMUNITY PLAN
  - 5 F.A.R.
  - 6 F.A.R.

# FAIRFAX-SANTA MONICA STATION AREA PLAN





## ALTERNATIVE III

### Concepts and Objectives

(Promote Extended Santa Monica Boulevard Mixed Use Development Emphasizing Street-Level Business and Pedestrian Activity.)

Alternative III utilizes the station and immediately adjacent high intensity new development as the cornerstone of an extended Santa Monica Boulevard commercial revitalization and redevelopment. The overall development and use concept is intended to draw the large volume of pedestrian activity focused on the station and adjacent development into the broader community setting along Santa Monica. With development bonuses and reduction of parking requirements as primary incentives, this concept would employ a range of development guidelines to promote creation of continuous pedestrian-oriented commercial development focused on a station area complex and extending somewhat beyond 1/4 mile east and west of the station. While the immediate station area would be developed as a high density complex, the remainder of Santa Monica Boulevard properties would be given a more moderate intensity bonus potential. Overall, the sidewalks and related pedestrian and retail commercial activities are the activity and planning focus of this alternative. Emphasis is directed toward all dimensions of pedestrian activity, including those of transit access, business access and the characteristics of local residential lifestyles.

Specific objectives of this alternative include an eventual redevelopment of the entire Santa Monica Boulevard commercial zone within the Station Plan area. This process of redevelopment would begin with a short term revitalization program directed toward enhancement of the pedestrian environment along Santa Monica and the street level businesses that are key elements of the street setting. Guidelines for new development of the commercial properties along Santa Monica would promote the creation and maintenance of street level businesses, facades and sidewalk amenities supporting an attractive and vital pedestrian activity setting extending a quarter mile east and west of the immediate station area. County-initiated programs for cul-de-sacing of selected cross streets, creation of additional public parking space, planting of street trees, introduction of street furniture, and other means of improving the pedestrian-oriented business environment are intended to complement short and long term enhancement of the Santa Monica Boulevard commercial area.

The new moderate intensity development along Santa Monica is expected to include a component of local community serving commercial uses. The proposed changes in the character of the Santa Monica Boulevard street setting should attract greater local resident use as well as extend the station-centered pedestrian activities along the Santa Monica street commercial setting.

## Specific Plan Policy Areas

### Land Use

The pattern of land use promoted by Alternative III's policies matches that delineated by the West Hollywood Community Plan, with the exception of two areas of Mixed Commercial extended into residential portions of the first blocks east and west of Fairfax south of Santa Monica Boulevard. This adjustment of the Mixed Commercial designation is intended to give greater flexibility in mixed use development on properties adjacent to the station.

### Intensity of Development

In the concept of Alternative III, moderate development intensity increases are focused on all mixed use properties in the Station Plan area. Intensity bonus potentials can raise commercial FARs from the 2.0 limit of the Community Plan to 3.0 on all of the properties lining Santa Monica Boulevard, and to 4.0 on the properties-immediately adjacent to the station as indicated on the Alternative III Concept Map. Correspondingly, bonuses can raise the mixed use development FARs from the 3.0 Community Plan limits to 3.5 on Santa Monica Boulevard properties and to 5.0 on properties adjacent to the station. These bonus provisions raise the overall development potential of the Mixed Commercial zones in the Plan area to nearly twice the levels permitted by the West Hollywood Community Plan. Residential density potentials may be enhanced on properties within 1/4 mile walking distance of the station by reductions in parking requirements. Reduction of parking requirements for both commercial and residential developments would be varied on the basis of proximity to the metro rail station.

### Mixed Use Development

Alternative III offers development incentives to the majority of Mixed Commercial properties in the Station Plan area. The mixed use development promoted by this alternative includes a significant component of street level, pedestrian-oriented commercial space. This component of mixed use development is considered to be most important to the creation and maintenance of a vital pedestrian environment along Santa Monica Boulevard and around the station. Design and development guidelines for mixed use in this area are intended to encourage creation of a relatively continuous facade of interest to pedestrians along all Santa Monica and station-facing street fronts. Components of such street level development can include a full range of retail storefronts, plazas, arcades, and other elements attractive to pedestrians. Mixed use developments above the first floor street level would be regulated in less detail, with general guidelines governing separation of uses, setbacks from rear property lines, height limits, etc.

Use Guidelines

Uses to be permitted in the mixed use and commercial areas designated in Alternative III include:

- Office; generally located above street level, leaving ground floor space for pedestrian oriented commercial uses.
- Retail sales; located at street level with sidewalk exposure, at station level, or near station entrances. Retail business space should be located adjacent to sidewalks with transparent facades. A mixture of local and visitor-serving uses should occupy the extended street frontage along Santa Monica and adjacent to the station.
- Personal services; located at street or station level if the nature of the service warrants high exposure. These uses can become a key element in local community-serving business activity along Santa Monica Boulevard.
- Entertainment; located at various levels but preferably as part of public street or station-oriented settings.
- Recreational; preferably public recreational opportunities easily accessible from public circulation paths including sidewalks, plazas, station facilities and street bridges.
- Cultural; located according to the nature of the cultural facility and its potential for increasing street environment vitality.
- Residential; located off Santa Monica or Fairfax frontages and street levels. Residential components of development should be appropriately insulated from business and high activity portions of developments, but should have easy access to commercial and recreational facilities as well as to station entrances. Wherever existing residential uses are removed and are replaced by redevelopment in mixed use, an equal or greater number of new residential units should be incorporated in the mixed use development on that site.
- Parking; located to best serve, respectively, on-site businesses and office space, residential uses and park-and-ride access to the station. Parking facilities should not

interfere with the continuity or function of street level commercial or recreational facilities, and should be segregated according to the general type of use they serve. New public parking lots developed in vacated cross-street space may be used to offset or substitute for some of the required parking for businesses within 100' of the new parking lots.

• Design Guidelines

Building  
Setbacks;

All new mixed use or commercial developments fronting on Fairfax or Santa Monica should be built to the front setback line across their entire frontages with the exception of driveway openings, public plaza or arcade space, open sidewalk cafes or other such publicly accessible space. Where setback requirements locate new facades farther from the street than existing structures, the additional sidewalk width created should be treated in a manner which maintains the existing continuity of the sidewalk setting along that portion of the street. Temporary uses of the additional setback areas may include such facilities as tables and associated elements of sidewalk cafes, display stands for art or other exhibit items, awnings, etc. that can contribute interesting elements to the sidewalk setting and environment.

Side yard setbacks should be held to a minimum. Rear yard setbacks should employ stepped back concepts to minimize impacts of new development on existing residential development. That is, first floor rear yard setbacks should be minimal, while higher floors would be set back from the rear property line progressively further at higher levels of the structure.

Facade  
Design;

With an objective of creating and maintaining a street level continuous facade of interest to pedestrians, all street level business space should have transparent or open air facades which allow visual access from sidewalks to the interior activities or displays of the businesses. Where plazas or arcades are included in the street front facade design these spaces should not involve more than half of the respective building's street frontage, and should

be of limited depth from the front building setback line. Awnings or other such elements which provide continuity or otherwise enhance the public sidewalk setting should be encouraged.

Sidewalk &  
Public Space  
Design;

Public pedestrian spaces such as plazas and arcades should be located at street level with direct access from public sidewalks. Such pedestrian spaces should be located adjacent to public sidewalks or other major public pathways. Design of sidewalk furnishings should add elements of interest or convenience to the sidewalk setting without impeding pedestrian activity. Elements such as awnings and street trees are likely to add to the continuity of the overall sidewalk setting, while kiosks and other display elements can add interest to the localized portions of the setting. Consistent requirements and standards for such pedestrian level elements should be included in the Station Area Plan.

. Circulation

Alternative III proposes to change the Plan area's circulation pattern and functional characteristics through cul-de-sacing of about half of the cross streets to Santa Monica Boulevard and introducing parking space and sidewalk amenities in the vacated portions of cross streets. These alterations are intended to create greater continuity in the pedestrian-oriented sidewalk environment along both sides of Santa Monica for its full extent within the Plan area. This concept would effectively subordinate automobile traffic on cross streets in favor of pedestrian circulation and general auto traffic circulation along Santa Monica Boulevard. Other means of enhancing pedestrian circulation relative to existing streets may take the form of pedestrian bridges or underpasses which avoid pedestrian-automobile conflict or competition.

All other streets not affected by closures are intended to remain in their existing alignments and widths. Widening of Santa Monica Boulevard to planned improvement width would not be desirable in the short run if such action would constrict or disrupt the sidewalk setting considered a vital element in this alternative. Curb cuts for parking access to individual uses along Santa Monica should be minimized to avoid disruption of the sidewalk setting and pedestrian circulation.

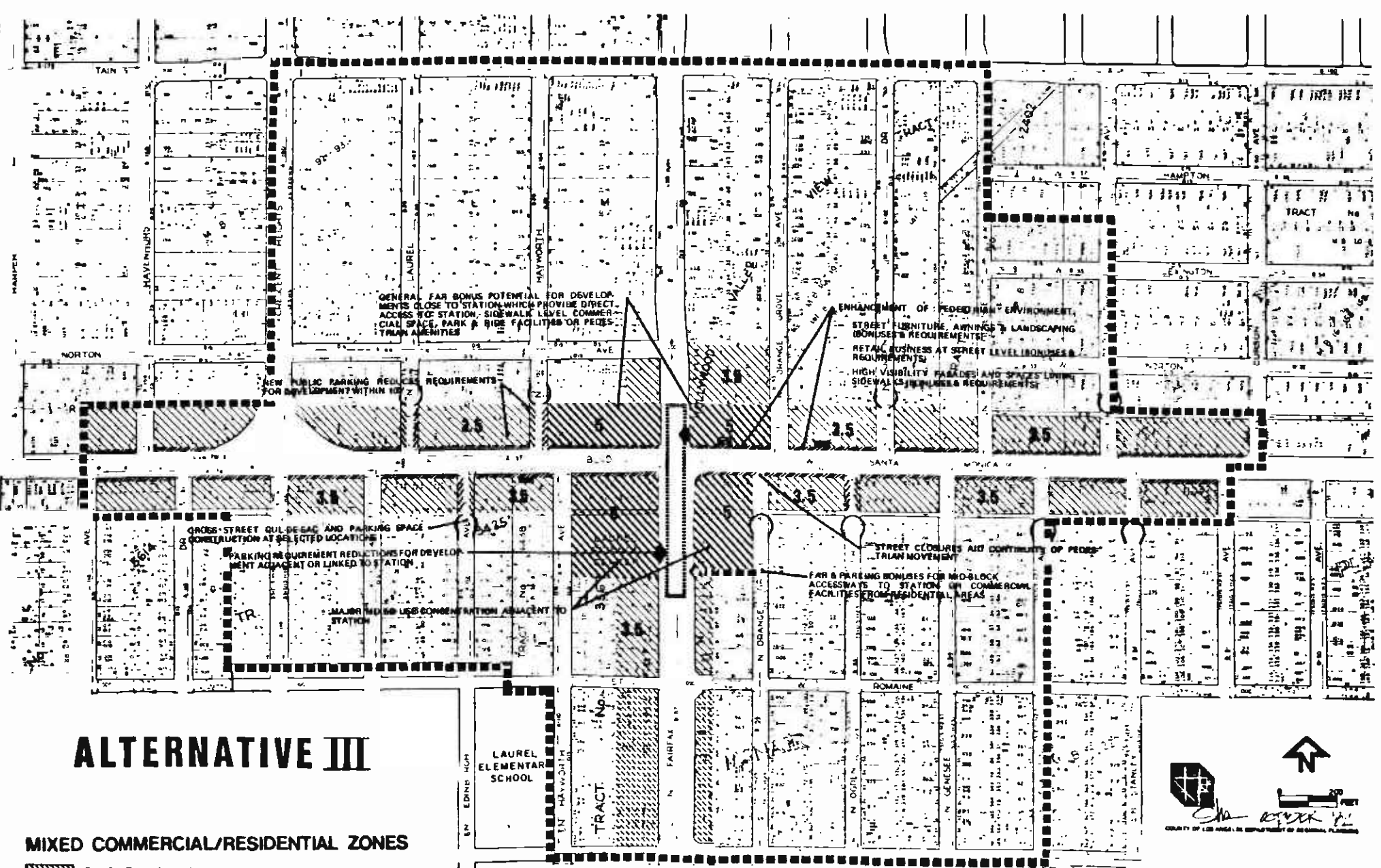
. Parking

The introduction of new small scale parking lots at a number of locations along Santa Monica Boulevard is intended to supplement existing curbside parking and to enhance pedestrian-oriented

street activity along the entire boulevard. These new parking facilities can also offset some off-street parking requirements for nearby businesses. Reductions in parking requirements should be employed as development incentives, and should be granted on the basis of proximity or accessibility to the metro rail station. Parking requirements for residential development in mixed use projects may be reduced to 50% of existing requirements, while commercial parking requirements may be reduced by as much as 30%. Generally, the maximum parking reductions should only be permitted within 1 or 2 blocks of the station.

• Public Facilities Improvements

The extended Santa Monica Boulevard sidewalk and commercial setting focus of Alternative III requires a significant amount of improvement and development of public facilities. Those facilities of key importance to the improvement of the Santa Monica street-level setting include the proposed cul-de-sacs and parking lots in vacated cross street rights-of-way, extensions of sidewalks across the vacated cross streets, introduction of street trees and furniture on existing sidewalks and creation of new business or concession space at parking lot entrances or in other areas of street vacations. These improvements should be funded through cooperative programs involving Los Angeles County and local businesses and property owners. Facilities such as street bridges, underpasses and station-related development are presumed to be implemented through joint development or other private initiatives.



# ALTERNATIVE III

## MIXED COMMERCIAL/RESIDENTIAL ZONES

- 2-3 F.A.R. AS PERMITTED BY COMMUNITY PLAN
- 3.5 F.A.R.
- 5 F.A.R.

# FAIRFAX-SANTA MONICA STATION AREA PLAN



CHAPTER III

PRELIMINARY FAIRFAX-SANTA MONICA  
STATION AREA PLAN

TASK 18BAH2947



## SUMMARY

The planned construction of a Metro Rail station at the intersection of Fairfax Avenue and Santa Monica Boulevard is likely to be the single most influential development affecting the eastern portion of West Hollywood for the next 20 years. The West Hollywood Community Plan, adopted in June 1981, called for a specific plan at the station area and this document responds to that mandate.

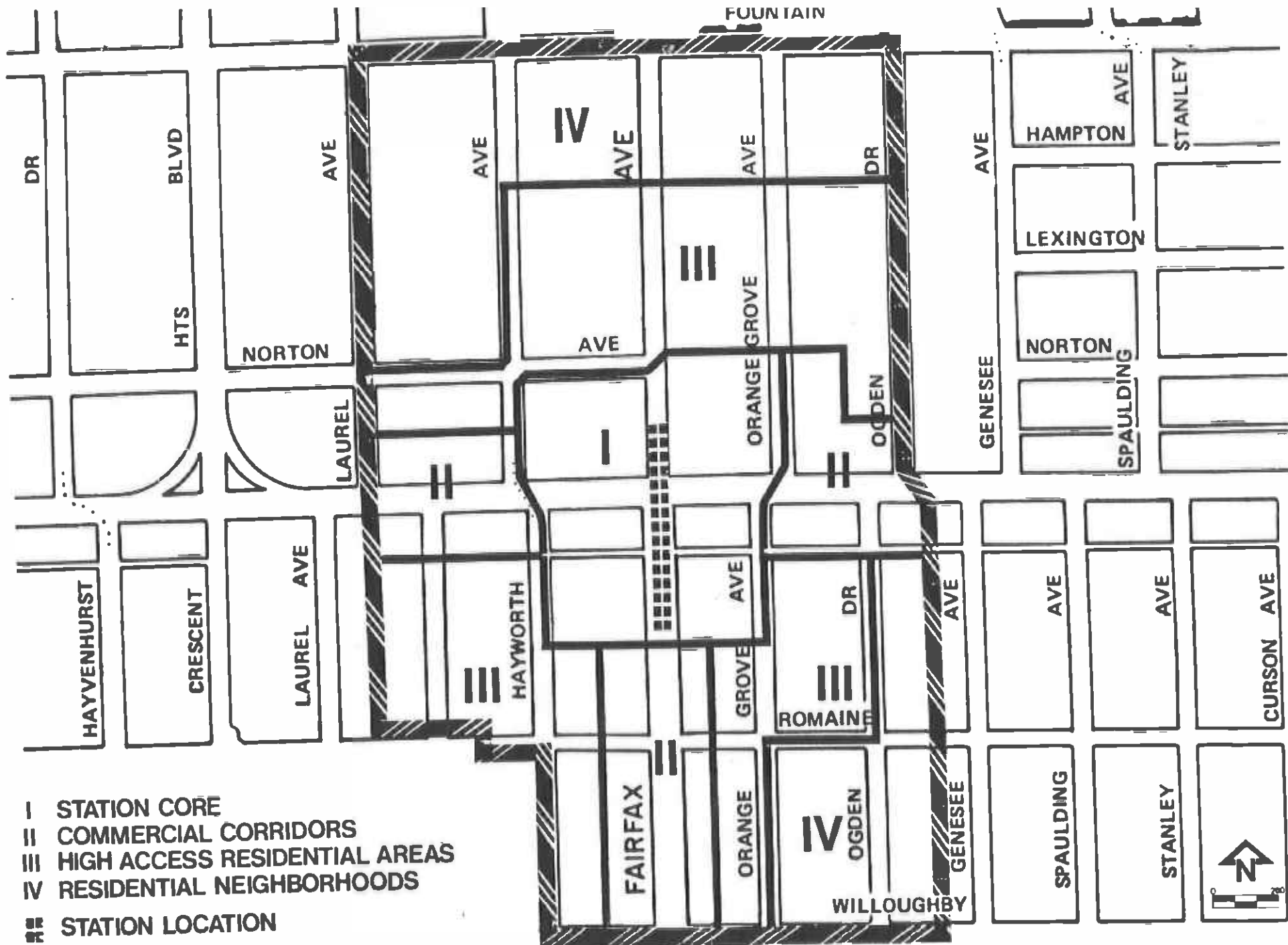
The Preliminary Fairfax-Santa Monica Station Area Plan presents a strategy that minimizes the negative impact of new station development on surrounding neighborhood areas by concentrating development in the immediate station area while allowing a building envelope adequate to accommodate the new development potential. The strategy is based on study and analysis of a wide range of conditions and factors relating to Metro Rail and the West Hollywood community, and was preceded by an evaluation of specific plan alternatives for this station area.

The Preliminary Station Area Plan sets forth objectives promoting increased pedestrian activity and development of a relatively intense, mixed use (commercial and residential) core of development around the Metro Rail station. Other objectives of equal importance are to maintain the character of nearby residential neighborhoods; maintain needed community-serving commercial uses; minimize impacts of station-related auto and bus traffic; and maximize transit/development interface.

The primary means of promoting appropriate station area development sensitive to the community setting is by linking the provision of community serving facilities with development incentives. These incentives offer developers greater flexibility in terms of density, height, building bulk to lot area ratios and parking requirements in return for recognition of unique community objectives.

In Mixed Use zones within easy walking distance of the station, development bonuses are granted for projects which include specific public amenities. Such amenities include neighborhood-oriented retail, pedestrian scale open spaces, and other pedestrian-related facilities. Some development types in the Mixed Use zones are granted reductions in parking requirements based on high accessibility to the station. To promote transit use, density increases are allowed in residential zones within easy walking distance of the station entrances; parking requirements for small residential units are also reduced.

Design guidelines for new development in the Station Plan area establish design standards in all areas within the Plan boundaries. The proposed policies and guidelines are tied to four sectors of the Station Plan Area defined by their different conditions and development objectives. The sectors and their general development controls are shown in the map and table on the following page:



- I STATION CORE
- II COMMERCIAL CORRIDORS
- III HIGH ACCESS RESIDENTIAL AREAS
- IV RESIDENTIAL NEIGHBORHOODS
- ▣ STATION LOCATION

# FAIRFAX-SANTA MONICA STATION AREA PLAN

PROPOSED MAXIMUM DEVELOPMENT CONTROLS  
FAIRFAX-SANTA MONICA STATION AREA

<u>Sector</u>	<u>Land Use</u>	<u>Development Intensity</u>	<u>Height Limits</u>	<u>Parking Requirements**</u>
I. STATION CORE	Mixed Commercial/ Residential	Max 6.0 FAR* with pedestrian amenities on > 30,000 sq. ft. sites.	Mixed Use:*** 175' maximum height  Single Use: 100' maximum height	Office: 1 space 600 sq. ft. floor area.  Retail Commercial: 1 space per 600 sq. ft. of floor space.
	Commercial Only	Max 4.0 FAR on < 30,000 sq. ft. sites.	Except: 60' maximum within 75' of residentially zoned (R-4) property	1 Br. & smaller units: 1 space/unit.
	Residential Only	Max 3.0 FAR  Max 4.0 FAR		All other uses per existing County Parking Ordinance.
II. COMMERCIAL CORRIDORS	Mixed Commercial/ Residential	Max 4.0 FAR with street-level retail sales space.	Mixed Use: 80' maximum height Single Use: 60' maximum height	Office: 1 sp./600 sq. ft. floor area
	Commercial Only	Max 3.0 FAR with street-level retail sales space.		Retail Commercial: 1 space per 600 sq. ft. of floor space.
	Residential Only	Max 4.0 FAR with street-level retail sales space.		1 Br. & smaller units - 1 space/unit.  All other uses per existing County Parking Ordinance.
III. HIGH ACCESS RESIDENTIAL AREA	High Density Residential	Rental: Max 100 du/ac. For Sale: Max 70 du/ac.	80' except 60' within 50' of residential areas with 45' height limits.	1 Br. & smaller units - 1 space/unit.  All other uses per existing County Parking Ordinance.
IV. RESIDENTIAL NEIGHBORHOODS	High Density Residential	Rental: Max 75 du/ac with bonus. For Sale: Max 50 du/ac.	45' maximum height	1 Br. & smaller units - 1 1/2 space/unit.  All other uses per existing County Parking Ordinance.

\* Floor Area Ratio (FAR is defined as a ratio of building floor area, excluding parking, to lot area.

\*\* Parking requirement flexibility is applicable only when parking demand-reducing programs are fully committed by developer (and employers).

\*\*\* Mixed use involves combination of residential and commercial uses.

## INTRODUCTION

The proposed Metro Rail Station at Fairfax Avenue and Santa Monica Boulevard presents an attractive and unique alternative to the present automobile-oriented transportation systems of West Hollywood and greater Los Angeles. Localized travel, as well as development patterns, are expected to become focused on the Fairfax-Santa Monica station area. The property surrounding the station will receive an entirely new dimension of economic opportunity from the introduction of a station facility that will not only attract over 16,000 Metro Rail riders per day to the Fairfax-Santa Monica intersection, but will expand the accessibility of this location from points all along the Metro Rail system.

To appropriately respond to these new conditions, planning for the station area must promote a development concept which capitalizes on the opportunities presented by the new station while assuring that the character of new development enhances the broader community surrounding the station. The primary vehicle proposed to establish and carry through the planning process is this station area plan, which deals directly with station area conditions and development. The Preliminary Fairfax-Santa Monica Station Area Plan represents policy directions derived from the evaluation of land use plan alternatives. Analysis and evaluation of Plan alternatives by Los Angeles County Regional Planning staff and the West Hollywood Citizens Advisory Committee have led to selection of a preferred Land Use Plan alternative and the articulation of the Preliminary Station Area Plan which follows.

The Plan is structured as a set of Metro Rail station-related policies and guidelines which complement the adopted policies and regulations of the West Hollywood Community Plan. Application of the Station Area Plan's provisions to the Station Plan area defined by the Community Plan is proposed to take effect once construction of the Fairfax-Santa Monica Metro Rail Station is assured. This Station Area Plan articulates a strategy for the most advantageous development of the area surrounding the Metro Rail station facility with respect to the needs and objectives of the West Hollywood community as well as those of the overall Metro Rail system. Policy and Standards prescribed by the Plan are focused on new development in the Plan area and emphasize the interface between new development, the Metro Rail station, and the activity systems of the Plan area. Plan policies and guidelines for development concern land use, intensity of development, height limits, building setbacks, facade design, sidewalk and public space design, and parking requirements. Basic policies are complemented by a range of specific design and development standards which focus on guiding new development toward a pedestrian oriented type of development which maintains the more positive characteristics of the residential neighborhoods and their local business community, while at the same time being fully supportive of Metro Rail. The Policies are applied to and vary between four separate sectors within the Station Plan area. Sector areas reflect distinct differences in conditions, opportunities and development potential within the Plan area.

- SECTOR I, STATION CORE, includes the blocks or portions of blocks immediately adjacent to the Fairfax-Santa Monica intersection where the most intense development activity is anticipated to occur.
- SECTOR II, COMMERCIAL CORRIDORS, includes commercial/residential uses along Fairfax Avenue and Santa Monica Boulevard outside the Station Core which are expected to benefit from close proximity to the station.
- SECTOR III, HIGH ACCESS RESIDENTIAL, includes all residential property within very easy walking distance of the station.
- SECTOR IV, RESIDENTIAL NEIGHBORHOODS, includes all other residential property within the Plan area.

The following pages discuss the major Problems, Issues and Needs that are particularly relevant to the Station Plan area under consideration. Statements of the Plan's Goals and Objectives set the general tone and direction. Plan Policies and Guidelines for Development, Land Use and Intensity, Design Guidelines and Parking Requirements comprise the main body of policy and regulation for the Plan area.

## PROBLEMS, ISSUES AND NEEDS

- Commercial Revitalization and Development - Current programs and efforts to revitalize the commercial establishments lining Santa Monica Boulevard are complemented by a general trend toward increasing value and renovation of existing structures. While revitalization is becoming prevalent in areas west of Crescent Heights Boulevard, no major renovations have been completed east of Crescent Heights, including the Station Plan area.

Existing commercial structures are characteristically aging, low intensity developments with substandard on-site parking. The adaptability of such structures to the economic opportunities and full potential presented by Metro Rail station development and operation seem very limited. It would therefore appear most appropriate to anticipate and plan for an extensive, if not complete, reconstruction of the commercial properties within the area most influenced by the Metro Rail station. Beyond this area of high influence, continued use and revitalization of existing structures may be profitable and appropriate for a number of years. Such areas of older commercial structures are likely to provide the most accessible and economically viable locations for local community-serving businesses. While the Metro Rail station will present significant advantages to local residents in terms of mobility, new development influenced by the station is likely to displace some existing community-serving businesses. The Station Area Plan should therefore make specific provisions for the accommodation of vital local-serving businesses in the design of new developments.

The extent to which existing buildings and businesses are displaced by new station-oriented developments must be weighed relative to the economic and functional vitality the new development and station-centered activity will bring to the area. Properly programmed new development is likely to broaden the commercial and cultural opportunities of local residents while establishing a positive foundation for continuing improvement of the area's commercial and cultural opportunities of local residents while establishing a positive foundation for continuing improvement of the area's commercial streets. The intent of this Plan is to minimize the negative impact of new station development on surrounding neighborhood areas while concentrating development in the immediate station area by allowing a building envelope adequate to accommodate the new development potential. This concept will at the same time enhance the station area by requiring inclusion of community serving uses and sensitive urban design.

- Property Values, Rent Levels and Displacement - Introduction of Metro Rail station facilities into the existing community and business setting around Fairfax and Santa Monica is expected to open a wide range of new economic and lifestyle opportu-

nities. The advantages presented to existing businesses and residents located within walking distance of the station will be immediate in terms of increased business exposure and accessibility, and increased mobility for local residents. However, competition for the locations and space benefitted by Metro Rail is likely to place economic pressure on existing commercial buildings and the businesses they house as the advantages of reconstruction and new, more intense use of property begin to reshape the station area. Residential uses within easy walking distance of the Metro Rail station will receive significant access and mobility advantage from Metro Rail's proximity, and the attractiveness of dwelling units with this advantage is likely to increase their demand and value. It is predictable that the cost of commercial and residential space in the vicinity of the station will rise well above current levels. Without a carefully developed Station Area Plan and program, the pressure of rising real estate values is also likely to displace the less competitive businesses in the area as well as some residents least able to pay increased rents.

The economic vitality that increasing land and development values are signals of has a wide range of positive attributes and potentials which are difficult to compare directly with displacement and economic pressures felt by local businesses and residents. The influence of Metro Rail station development is likely to be great enough to cause a complete redevelopment of the commercial areas near the station. With the older development housing existing businesses removed, the cost of reestablishing those same businesses in new station area development may be so high as to effectively eliminate them from the area. In the interest of maintaining local services and facilities of importance to the existing community, special consideration for maintaining or creating new and affordable space for community-serving businesses in the Plan area's commercial sections should become a component of Station Area Plan design and development guidelines. Well planned new development can offer possibilities for even more business establishments of value to local residents and users of the station facilities than exist under present conditions, while offering vital support to the operation of the Metro Rail system.

Traffic and Circulation - The impacts of large volumes of traffic on West Hollywood streets are considered to rank among the community's greatest problems. Present conditions on the major arteries in the Station Plan area, Fairfax Avenue and Santa Monica Boulevard, are not critical, but are characterized by heavy volume with peak hour conditions at the Fairfax-Santa Monica intersection resulting in delay and congestion. Growth projected for West Hollywood, even without a Metro Rail station, is expected to generate levels of additional traffic that will surpass the present capacity of the Fairfax and Santa Monica intersection. The projected impact will be the creation of unstable, stop-and-go traffic, with stoppages of long duration around the intersection. With a Metro Rail station at

Fairfax and Santa Monica, traffic projections are for slightly lower volumes than would occur with no station. Metro Rail is therefore expected to absorb the equivalent of all traffic generated by new development it would induce in the area, as well as some of the traffic generated by overall growth in West Hollywood.

Options for expanding the capacities or improving the efficiency of the Plan area's street system appear to be limited. While Santa Monica Boulevard could be widened to its full right-of-way width, it would necessitate relocation of the established building facade line on both sides of the Boulevard. Therefore this option appears feasible primarily as a long term project dependent on major redevelopment involving most of the properties lining Santa Monica. Current Route 2 studies and proposals do include a widening of Santa Monica Boulevard west of Fairfax to full right-of-way width, and should therefore be considered a potential future condition affecting new development and traffic characteristics in the Station Plan area.

Interim solutions to expanding Santa Monica Boulevard's capacity may include removal of curb parking on one or both sides, but the benefits of expanded capacity may be outweighed by the reduced parking access to local businesses that it would entail. Flow of through traffic on Santa Monica will be improved by reduction of cross street access to Santa Monica through cul-de-sacing of selected streets as called for in the Community Plan. Reduction of the number and frequency of cross-street intersections along Santa Monica will reduce the number of left turn and entry points along Santa Monica as well as aid the continuity of pedestrian movement along the Boulevard. Improvements in traffic control devices will improve circulation to some degree at major intersections such as Fairfax and Santa Monica. Such improvements are desirable in any case to accommodate the additional traffic and turning movements shifted from closed cross street locations. Expanded off-street parking with improved circulation and access in new development should reduce a significant amount of the present traffic disruption caused by parking space hunting, by stopping and backing movements in traffic lanes during parallel parking, and by poorly located or inadequate driveways opening onto Santa Monica. Cul-de-saced streets, while possibly losing a degree of direct accessibility to and from Santa Monica, will provide overall benefit from reduced pass-through traffic and generally safer and quieter conditions on residential streets.

The planned Metro Rail station at the Fairfax-Santa Monica intersection introduces an entirely new component of access demand and trip generation focused on the intersection. Depending on the eventual mode split characteristics of station arrivals and departures, the nature of increased traffic and movement in the Plan area will affect the station area in a variety of ways. With the development of substantial component of walking access to the station from surrounding residential and



commercial areas, the station area will become more pedestrianized, to the benefit of both local residents and merchants. It will also reduce some auto travel and parking demand in the vicinity of the station. Nevertheless, a considerable level of automobile access demand will undoubtedly continue in this heavily auto-oriented area.

Projected high levels of bus access to the station will produce highly localized pedestrian activity and is expected to constitute the major vehicular mode of access to the station. Well-designed bus stop and loading facilities are necessary for efficient station access and in avoiding traffic conflict and congestion around the Fairfax-Santa Monica intersection. The bus system is expected to function as a vital extension of the transit system into the remainder of West Hollywood and beyond, and therefore is given high priority in station area circulation planning.

Parking - Problems associated with parking conditions are perhaps the most frequent and persistent source of frustration and complaint among West Hollywood residents, businessmen and patrons. The problems stem most often from insufficient numbers and inconvenient location of parking spaces serving commercial, entertainment and recreational uses. Inability of on-site or nearby parking facilities to accommodate peak demands leads to inconvenience for patrons as well as nearby businesses and residents whose own parking needs are infringed upon by the spillover from businesses unable to supply adequate parking to their customers or employees. In many residential neighborhoods as well, on-site parking space for existing structures can not fully accommodate the total numbers of cars respective residents own. Therefore curb space along most West Hollywood residential streets is scarce during peak demand hours. Spillover from commercial uses may take up the last available space in nearby residential neighborhoods and, in some cases, may make it impossible for local residents to find parking space anywhere near their own homes or apartments.

The commercial strips lining Fairfax and Santa Monica within the Station Plan area generally have insufficient on-site parking to meet their patronage demands. While curb parking in front of or near business establishments is the most frequently utilized, it sometimes does not accommodate demand, and spillover affects parking for other businesses and adjacent residential neighborhoods. With curb parking metered in virtually all commercial areas, employees are often forced to park in residential neighborhoods.

The location and arrangement of commercial parking space in the Plan area often create conflict and congestion on major streets. Poorly located driveways and parking lot entrances tend to disrupt auto as well as pedestrian traffic. Stopping and backing movements in traffic lanes cause substantial disruption and danger on heavily traveled Santa Monica Boulevard.

Future commercial development should contribute to a reduction of parking problems by supplying more adequate levels of on-site parking than do present establishments and by orienting parking facilities for more efficient access and coordination with the area's circulation system. Alternatively, a Parking District, when established, could provide improved public parking in the area.

Accommodation of a large volume of park-and-ride demand for the station would appear to present little benefit to areas affected by parking lot construction. Parking lots or structures filled with cars for long periods of time would not contribute to the community's or station area's character or vitality. There is likely to be, however, a significant level of auto access demand generated by kiss-and-ride activities around the station. Accommodation of the limited, short term parking demand associated with this mode of access appears necessary if traffic congestion and parking demand around the station is to be reduced. Appropriate off-street short term parking facilities should be encouraged within easy walking distance of the station.

## GOALS AND OBJECTIVES

Goals and objectives for the Fairfax-Santa Monica Station Area Plan are as follows:

### Goals

- . Enhance and expand pedestrian activity and opportunity within walking distance of the station.
  
  
  
  
  
  
  
  
  
  
- . Create an economically and culturally vital urban center focused on the Metro Rail station.
  
  
  
  
  
  
  
  
  
  
- . Protect existing residential neighborhoods from the adverse impacts of non-residential uses.

### Objectives

- . Improve pedestrian access and circulation in the immediate station area and extended commercial zones.
  
- . Improve pedestrian environments and facilities along all commercial street frontages.
  
- . Reduce pedestrian/auto conflicts on sidewalks and street crossings.
  
- . Expand pedestrian open space near the station and in commercial areas.
  
  
  
  
  
  
  
  
  
  
- . Promote reconstruction of areas near the station to an intense mixture of uses which support Metro Rail use and street level pedestrian activity.
  
  
- . Enhance the commercial environment extending east and west along Santa Monica Boulevard.
  
  
- . Encourage development with a wide variety of uses attractive and accessible to local residents as well as Metro Rail users.
  
  
  
  
  
  
  
  
  
  
- . Concentrate major commercial development in the immediate vicinity of the station.
  
  
- . Discourage intrusion of non-residential traffic and parking into residential neighborhoods outside the Station Core area.

- . Optimize visual impacts of new commercial development on residential neighborhoods outside the Station Core area.
- . Maintain community services and commercial uses within walking distance of residential neighborhoods.
- . Provide for practical accommodation of community-serving business in new development.
- . Preserve opportunities for commercial establishments vital to local residents.
- . Provide adequate bus stopping and maneuvering space to avoid disruption of auto traffic.
- . Provide adequate parking and kiss-and-ride space near the station to prevent parking and general congestion problems.
- . Promote direct connections between new development and station entrances.
- . Promote joint development of station entrances and commercial facilities.
- . Promote development and uses which are most likely to benefit from transit access and to support the transit system.
- . Promote incorporation of transit-serving facilities in new private developments.
- . Ensure provision of needed community serving commercial uses within the Station Plan area
- . Minimize the impacts of Metro Rail related auto and bus traffic on the station area
- . Maximize the efficiency of transit/development interface

## POLICIES AND GUIDELINES FOR DEVELOPMENT

The development policies and guidelines presented in the Station Area Plan are based on the presumption that during a period covering the next 15 to 20 years, market demand and other factors spurred by the location of a Metro Rail station at Fairfax and Santa Monica will foster an extensive if not complete redevelopment of the commercial property along Fairfax and Santa Monica adjacent to the station. This reconstruction is certain to involve the blocks immediately surrounding the station and perhaps beyond. Station Plan development policies and guidelines are intended to promote the most appropriate overall development with respect to the needs of the West Hollywood community and the potentials of the Metro Rail station area. The nature of the expected transition, and the eventual new character of the station area and its surrounding neighborhoods are the focus of the Plan's development objectives, policies and guidelines.

Station Area Plan Policies and Guidelines for Development include recommendations for land use and intensity, urban design, parking, pedestrian circulation and amenities, traffic circulation, and public improvements. Specific guidelines vary between sectors of the Plan area as they apply to the differing conditions and planning objectives in those sectors. The sectors basically encompass the following portions of the Station Plan area:

- I. STATION CORE - This area includes the blocks or portions of blocks immediately adjacent to the Fairfax-Santa Monica intersection.
- II. COMMERCIAL CORRIDORS - This includes the Mixed Commercial/Residential zoned property in the Station Plan area outside the Station Core.
- III. HIGH ACCESS RESIDENTIAL AREA - Includes all residential property within very easy walking distance of the station.
- IV. RESIDENTIAL NEIGHBORHOODS - Includes all other residential property within the Station Plan area.

## LAND USE AND INTENSITY OF DEVELOPMENT

The existing land use pattern in the Station Plan area is an arrangement of commercial strips, one lot deep, lining all of Santa Monica Boulevard and most of Fairfax Avenue frontages, with the remaining area devoted to residential uses ranging from single family homes to high density apartments. This pattern has been reinforced by historic zoning.

The residential areas have experienced some development of relatively high densities. In contrast, the commercial strips, with the exception of a few areas of combined lots, are characterized by collections of old one and two story structures with narrow frontages interspersed with parking lots and other paved areas. Overall, the commercial properties in the Plan area are developed to less than 1/4 of their zoned capacity. Existing buildings are typically old and many are in deteriorating condition. The likelihood that many, if any, of these structures could fit into an extensive station-oriented construction program is remote. Nevertheless the existing lot and development patterns tend to make reconstruction difficult. To create appropriate parcel areas for new development in the commercial strips numerous small lots must be consolidated. To date there has been little economic incentive for acquiring and merging these small parcels for new development. Even when consolidated, the shallow depth of most commercial lots in the area inhibits development to the 2.0 floor area ratio permitted for commercial uses.

The Station Area Plan proposes incentives for development of facilities which promote creation of a high intensity focal point of activity around the station, gradually scaling down to the existing intensity and height of development at the edges of the Plan area.

Area-specified land use and development intensity guidelines are organized and presented below by Station Plan area sectors.

### . STATION CORE (Sector I)

The Station Core area should be the focal point of activity in the Plan area. Opportunity for development of an intense and vital hub of activity surrounding the station exists in the direct accessibility of the Core area properties to the station itself. Plan guidelines are therefore designed to provide flexibility and potential for development in the Core area through incentives, expanded capacities, and land use policies.

Only a few parcels presently zoned for residential uses are proposed for change to the mixed commercial/residential designation of the majority of the properties in the Core area. Those in the northwest portion of the Station Core area, fronting on Norton Avenue, currently in residential use, are the only residential properties potentially subject to eventual displacement. These land use changes are intended to promote and facilitate consolidation of large areas for new station-oriented mixed use development and to permit flexibility in project design.

## STATION CORE (continued)

Alleys within the Station Core should be vacated and the land incorporated into the overall development proposal.

### General Uses

The land use categories compatible with the concept of the Station Core are mixed commercial and high density residential. Despite this mixed use objectives, certain uses should be discouraged in the Plan area; these uses are those which do not contribute to street-level pedestrian environments or do not take full advantage of proximity to the station location. Such uses include:

- . automobile repair, service or sales;
- . wholesale and other establishments not open to the general public;
- . warehouses and other storage facilities or yards;
- . equipment and/or truck storage yards, garages or sales areas;
- . major utility or mechanical facilities;
- . uses with no need for public access or exposure;
- . permanent parking lots or structures with no other on-site use; and
- . single use office space at ground level.

These and other such uses not conducive to attractive pedestrian environments should be discouraged in favor of uses which contribute elements of interest and opportunity to pedestrians in the Station Plan area.

Uses encouraged in the Station Core include:

- . grocery and specialty food markets;
- . restaurants, including sidewalk cafes;
- . retail sales, particularly local serving;
- . personal services, primarily local service establishments;
- . motion picture and legitimate theaters;
- . galleries and other display uses;
- . mixtures of commercial use types; and,
- . mixed commercial and residential uses.

### Development Intensity

The objective for the Station Core is to promote mixed-use development with a combination of office, residential and support activities within each development project, with the more attractive retail and entertainment activities oriented to street level public sidewalks, plazas and station entrances, which will enhance pedestrian access and opportunity in the Station Core area.

An overriding concern and objective in the Station Core's development bonus system is the continued availability of local community-serving businesses and services located in the Core area. Therefore, development bonus provisions require development

STATION CORE (continued)

commitments to maintenance or creation of community-serving types of uses and facilities as part of new development in the Station Core.

Development Bonuses

Floor area ratios may be increased on project sites of 30,000 square feet or greater to a maximum of 6.0, and to a maximum of 4.0 on project sites of less than 30,000 square feet under the following conditions:

Eligibility

Bonus eligibility for developments located within the Station Core area is contingent upon the inclusion of at least one of the following First Priority Uses in each project:

<u>First Priority Use</u>	<u>Floor Area Bonus</u>
---------------------------	-------------------------

For each one (1) square foot of:

- |  |  |
|--|--|
| . Grocery Store/Supermarket<br>(at least 20,000 sq. ft. in size).  | . 2 additional square feet of any permitted use. |
| . Community-Serving Business<br>(equivalent to at least 0.3 FAR),<br>such as fish markets, delicatessens,<br>hardware stores, shoe repair shops,<br>cleaners, etc. | . 2 additional square feet of any permitted use. |
| . Short Term Metro Rail-serving<br>Parking Facilities (within 10' of<br>street grade and a minimum of 20<br>car capacity).   | . 2 additional square feet of any permitted use. |
| . Additional Entrance to Metro Rail<br>Station (equivalent to existing<br>entrance design).  | . 4 additional square feet of any permitted use. |

Projects including one or more First Priority Uses are also eligible for Second Priority Use bonuses as follows:

Second Priority Use

For each one (1) square foot of:

- |  |  |
|--|--|
| . Street level retail sales space<br>exposed to and accessible from<br>Fairfax or Santa Monica sidewalk<br>frontage. | . 2 additional square feet of any permitted use. |
|--|--|



STATION CORE (continued)

- . Street level restaurant space open to and accessible from Fairfax or Santa Monica sidewalk frontage (i.e. sidewalk cafes or restaurants with open sidewalk facades)
- . Street level public plaza space with appropriate amenities.
- . 2 additional square feet of any permitted use.
- . 1 additional square feet of any permitted use.

. COMMERCIAL CORRIDORS (Sector II)

The Mixed Commercial/Residential strip areas outside the designated Station Core area are expected to benefit from the influence of the high levels of development and pedestrian activity that the Station Core area and Metro Rail operation will introduce to the Station area. To promote an extended Santa Monica Boulevard commercial revitalization and reconstruction, some increase in development potentials coupled with required street level retail sales, theaters or other pedestrian environment-enhancing facilities developments are proposed for the commercial areas. Alleys adjacent to commercial properties may be vacated and incorporated into the development schemes of commercial properties greater than 15,000 square feet in order to expand development area and improve site design options. Vacations are subject to County approval of a workable plan for delivery and service access.

No residentially zoned property is changed to Mixed Use classification in the Commercial Corridors.

General Uses:

Uses intended for the Commercial Corridors (II) are the same as those included in the Station Core (I)

Development Intensity:

The Station Area Plan promotes an extended pedestrian oriented mixed use area along Fairfax Avenue and Santa Monica Boulevard frontages within the Commercial Corridors. The objective is to create and maintain a relatively continuous environment of interest and value to pedestrians, which serves both Metro Rail users and local community needs.

Floor area ratios in the Commercial Corridors may reach a maximum of 3.0 for commercial and 4.0 for mixed commercial residential uses under the following conditions:

- for each square foot of floor space expressly created for community-serving uses including grocery stores or markets, drug stores and other such sales and services, two square feet of any permitted use may be added to the permitted base commercial or mixed use FAR.
- for each square foot of street level retail sales, motion picture or legitimate theater, or restaurant space, one square foot of any permitted use may be added to the permitted base FAR.

. HIGH ACCESS RESIDENTIAL AREAS (Sector III)

Residential developments within roughly one long block or two short blocks of the transit station, as shown on Maps B and C, are considered to have very high accessibility to Metro Rail and are expected to generate a relatively high percentage of pedestrian access trips to the station and Core area. To capitalize on the very efficient access potentials of the High Access Residential Areas, residential density bonuses are allowed for both rental and owner occupied developments.

While these density bonuses will tend to promote a somewhat higher level of residential development in the High Access Areas than exists there today, very few existing single family and other low density uses will be subject to conversion to higher densities. The Residential Neighborhoods beyond the High Access Residential Areas contain the vast majority of single family and low density housing remaining in the Station Plan area.

The residential zone classifications of all High Access Residential Areas outside the Station Core area remain unchanged.

Proposed Station Area Plan residential densities permit residential development to the following levels:

- Up to 70 dwelling units per acre for condominiums and other units intended for sale.
- Up to 100 dwelling units per acre for projects composed completely of rental units.

. RESIDENTIAL NEIGHBORHOODS (Sector IV)

For the other residential areas within the Station Area Plan boundaries (areas designated IV), this Plan reiterates and reinforces the development standards set forth in the West Hollywood Community Plan. These areas are not recommended for major redevelopment and intensification.

RESIDENTIAL NEIGHBORHOODS (Sector IV) (continued)

Future extension of policy and standards of the High Access Residential Areas or other such measures should only be instituted in response to demonstrated need or opportunities in these areas and with full public discussion.

## DESIGN GUIDELINES

The design guidelines which follow are intended to promote development which contributes identity, vitality and functional efficiency to the Metro Rail station area setting. A primary focus of the guidelines is on the opportunities and potentials presented by dramatically increased pedestrian activities associated with the station's daily operation. The intent of street-level development guidelines is to expand the realm of heavy pedestrian activity beyond the station entrances into surrounding commercial developments to create a vital pedestrian street life setting extending along Fairfax and Santa Monica sidewalks. Design requirements for street-level sales and entertainment uses are intended to enhance the attractiveness of commercial developments to pedestrians and promote a mutually supportive combination of pedestrian activities and commercial facilities.

Building height limits and setback requirements are intended to permit development of distinctive as well as functional structures which contribute to the station area's vitality without infringing upon surrounding residential and pedestrian environments.

### STATION CORE (I)

#### Height Limits

- Mixed Use structures in the Station Core may be 175 feet in height above street grade on portions of property which are at least 75 feet away from adjacent properties zoned for residential use. All residential properties in parking overlay zones (generally found adjacent to commercially zoned strips along Santa Monica Boulevard) shall not be considered as residential for purposes of establishing these setbacks and height limits. Structure heights on portions of property within 75 feet of adjacent residentially zoned property shall not exceed 60 feet.
- Single use structures in the Station Core shall not exceed 100 feet above street grade, and are limited to 60 feet in height within 75 feet of adjacent residentially zoned property.

#### Building Setbacks

- All commercial and mixed use developments in the Station Core area shall provide an additional five feet of sidewalk width, beyond existing requirements, for all Santa Monica Boulevard and Fairfax Avenue frontages.

All new mixed use or commercial developments fronting on Fairfax or Santa Monica shall be built to the front setback line across their entire frontages with the exception of driveway openings, public plaza or arcade space, sidewalk cafes or other such publicly accessible space.

Uses of the additional setback areas may include such facilities as tables and associated elements of sidewalk cafes, display stands for art or other exhibit items, awnings, etc. that can contribute interesting elements to the sidewalk setting and environment without impeding normal pedestrian movement.

Ground floor developments should extend across their entire street/sidewalk frontages. Side yard setbacks should be held to a minimum in commercial developments, and shall not, in aggregate, amount to more than 15% of a respective building site's street frontage.

### Facade Design

- . With an objective of creating and maintaining a street level continuous facade of interest to pedestrians, all street level business space is required to have transparent or open air facades which allow visual access from sidewalks to the interior activities or displays of the businesses. A minimum of 50% of ground floor facades shall be transparent.
- . Where plazas or arcades are included in the street front facade design these spaces shall not involve more than half of the respective building's street frontage, and will be limited in depth to 100% of the arcade or plaza's respective streetfront width.

### Sidewalk and Public Space Design

- . Public pedestrian spaces such as plazas, arcades and other amenity spaces shall be located at street level with direct access from public sidewalks. Such pedestrian spaces must be located adjacent to public sidewalks or other major public pathways and their surface elevations must be within 3 feet of the adjacent public sidewalk elevation.
- . Street level open space shall occupy no more than 50% of a respective site's sidewalk frontage.
- . A minimum of 50% of building space at the edge of a ground level plaza must be occupied by retail sales, personal services or restaurant space.
- . Public plazas and other street level amenity spaces must include landscaping, seating and other pedestrian environment-enhancing elements.
- . Outdoor public plazas shall be open to the sky, and covered or canopied only by trees or temporary elements such as canvas sheets, umbrellas, etc.

- . Design of sidewalk furnishings should add elements of interest or convenience to the sidewalk setting without impeding pedestrian activity. Elements such as awnings and street trees are likely to add to the continuity of the overall sidewalk setting, while kiosks and other display elements can add interest to localized portions of the setting, so long as they do not impede circulation.

Delivery and Maintenance Access Facilities

- . All uses in new developments shall be supplied with off-street loading and delivery facilities. Collections of small uses may have pooled facilities where individual facilities are not necessary for efficient use.
- . Loading and delivery facilities for all new uses should be located off-street and, where feasible, below street level. In any case, curbside delivery is to be generally prohibited on Fairfax and Santa Monica frontages, and limited on all other street frontages.
- . Access to loading and delivery facilities should be on street-fronts other than Santa Monica Boulevard.

### Height Limits

- . Mixed use structures are limited to 80 feet in height and single use structures limited to 60 feet above street grade.
- . Parking structures constructed in Community Plan-designated Parking Overlay Zones shall be limited to 35 feet in height above adjacent street grade.

### Building Setbacks

- . All new mixed use or commercial developments fronting on Fairfax or Santa Monica shall be built to the front setback line across their entire frontages with the exception of driveway openings, public plaza or arcade space, sidewalk cafes or other such publicly accessible space.
- . Uses of the additional setback areas may include such facilities as tables and associated elements of sidewalk cafes, display stands for art or other exhibit items, awnings, etc. that can contribute interesting elements to the sidewalk setting and environment without impeding normal pedestrian movement.
- . Ground floor developments should extend across their entire street/sidewalk frontages. Side yard setbacks should be held to a minimum in commercial developments, and should not, in aggregate, amount to more than 15% of a respective building site's street frontage.

### Facade Design

- . With an objective of creating and maintaining a street level continuous facade of interest to pedestrians, all street level business space is required to have transparent or open air facades which allow visual access from sidewalks to the interior activities or displays of the businesses. A minimum of 50% of ground floor facades shall be transparent.
- . Where plazas or arcades are included in the street front facade design these spaces shall not involve more than half of the respective building's street frontage, and will be limited in depth to 50% of the arcade or plaza's respective streetfront width.

### Sidewalk and Public Space Design

- . Public pedestrian spaces such as plazas, arcades and other amenity spaces shall be located at street level with direct access from public sidewalks. Such pedestrian spaces must be located adjacent to public sidewalks or other major public pathways and their surface elevations must be within 3 feet of the adjacent public sidewalk elevation.

## COMMERCIAL CORRIDORS (II) (continued)

- . A minimum of 50% of building space at the edge of a street level plaza must be occupied by retail sales, personal services or restaurant space.
- . Public plazas and other street level amenity spaces must include landscaping, seating and other pedestrian environment-enhancing elements.
- . Outdoor public plazas shall be open to the sky, and covered or canopied only by trees or temporary elements such as canvas sheets, umbrellas, etc.
- . Design of sidewalk furnishings should add elements of interest or convenience to the sidewalk setting without impeding pedestrian activity. Elements such as awnings and street trees are likely to add to the continuity of the overall sidewalk setting, while kiosks and other display elements can add interest to the localized portions of the setting, so long as they do not impede circulation.

### Delivery and Maintenance Access Facilities

- . All uses in new developments shall be supplied with off-street loading and delivery facilities. Collections of small uses may have pooled facilities where individual facilities are not necessary for efficient use.
- . Loading and delivery facilities for all new uses should be located off-street and, where feasible, below street level. In any case, curbside delivery is to be generally prohibited on Fairfax and Santa Monica frontages, and limited on all other street frontages.
- . Access to loading and delivery facilities should be on street-fronts other than Santa Monica Boulevard.



. HIGH ACCESS RESIDENTIAL AREA (III)

Residential structures in the High Access Residential Areas may be built to 80 feet in height, with the exception that within 50 feet of residential property with 45 foot height limits, building height shall not exceed 60 feet.

. RESIDENTIAL NEIGHBORHOODS (IV)

The height of residential development is limited to 45 feet in all residential neighborhoods outside the High Access Residential Area.

## PARKING REQUIREMENTS

The effects of Metro Rail service availability at Fairfax and Santa Monica on automobile use in the Station Plan area are a key consideration in determining appropriate parking standards for future development. To the extent that Metro Rail can provide an attractive alternative means of access to local as well as distant destinations, parking standards could be reduced accordingly without increasing congestion or reducing accessibility in the Plan area.

Parking requirements for selected commercial and residential uses in the Station Plan area may be reduced in the Plan area sectors to the levels outlined below. Reductions of office and retail commercial parking requirements are permitted only in conjunction with project developer-and/or employer-sponsored programs to reduce parking demand through ride-sharing, shuttle service, car pooling, mass transit or other modes of access presenting a viable long term alternative to automobile access and related on-site parking facilities for employees and/or patrons. Upon Regional Planning Commission approval of a long term program reducing parking demand, a development and/or use may reduce its on-site required parking commensurate with the demonstrated reduction in parking demand as a result of such a program to the limits specified as follows:

### STATION CORE (I)

- . Office uses - 1 parking space per 600 square feet of floor area
- . Single, bachelor and one bedroom units - 1 parking space per unit
- . Retail Commercial - parking space per 600 square feet of floor area

### COMMERCIAL CORRIDORS (II)

- . Office uses - 1 parking space per 600 square feet of floor area
- . Retail Commercial - 1 parking space per 600 square feet of floor area
- . Personal Services and Retail Sales - May have required parking in off-site lots within 500 feet of respective businesses.  
(Current regulations require a Conditional Use Permit for off-site parking)
- . Single, bachelor and one-bedroom units - 1 parking space per unit

HIGH ACCESS RESIDENTIAL AREA (III)

- . Single, bachelor and one-bedroom units - 1 parking space per unit

RESIDENTIAL NEIGHBORHOODS (IV)

- . Single, bachelor and one-bedroom units - 1 1/2 parking spaces per unit (no change from present requirements)

All Employee parking demand not obviated by access and parking alternative programs must be accommodated by developer-and/or employer-supplied free parking within 500 feet of respective commercial facilities and businesses.

CHAPTER IV

FAIRFAX - SANTA MONICA  
STATION AREA PLAN IMPLEMENTATION

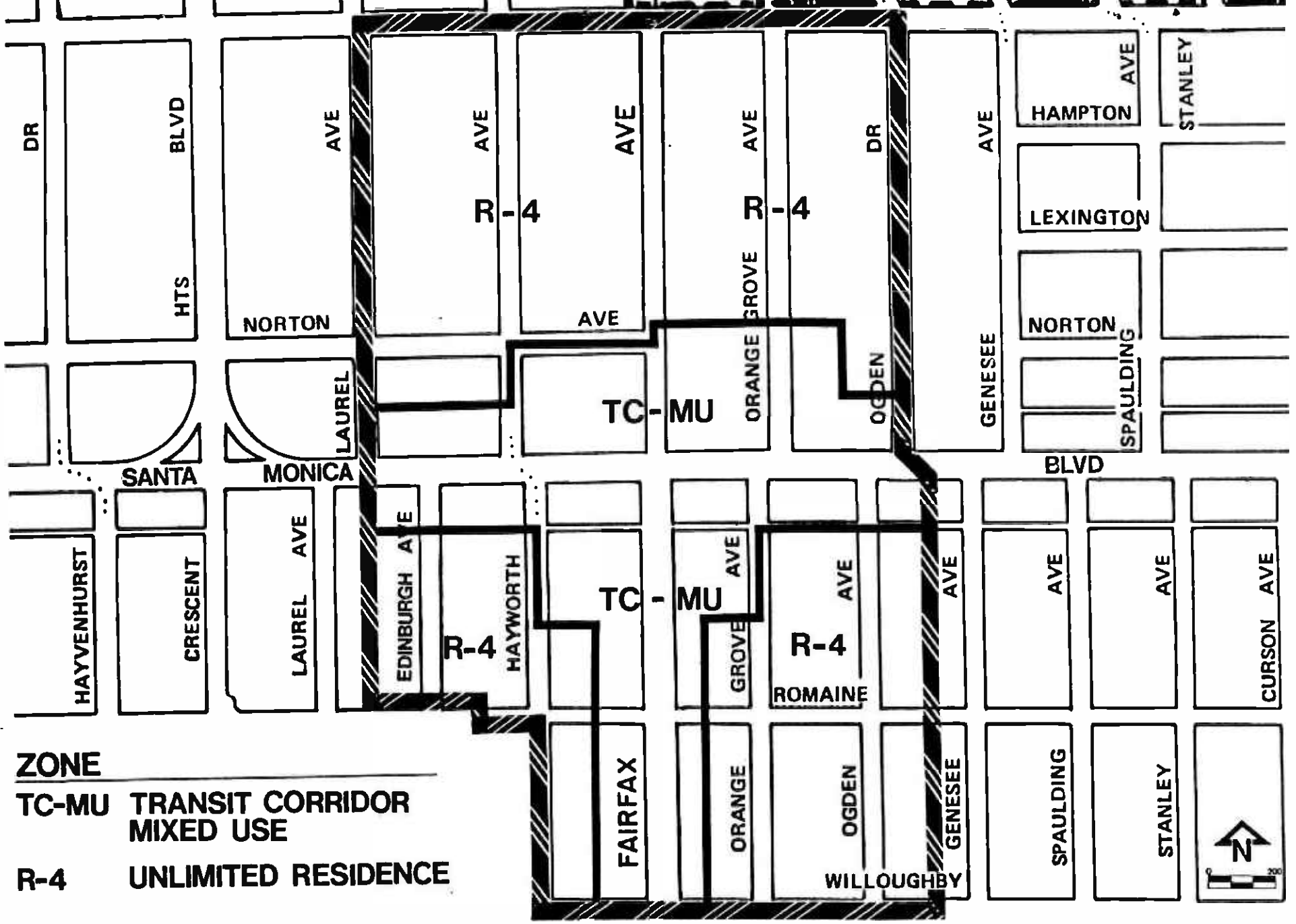
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## INTRODUCTION

This chapter constitutes the implementation phase of the Santa Monica - Fairfax Station Area Planning report and sets forth two ordinance amendments to the Zoning Section of the Los Angeles County Code. The first ordinance, the Transit Corridor Mixed Use Zone, (TC-MU), represents a new zoning classification which provides for the application of specific standards to new commercial/residential development in the Station area. The TC-MU Zone regulates uses, development intensity, standards for ground floor development, short term Metro Rail Serving parking facilities and height limits. The TC-MU and surrounding R4 zone boundaries are delineated on the Proposed Zoning map.

The second ordinance, the Fairfax - Santa Monica Transit Corridor Community Standards District establishes special development standards and policies designed to permit increased intensity and scale of development in the Station Core. In return for the increased intensity, the project must incorporate significant public facilities, pedestrian amenities and station access improvements. Residential densities can also be increased in those areas within easy walking distance at the station. These areas having significant transit access advantages, have been designated high access residential areas.

FOUNTAIN



**ZONE**

**TC-MU** TRANSIT CORRIDOR  
MIXED USE

**R-4** UNLIMITED RESIDENCE



# FAIRFAX-SANTA MONICA STATION AREA PROPOSED ZONING

## TC-MU TRANSIT CORRIDOR MIXED USE ZONE

I. ESTABLISHED - PURPOSE. Zone TC-MU is established to provide for the application of specific standards to new commercial and mixed commercial/ residential development in the vicinity of the planned transit stations. The purpose of these standards is to promote development of a type and character which improves pedestrian circulation, creates attractive public open spaces, facilitates access to the subway station and establishes a mixed use urban center around the station

### II. PERMITTED USES

Uses located above or below the ground floor of a development, and those uses which have no frontage on a Master Planned Highway or a public plaza may include any uses permitted in the C-3-CRS zone, subject to the requirements specified in the appropriate adopted Community Plan and its Community Standards District.

### III. USES SUBJECT TO DIRECTOR'S REVIEW AND APPROVAL

Uses located on the ground floor level or within 5 feet of curb level fronting on Master Planned Highways or a public plaza shall be limited to the following:

Antique shops, genuine antiques only  
Apartment hotels - only lobby space is permitted at ground level. Lobby space is limited to 20% of total development lot frontage  
Appliance stores, household  
Art galleries - open to general public  
Art supply stores  
Athletic goods stores  
Bakery shops, including baking only when incidental to retail sales from the premises  
Banks - limited to 20% of total development lot frontage  
Bicycle shops  
Book stores  
Camera and accessories stores  
Clock or watch stores or repair shops  
Clothing rental establishments  
Clothing stores  
Coin and precious metals stores  
Confectionary or candy stores, including making only when incidental to retail sales from the premises

Crafts shops  
Cultural exhibits  
Costume rental establishments  
Dress shops  
Drugstores  
Dry cleaners  
Eating or drinking places  
Florist shops or stands  
Food stores, including supermarkets, grocery stores,  
markets or delicatessens  
Furniture stores  
Furrier shops, custom  
Gift shops  
Hair products  
Hardware stores  
Health food stores  
Historical exhibits  
Hobby supply stores  
Home appliances, sales  
Home video, computer equipment and game stores  
Hotels - only lobby space is permitted at ground level.  
Lobby space is limited to 20% of total development lot  
frontage  
Ice cream shops, parlors or stands  
Interior decorating establishments  
Jewelry stores  
Leather goods stores  
Liquor stores  
Luggage stores  
Millinery shops  
Music stores  
Newstands  
Offices - only lobby space is permitted at ground-  
level. Lobby space is limited to 20% of total  
development lot frontage  
Outdoor or open front cafes or restaurants  
Pastry stores  
Pet stores  
Photographic equipment and supply stores  
Record stores  
Residences - only lobby space is permitted at ground  
level. Lobby space is limited to 20% of total develop-  
ment lot frontage  
Shoe repair shops  
Shoe stores  
Sound equipment, home stereo, radio, etc. stores  
Sporting goods stores  
Stationery stores  
Tailor shops, custom  
Theaters - motion picture or legitimate-lobby entrance  
at ground level limited to 40 feet of sidewalk or  
plaza frontage  
Tobacco shops  
Toy stores  
Travel bureau/agencies  
Variety stores



Other uses which are determined by the Planning Director as contributing to an attractive pedestrian environment or as enhancing the Transit station area's efficiency or commercial character may be included at ground level.

#### IV. DEVELOPMENT STANDARDS

##### A. Floor Area Ratio

Permitted development levels, defined in terms of Floor Area Ratio (FAR), are comprised of two components; Base FAR (permitted on all TC-MU zoned property in the Station Plan Area), and Bonus FAR (based on type and square footage of public-serving facilities included in a respective development proposal).

###### (1) Base FAR:

All Transit Corridor Mixed Use zones in the Station Plan Area are subject to Base FAR limits as follows:

<u>Maximum FAR</u>	<u>Type Of Development</u>
2.0	Commercial Only
3.0	Commercial/Residential
3.0	Residential Only

###### (2) FAR Bonus:

All TC-MU zoned properties in the Station Plan Area are subject to granting of development bonuses based on inclusion of specified improvements in new development projects.

##### B. Development Intensity

Floor area ratios in the TC-MU Zone may reach a maximum of 3.0 for commercial and 4.0 for mixed commercial/residential uses under the following conditions:

- (1) For each square foot of floor space created and used for community-serving businesses or community services, two square feet of any permitted use may be added to the permitted base commercial or mixed use FAR.
- (2) For each square foot of ground floor retail sales, motion picture or legitimate theater, or restaurant space with open sidewalk facade, one square foot of any permitted use may be added to the permitted base commercial or mixed use FAR.

### C. Sidewalks

- (1) All sidewalks on Master Planned Highways, one block from station intersection shall be 15 feet wide as measured from the street curb face. The sidewalk width requires improvement of an additional 5 feet beyond the standard 10 foot requirement in this area. All development lot area allocated to the additional 5 feet of sidewalk width shall be counted as "Public Plaza" space and awarded a development intensity bonus of one square foot of any type of use for each square foot of new sidewalk space created.
- (2) Upon County Road Department's approval, sidewalk surfaces may be covered in materials utilized in adjacent new developments where such special sidewalk surfacing will enhance the appearance and continuity of localized pedestrian circulation space. Where used, such special surfacing must involve the entire frontage of a development project.

### D. Curb Cut Restrictions

No driveway curb cuts for parking or loading facilities shall be permitted on Master Planned Highway frontage. Exceptions may be granted by the Regional Planning Commission and CALTRANS, where there are no alternative means of access from other streets bounding the respective block or property.

### E. Ground Floor Facade Design

- (1) Ground floor street facades of developments shall be built contiguous to the adjacent sidewalk edge along no less than 50% of a development's Master Planned Highway frontages. Recesses in this sidewalk edge facade shall not exceed 10 feet in depth from the sidewalk and shall not, in aggregate, involve more than 20% of the facade meeting the zero setback requirement.
- (2) Banks, savings, lending and other financial institutions, and office lobby spaces shall, in aggregate, occupy no more than 20 percent of the ground floor Master Planned Highway frontage of any respective continuous block.
- (3) The following provisions apply to all ground floor building facades along Master Planned Highway frontages and those adjacent to plazas, arcades and other pedestrian circulation spaces:

- (a) At least 50 percent of the front facade wall of a development shall be glazed at the ground floor level with transparent material permitting visual access from adjacent public circulation areas to interior spaces or displays of business establishments. For the purpose of glazing requirements, the building's front facade wall surface at the ground floor level shall be measured from the floor to the height of the ceiling or 12 feet above adjacent sidewalk grade, whichever is less.
- (b) All new ground floor commercial spaces fronting on a Master Planned Highway or a public plaza shall have their respective main entrances directly on the adjacent publicly accessible pedestrian space or sidewalk. Doorways may be recessed up to 10 feet from building facade line. Doorway recesses shall be no greater than 20 feet in width as measured along the front facade.

#### F. Delivery and Maintenance Access Facilities

- (1) All uses in new developments shall be supplied with off-street loading and delivery facilities. Collections of uses may have pooled facilities where individual facilities are not necessary for efficient use.
- (2) Loading and delivery facilities for all new uses shall be located off-street and, where feasible, out of view from public areas. Curbside delivery shall be prohibited on all Master Planned Highway frontages, and limited on all other street frontages.
- (3) Access to loading and delivery facilities shall be located on street fronts other than Santa Monica Boulevard.

#### G. Public Plaza Design

- (1) All ground level open space located within 100 feet of a development property's Master Planned Highway sidewalk frontage shall be developed as public plaza space meeting the following requirements:
  - (a) Permitted Uses
    - Uses fronting on public plazas shall be limited to the "Permitted Ground Floor Uses" listed in Section III.

- . At least 50 percent of the total frontage of building walls of the development facing on a plaza, exclusive of such frontage occupied by vertical circulation elements, building lobbies and Transit station access, shall be allocated for occupancy by retail sales, sidewalk cafe, or permitted ground floor services uses. Such uses shall not include banks, savings and loan offices or other lending/financial offices.

(b) Area and Dimensions

- . A plaza shall contain an area of not less than 1,000 square feet, but shall not exceed 50 percent of the buildable area of its respective development site.
- . A plaza shall have a maximum dimension, inward from its sidewalk frontage, no greater than its respective frontage dimension along the sidewalk.
- . The main section of a plaza comprising at least 70 percent of a plaza's total contiguous area) shall be so designed that all points within the main section of the plaza are visible from all other points within the main section of the plaza.
- . The main section of any plaza shall be visible from and contiguous with a development's respective Master Planned Highway sidewalk.
- . The main section of a plaza shall have a minimum dimension of 30 feet.
- . Plaza space oriented to highway frontages shall occupy a maximum of 50 percent of the frontage of any development.

(c) Orientation and Adjacent Buildings

- . Buildings constructed on the south side of plazas shall be limited to 25 feet in height at the southern edge of the plaza, and shall be built below a 40 degree vertical profile plane extending southward from a point at the top of the plaza edge 25 foot building height limit line.

(d) Access and Circulation

- . Within 10 feet of a Master Planned Highway sidewalk, along at least 50 percent of a plaza's street frontage, a plaza shall be constructed at the elevation of the adjacent public sidewalk.

- . The elevation of a plaza shall not at any point be more than three feet above or three feet below the curb elevation of the nearest adjacent public sidewalk.

- . No walls or fences shall inhibit or restrict visual or physical access to plazas from adjacent public sidewalk areas.

(e) Use and Facility Restrictions

- . No building trash storage facilities shall be permitted on, or within 25 feet of, any plaza.

- . All trash storage areas shall be screened from view by surrounding fences or other structures. Such screening elements shall be of the same colors and materials as adjacent respective buildings.

- . No driveways, loading facilities, parking spaces or passenger drop off spaces shall be permitted in, or as part of, a plaza.

- . No exhaust vents shall be permitted on any building wall fronting on a plaza, except where such vents are at least eight feet above the adjacent surface of the plaza.

- . Noise, fume or wind producing building mechanical, equipment potentially disruptive to a plazas ambiance shall not be located on, or adjacent to, building walls facing onto plazas.

- . Plazas shall be open to the sky, and covered or canopied only by trees or temporary elements such as canvas or plastic sheets, awnings or umbrellas.

(f) Landscaping and Facilities Requirements

- . A minimum of 150 square feet per 1,000 square feet of plaza shall be developed as planters or planting beds with seasonal flowers, shrubs, ground cover or other plants. Such planted areas shall have a soil surface no more than three feet above the adjacent plaza walking surfaces.

- . Plazas shall be equipped with lighting which supplies two horizontal foot candles of illumination to all portions of the plaza throughout the hours of darkness.
- . One litter receptacle with a minimum capacity of one cubic foot shall be provided for each 1,000 square feet of plaza area.
- . All fast food and/or take out food establishments fronting on or located within 25 feet of a public plaza shall supply litter receptacles, in addition to those generally required for the plaza, at a rate of one receptacle (with a minimum capacity of 3 cubic feet) for each 1,000 square feet of plaza area located within 50 feet of the respective food establishment.
- . Trees, measuring at least 4 inches in diameter at time of planting, shall be planted at a ratio of one tree per 1,000 square feet of plaza area. Adequate soil volume and depth shall be provided for all trees.
- . Seating shall be provided in plazas at a minimum ratio of one linear foot per 40 square feet of plaza area. Not more than 25 percent of the linear seating capacity may be moveable seats, which may be stored between the hours of 7:00 p.m. and 8:00 a.m. Seating shall conform to the following specifications:
  - . Seating with backs at least 12 inches high shall have a minimum depth of 14 inches; while seating without backs shall have a minimum depth of 16 inches.
  - . All surfaces counted as seating must be at least 16 inches deep and no more than 30 or less than 18 inches above the adjacent walking surface.
  - . Tops of walls, planters, fountains and other accessible elements of plazas meeting the above seating specifications may be counted toward seating requirements in plazas.
- . Each plaza over 3,000 square feet in area shall be served by a restroom facility open to the general public during hours when the plaza and/or businesses adjacent to the respective plaza are open to the public. Plaza restrooms shall meet the following standards:

- Plaza restroom entrances shall be visible from, and located within, 25 feet of the plaza's main section, and shall have easily visible identifying signs.
- Plaza restrooms and their entrances shall be lighted in a manner which enhances user safety and security.

(g) Maintenance

- Owners of plazas shall be responsible for their maintenance, including, but not limited to, litter control, care and replacement of vegetation within the plaza and adjacent sidewalk spaces, and public facilities maintenance.

V. SIGN REGULATIONS

- (1) The aggregate surface area of all signs and any other elements in windows (other than displays) which obscure or block visual access from outside to interior business space shall be limited to one-fourth of the window area. No signs or logos are permitted in windows above the ground floor, except as provided in Section 2-a below.
- (2) Signs on building wall surfaces or in windows shall be confined to the ground floor, within 14 feet above sidewalk or plaza level. No signs shall be displayed on wall surfaces or in windows beyond 14 feet above sidewalk or plaza level with the following exceptions:
  - (a) Uses located above ground floor level which are open to the general public and which are both visible and directly accessible from ground or plaza level may display signs conforming to the regulations for ground floor and plaza level uses.
  - (b) One logo per building facade may be displayed above the level(s) at which signs are permitted.
- (3) All signs shall conform to a sign program submitted for approval as a component of the Conditional Use Permit application. The sign program shall:
  - (a) Limit the area of wall surfaces covered by signs, to 2 square feet per linear foot of respective sidewalk or plaza frontage;

- (b) Limit window area covered or obscured by signs;
  - (c) Specify a range of compatible colors and lettering styles and sizes for all signs in the development.
  - (d) Specify consistent lighting and materials for all signs in the development;
  - (e) Specify locational criteria for wall surface signs; and,
  - (f) Describe the physical design and location of any free standing collective sign structure to be built on the development site.
- (4) All business signs shall be displayed on the building facade or windows of respective businesses. Any free-standing signage or advertising shall be confined to kiosks and other collective sign structures approved as part of the design plans of respective developments. Sign boards, billboards and other freestanding individual signs are prohibited.
- (5) No sign shall be constructed on top of, or project above, the roof, eave or parapet line, whichever is lowest, of any building in the Station Plan area.
- (6) Convenience and directional signs with no advertising shall be exempt from restrictions of this section, provided such signs are approved as a part of the required sign program.

#### VI. SHORT TERM TRANSIT SERVING PARKING FACILITIES

- A. All short term parking facilities constructed to fulfill area bonus requirements shall meet the following standards:
- (1) Short term parking facilities shall be designed and constructed to accommodate parking and efficient circulation of at least 20 passenger cars at one time.
  - (2) Short term parking facilities shall be located out of street rights-of-way and within 10 feet of adjacent street curb level.
  - (3) All short term parking spaces shall be located within 400 feet of the station intersection and have pedestrian access to adjacent street sidewalk level or station entrance level.



- (4) Where traffic does not adversely impact residential areas, driveway access to short term parking facilities shall be located on streets other than Master Planned Highways.

## VII. HEIGHT LIMITS

### A. STATION CORE (I)

- (1) Mixed use developments shall not exceed 175 feet of height above street grade on portions of property which are within 75 feet of adjacent properties zoned for residential use. Structure heights on portions of property within 75 feet of adjacent residentially zoned property shall not exceed 60 feet.
- (2) Single use developments shall not exceed 100 feet above street grade, and shall not exceed 60 feet in height within 75 feet of adjacent residentially zoned property.

### B. COMMERCIAL CORRIDORS (II)

- (1) Mixed use developments shall not exceed 80 feet in height above street grade.
- (2) Single use developments shall not exceed 60 feet above street grade.

EFFECTIVE DATE:

An ordinance amending Title 22 (Zoning Ordinance) of the Los Angeles County Code, creating the Fairfax-Santa Monica Transit Corridor Community Standards District.

The Board of Supervisors of the County of Los Angeles do ordain as follows:

SECTION 1. Section 22.44.110 is added to the Los Angeles County Code to read as follows:

<u>District Number</u>	<u>District Name</u>	<u>Ordinance of Adoption</u>	<u>Date of Adoption</u>
2	Fairfax-Santa Monica Transit Corridor	(to be determined at time of adoption)	

SECTION 2. Section 22.44.112 is added to the Los Angeles County Code to read as follows:

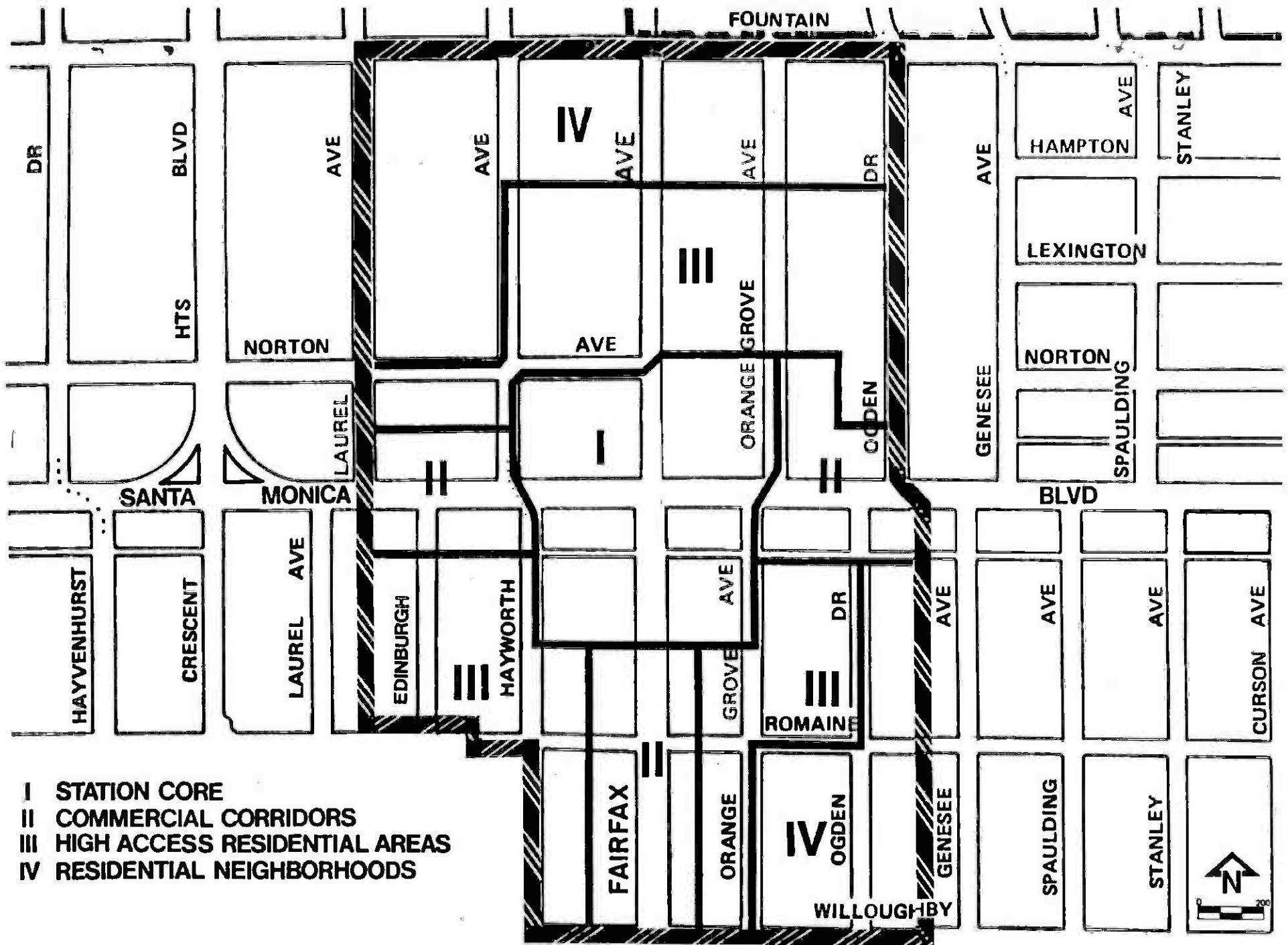
22.44.112 Fairfax-Santa Monica Transit Corridor Community Standards District.

A. INTENT AND PURPOSE

The Fairfax-Santa Monica Transit Corridor Community Standards District is established to provide a means of implementing special development policies and standards contained in the adopted Fairfax-Santa Monica Station Area Plan, and to complement the design and development standards of the Transit Corridor Mixed Use (TC-MU) and Unlimited Residence (R-4) zone requirements within the boundaries of the Station Area Plan.

The standards established for the designated Station Core area are designed to permit increased intensity and scale of development where significant public facilities, pedestrian amenities and station access improvements are incorporated in new developments.

Residential densities and structure height limits are increased in specified areas which are determined to be within easy walking distance of the subway station due to the significant transit access advantages presented to these locations by the station and metro rail system.



- I STATION CORE
- II COMMERCIAL CORRIDORS
- III HIGH ACCESS RESIDENTIAL AREAS
- IV RESIDENTIAL NEIGHBORHOODS



# FAIRFAX-SANTA MONICA TRANSIT CORRIDOR COMMUNITY STANDARDS DISTRICT

## B. DESCRIPTION OF DISTRICT

The Fairfax-Santa Monica Transit Corridor Community Standards District occupies approximately 83 acres of the unincorporated community of West Hollywood. Centered on the intersection of Fairfax Avenue and Santa Monica Boulevard, the Community Standards District extends westward to Laurel Avenue and mid-block between Laurel Avenue and Edinburgh Avenue; eastward to Ogden Drive and mid-block between Ogden Drive and Genesee Avenue; northward to Fountain Avenue; and southward to Romaine Avenue and Willoughby Avenue as shown on the attached map, titled Fairfax-Santa Monica Transit Corridor Community Standards District.

## C. COMMUNITY-WIDE PROVISIONS

### 1. DEFINITIONS

- "Architectural rendering" means a perspective drawing of a proposed development which accurately depicts its scale, architectural features and site characteristics.
- "Building footprint" means the ground area covered by a building, defined by the outside perimeter walls of the structure.
- "Community serving business" means businesses which have local neighborhood residents as their primary clientele, or which provide goods or services which are typically consumed by local residents.
- "Density" means the ratio of residential units per net acre of respective buildable parcel area.
- "Density bonus" means a density increase over otherwise allowable residential density as provided in this title.
- "Development intensity" means the overall size of a commercial or mixed commercial/residential development measured in terms of floor area ratio.
- "Development intensity bonus" means a development floor area ratio increase over otherwise permitted floor area ratio as provided in this title.
- "Floor area ratio" means the numerical value obtained through dividing the gross floor area of a building or buildings located on a lot or parcel of land by the total area of such a lot or parcel of land.

- "Ground floor" means any floor or portion of a building with its floor surface located within three feet above or below the elevation of the street curb adjacent to the respective development.
- "Ground floor facade" means the outside wall, be it solid, glass, open or a combination of such conditions, of the ground floor of a building up to an elevation of 14 feet above the nearest adjacent public sidewalk or public plaza walking surface.
- "Ground floor uses" means the specific types of businesses, activities and facilities located in the ground floor space of a building or buildings.
- "Ground level" means the public sidewalk, public plaza or street level of a development and refers to outdoor and other unenclosed spaces on the development site.
- "Mixed commercial development" means a combination of different commercial uses in one development.
- "Mixed use development" means a combination of different types of uses including both commercial and residential uses in one development.
- "Outdoor cafe" means a table service or standing counter eating establishment at ground level which has temporary or no overhead weather protection.
- "Outdoor restaurant" means a table service eating establishment at ground level which has temporary or no overhead weather protection.
- "Plaza level" means the elevation of the walking surface(s) of a public plaza.
- "Short term parking facilities" means automobile parking or stopping lots, spaces or facilities designed and managed to accommodate temporary stopping for loading and unloading of metro rail system riders.
- "Single use development" means a development project which is designed for occupancy by one basic type of use such as commercial or residential.
- "Sidewalk cafe" means a table service or standing counter eating establishment which is open on at least one side to a public sidewalk or public plaza and has permanent, temporary or no overhead weather protection.
- "Transparent facade" means a ground floor building wall or plane which can be seen through from an adjacent public sidewalk or plaza.

## 2. CONDITIONAL USE PERMIT REQUIREMENTS

All new developments and renovations or additions which alter the building footprint or front facade or adds useable floor space to existing structures on TC-MU zoned property located within the Fairfax-Santa Monica Station Plan area shall be subject to Conditional Use Permits. In addition to the requirements and procedures specified in Section 22.56.010 - 22.56.255 of this title, applications for Conditional Use Permits shall include complete elevations of proposed projects and, at the Planning Director's discretion, may be required to include architectural renderings of the proposed project.

## 3. DEVELOPMENT AGREEMENTS

Applicants for Conditional Use Permits may, at the County's discretion, be required to enter into development agreements with Los Angeles County and/or the Southern California Rapid Transit District to meet specific infrastructural development requirements and/or joint or cooperative development agreements related to respective development proposals.

## D. AREA SPECIFIC STANDARDS

### 1. STATION CORE

A. On properties located within the Station Core area delineated on Map I, floor area ratios may be increased on project sites of 30,000 square feet or greater to a maximum of 6.0, and to a maximum of 4.0 on project sites of less than 30,000 square feet under the following conditions:

#### (1) Eligibility:

Bonus eligibility is contingent upon the inclusion of at least one of the following First Priority Uses in each project:

#### (2) First Priority Uses                      Floor Area Bonus

For each one (1) square foot of:

- |   |  |
|---|--|
| . Grocery Store/Super market (at least 20,000 sq. ft. in size)              | . 2 additional sq. ft. of any permitted use. |
| . Community-Serving business or Facilities (equivalent to at least 0.3 FAR) | . 2 additional sq. ft. of any permitted use. |

- . Short Term Metro Rail-Serving Facilities . 2 additional sq. ft. of any permitted use.
- . Additional Entrance to Metro Rail Station . 4 additional sq. ft. of any permitted use.

(3) Second Priority Uses

For each one (1) square foot of:

- . Ground floor retail sales space visible and accessible from Fairfax or Santa Monica sidewalk frontage. . 1 additional sq. ft. of any permitted use.
- . Ground floor or ground level restaurant space open to, and accessible from, a public plaza or Fairfax or Santa Monica sidewalk frontage. . 1 additional sq. ft. of any permitted use.
- . Ground floor public plaza space with required amenities and facilities. . 1 additional sq. ft. of any permitted use.

2. HIGH ACCESS RESIDENTIAL AREAS

A. On properties located within the High Access Residential Areas delineated on Map I, all conditions and requirements of the R-4 zone as regulated by the West Hollywood Community Standards District apply with the following exceptions.

(1) Residential densities may be built to density limits as follows:

- . 70 dwelling units per net acre for condominiums and other units for sale.
- . 100 dwelling units per net acre for developments consisting entirely of rental units.

(2) Residential structures may be built to 80 feet in height, with the exception that within 50 feet of residential property with 45 foot height limits, building height shall not exceed 60 feet.

(3) Parking structures constructed in Community Plan-designated Parking Overlay Zones shall not exceed 35 feet in height above street grade.

### 3. PARKING REQUIREMENTS

A. Parking Requirements for specified commercial uses in TC-MC zones may be reduced below County Code requirements to the levels outlined in section B below. Reductions of office and retail commercial parking requirements shall be permitted only in conjunction with project developer-and/or employer-sponsored programs to reduce parking demand through ride-sharing, shuttle service, car pooling, mass transit or other modes of access presenting a viable long term alternative to automobile access and related on-site parking facilities for employees and/or patrons. All employee parking demand not obviated by access and parking alternative programs must be accommodated by developer-and/or employer-supplied parking within 500 feet of respective commercial facilities and businesses.

B. Upon Regional Planning Commission approval of a long term program reducing parking demand, a development and/or use may reduce its on-site required parking commensurate with the demonstrated reduction in parking demand as a result of such a program to the limits specified as follows:

#### (1) STATION CORE (I)

Office uses	- 1 parking space per 600 sq. ft. of floor area
Residential units - single, bachelor and one bedroom	- 1 parking space per unit

#### (2) COMMERCIAL CORRIDORS (II)

Office Uses	- 1 parking space per 600 sq. ft. of floor area
Retail Commercial	- 1 parking space per 600 sq. ft. of floor area
Personal Services Retail Sales and Restaurants	- May have required parking in off-site lots within 500 feet of respective



C. Parking requirements for specified units in designated High Access Residential Areas are reduced below County Code requirements as follows; while requirements for Residential Neighborhoods are specified by the County Code.

(1) HIGH ACCESS RESIDENTIAL AREA (III)

Single, bachelor and one-bedroom units - 1 parking space per unit

(2) RESIDENTIAL NEIGHBORHOODS (IV)

Single, bachelor and and one-bedroom units - 1 1/2 parking spaces per unit (conforms to current County Code requirements)